

# Environmental Assessment

SR0080 Section 08B

## I-80 Over Lehigh River Bridges Project

*White Haven Borough, Luzerne County*

*East Side Borough and Kidder Township, Carbon County*

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## ACRONYMS AND ABBREVIATIONS

ALPP	Agricultural Land Preservation Policy
AOC	Areas of Concern
APE	Area of Potential Effects
ATON	Aids to Navigation
BMPs	Best Management Practices
CCCD	Clarion Country Conservation District
CE	Categorical Exclusion
CFR	Code of Federal Regulations
CRPs	Cultural Resource Professionals
CWF	Cold Water Fishery
DCNR	Department of Conservation & Natural Resources
DEP	Department of Environmental Protection
DHS	Department of Human Services
E&S	Erosion & Sedimentation
EA	Environmental Assessment
EB	Eastbound
ECMTS	Environmental Commitments & Mitigation Tracking System
EDD	Environmental Due Diligence
ESA	Environmental Site Assessment
ESF	Environmental Stewardship Fund Act
EV	Exceptional Value
FEMA	Federal Emergency Management Agency
FHWA	Federal Highway Administration
FIRM	Flood Insurance Rate Map
FPPA	Farmland Protection Policy Act
GFS	Girder–Floorbeam–Stringer
GG2	Growing Greener Bond Fund
H&H	Hydrology and Hydraulics
HOV	High Occupancy Vehicle
HQ	High Quality
HQ-CWF	High Quality-Cold Water Fishes

I-79	Interstate 79
I-80	Interstate 80
I-81	Interstate 81
I-476	Interstate 476
Key 93	Keystone Recreation, Park and Conservation Fund
LIHEAP	Low Income Home Energy Assistance Program
LOD	Limits-of-Disturbance
LWCF	Land and Water Conservation Fund
MF	Migratory Fishery
MIT	Massachusetts Institute of Technology
MPO	Metropolitan Planning Organization
MSATs	Mobile Source Air Toxics
NAC	Noise Abatement Criteria
NAAQS	National Ambient Air Quality Standards
NEPA	National Environmental Policy Act
NESHAP	National Emission Standards for Hazardous Air Pollutants
NPDES	National Pollutant Discharge Elimination System
NPS	National Park Service
NRCS	Natural Resources Conservation Service
NRHP	The National Register of Historic Places
OSHA	Occupational Safety and Health Administration
P3	Public Private Partnership
PA	Pennsylvania
PA DCNR	Pennsylvania Department of Conservation and Natural Resources
PADEP	Pennsylvania Department of Environmental Protection
PAGWIS	Pennsylvania Groundwater Information System
PASPGP	Pennsylvania State Programmatic General Permit
PCSM	Post Construction Stormwater Management
PEL	Alternative Funding Planning and Environmental Linkages Study
PEM	Palustrine Emergent
PennDOT	Pennsylvania Department of Transportation
PFBC	Pennsylvania Fish and Boat Commission

PFO	Palustrine Forested
PNDI	Pennsylvania Natural Diversity Inventory
PSA	Project Study Area
PSS	Palustrine Scrub-Shrub
RFFAs	Reasonably Foreseeable Future Actions
RIRA	Recreational Improvement and Rehabilitation Act
ROW	Right-of-Way
SNAP	Supplemental Nutrition Assistance Program
SR	State Route
TCE	Temporary Construction Easements
TIP	Traffic Improvement Program
TMC	Traffic Management Center
TNM	Traffic Noise Model
TOYRs	Time-of-Year Restrictions
TSF	Trout Stock Fishery
USACE	US Army Corps of Engineers
USDA	United States Department of Agriculture
USDOT	U.S. Department of Transportation
USFWS	U.S. Fish and Wildlife Service
USGS	U.S. Geological Survey
USTs	Underground Storage Tanks
UNT	Unnamed Tributary
VMT	Vehicle Miles Traveled
WB	Westbound
WSE	Water Surface Evaluation
WWF	Warm Water Fishery

## 1.0 INTRODUCTION

The Pennsylvania Department of Transportation (PennDOT) has an \$8.1 billion (and growing) funding gap between its current funding levels and what it needs to provide a system of highways and bridges in a state of good repair. To fill this funding gap, PennDOT developed an alternative funding program called PennDOT Pathways and prepared an **Alternative Funding Planning and Environmental Linkages (PEL) Study** to identify potential near- and long-term solutions for highway and bridge funding.

A PEL Study is a flexible tool that can be used to connect the planning process with the environmental process required by National Environmental Policy Act (NEPA). The analyses conducted for the PEL Study can be incorporated by reference into the subsequent NEPA process, which can facilitate completion of the NEPA process.

In the case of the Alternative Funding PEL Study, PennDOT used the PEL Study to:

- Establish the purpose and needs for additional highway and bridge funding.
- Identify potential funding sources and analyze them for meeting the near-term and longer-term funding needs.
- Develop a plan for implementation, which identified alternative bridge funding as an immediately needed priority and bridge tolling as the reasonable means for financing priority bridge improvements.

The Major Bridge Public-Private Partnership (P3) Initiative resulted from this conclusion.

PennDOT developed the Alternative Funding PEL Study with input and oversight from Federal Highway Administration (FHWA) and undertook an extensive public and agency outreach program via the PennDOT website, social media, e-newsletters, and public meetings. PennDOT provided opportunities for public input on the PEL Study via a public engagement platform on the Pathway Program’s website which ran between November 17 and December 17, 2020. In addition, comments were solicited during a virtual public meeting held from February 19 to March 23, 2021, when the nine candidate bridges for major bridge tolling were announced. Finally, the Draft PEL Study was made available for formal public comment from April 29 to June 1, 2021. The PEL Study contains details on the outreach activities and the comments received.

### 1.1 Purpose and Need for Bridge Tolling

The PEL Study documents the purpose and needs for alternative sources of highway and bridge funding. The results are summarized below and the full results from the PEL Study are incorporated herein by reference.

As discussed in the PEL Study, highways and bridges represent the largest PennDOT transportation expenditure. Moreover, maintaining and improving highways and bridges in Pennsylvania is highly dependent (approximately 75% of funding) on gas taxes for funding—the funding source that is at greatest risk of decline. In recent years, gas taxes have become a less predictable source of revenue for transportation agencies across the country. As passenger vehicles become more fuel-efficient, and all-electric vehicle technology continues to evolve, gas tax revenues are being reduced. The continued trend toward more electric vehicles, both for personal and commercial transportation, will further reduce gas consumption and revenue from gas taxes.

The result is that the gap between available transportation revenue and the projected funding required to adequately maintain and improve reliable highways and bridges in the Commonwealth is substantial. Available budgeted funding for highways and bridges is currently \$6.9 billion per year, while the identified current (2020-

Supporting documentation for Chapter 1 includes:

- [Alternative Funding: Planning and Environmental Linkages Study \(September 2021\)](#)



2021) funding need is \$15 billion, resulting in a current funding gap of \$8.1 billion. This funding gap is projected to increase by about \$400 million per year to \$12.6 billion in 2030.

Subsequent to the completion of the PEL Study, the new 2021 federal transportation act (Infrastructure Investment and Jobs Act), the largest federal infrastructure investment in decades, was passed and is expected to bring \$4 billion in new federal highway and bridge funds to Pennsylvania spread over 5 years. Because federally funded projects usually require a 20 percent state match, PennDOT will need approximately \$1 billion in additional state funds to be able to fully leverage the new federal funds. With roughly \$2.2 billion in construction value, if federal funds were committed to the nine candidate bridge projects in the Major Bridge P3 Initiative, those projects would take up more than half of the new federal funding. This means that these much-needed funds will only benefit a handful of projects, minimizing the bill's overall effect for Pennsylvania. While the Infrastructure Investment and Jobs Act investment is a huge step in the right direction, it does not fully address Pennsylvania's highway and bridge funding challenges, and therefore PennDOT is continuing to advance the Major Bridge P3 Initiative.

Insufficient funding for critical maintenance work and for expanding the capacity of roadways in the Commonwealth places a burden on taxpayers and drivers. Inadequate timely maintenance results in more extensive and more expensive repairs in the long run, increasing the overall cost to taxpayers. Poor asset conditions and congestion translate into additional costs to roadway users, including more time spent driving in congested conditions, higher vehicle maintenance costs, and increased emissions. Additional delay experienced by freight transportation can also translate to higher prices to consumers.

## 1.2 Summary of Potential Bridge Financing Alternatives

PennDOT evaluated a number of potential funding options to fill their highway and bridge funding gap as summarized below. The full analysis of highway and bridge funding alternatives is contained in the PEL Study and is incorporated herein by reference.

PennDOT evaluated the following mechanisms to fill their highway and bridge funding gap: sales taxes, personal income taxes, real estate and property taxes, fuel tax increases, road user charges, other taxes and fees, and various forms of tolling. Based on the analysis in the PEL Study, each of the potential funding options has some merit and could be considered as part of PennDOT's long-term strategy in securing sustainable and dedicated revenue for highways and bridges. However, without action by the legislature and/or others, PennDOT only has the ability to implement the following potential solutions: (1) Bridge Tolling and (2) Managed Lanes. To support bridge improvement needs, like those evaluated in the accompanying NEPA document, PennDOT identified bridge tolling as the reasonable near-term financing mechanism for the following reasons:

- Those that use the bridge will pay for it.
- It provides dedicated funding that is used to construct and maintain the bridge from which the toll was collected.
- It helps keep the regional transportation funding program from being diverted to the interstate program.
- Toll collection systems already exist in Pennsylvania, thereby lowering the cost of collection.
- PennDOT has mechanisms in place to implement and collect bridge tolls.

Today, PennDOT must divert funding away from regional projects across the state to fund critical interstate and bridge needs. Moving forward with bridge tolling will allow critical bridges to pay for themselves through tolls, keeping funds available for other regional projects to also move forward. Bridge tolling is also being considered

as a solution due to the proven success of toll collections throughout the country as a funding strategy for the replacement or rehabilitation of bridges.

### 1.3 How were bridges selected for the initial tolling program?

To meet critical bridge funding needs, PennDOT created the first initiative of the **PennDOT Pathways Alternative Funding Program – The Major Bridge P3 Initiative**. The Major Bridge P3 Initiative is designed to raise revenue through tolling to address the state’s growing backlog of replacement and rehabilitation needs for major bridges that are approaching the end of their useful lives. At the end of a bridge’s useful life, substantial repairs and emergency lane closures become more frequent.

PennDOT is considering nine candidate bridges across the state of Pennsylvania for tolling through the Major Bridge P3 Initiative. These candidate bridges are being considered because they meet the following criteria:

- Located on the interstate or expressway
- Structures of significance based on size, location, and cost to replace or rehabilitate
- Structural conditions that warrant timely attention to enhance safety and avoid disruption and community impacts if closure or weight restrictions were imposed
- Geographic balance across the state
- Can begin construction in two to four years for near-term benefit
- The ability for the project to be financially viable with a reasonable toll rate

**Table 1** identifies the initial list of candidate bridge projects meeting these criteria.

**Table 1  
Candidate Bridge Projects**

PennDOT District	Bridge Project	Year(s) Built
4	I-81 Susquehanna Bridges Project	1961
4	I-80 Nescopeck Creek Bridges	1965
5	I-78 Lenhartsville Bridge Replacement Project	1955
5	I-80 Over Lehigh River Bridges Project	1965
6	I-95 Girard Point Bridge Improvement Project	Late 1960s-1970s
8	I-83 South Bridge Project	1960
10	I-80 Canoe Creek Bridges	1966
10	I-80 North Fork Bridges Project	1962
11	I-79 Widening, Bridges and Bridgeville Interchange Reconfiguration	1965

Each candidate bridge project is undergoing environmental studies in accordance with the NEPA and other applicable environmental laws and regulations. This includes an assessment of the potential effects of tolling on low-income and minority populations in accordance with a Presidential Executive Order 12898 on Environmental Justice. This Environmental Assessment (EA) is the NEPA documentation for the I-80 Over Lehigh River Bridges Project. Effects on communities due to traffic choosing to avoid the toll are considered in the I-80 Lehigh River Environmental Justice Analysis Technical Memorandum, March 2022, which is summarized in Chapter 6 of this EA.

## 2.0 I-80 OVER LEHIGH RIVER BRIDGES PROJECT OVERVIEW

### 2.1 Project Bridges

PennDOT, in cooperation with the Federal Highway Administration (FHWA), is advancing the replacement of two bridges on Interstate 80 (I-80), Section 08B. The dual bridges carry I-80 over the Reading Blue Mountain Northern (RBMN) railroad, Lehigh Gorge State Park and Trail, Lehigh River, and State Route (SR) 1005 (River Road), in White Haven Borough, Luzerne County, and East Side Borough and Kidder Township, Carbon County, Pennsylvania. The anticipated project limits are approximately 1.5 miles along I-80 and are bound between the White Haven interchange, Exit 273, to the west and the Hickory Run State Park interchange, Exit 274, to the east (mile marker 272.5 to mile marker 273.3).

### 2.2 Project Purpose and Needs

**Purpose:** The purpose of this project is to address the deterioration of the I-80 Lehigh River Bridges and thereby provide safe and structurally sufficient bridges that will provide connectivity for interstate travelers, commuters, commercial users, emergency services, tourists, and local residents.

**Needs:**

- The existing I-80 dual bridges are more than 50 years old. The bridges, originally constructed in 1965, have nonredundant critical elements, show deterioration, and are approaching the end of their service life. The bridges underwent a major rehabilitation in 1981 and 1982 for westbound and eastbound, respectively. Maintenance and repairs were completed on the bridges in 1991 and again in 2014. Due to the age and condition of the bridges, they require maintenance and repairs at increased frequency. The most recent significant rehabilitation of the bridges occurred over 35 years ago in 1981/1982, and the bridges require rehabilitation or replacement in the near future.
- I-80 provides a critical link in the Interstate commerce system, and as such is vital to efficient movement of goods and people. Further deterioration of the I-80 Lehigh River Bridges could cause the bridges to be load posted.

Based on the 2018 bridge inspections, the superstructures and substructures of both I-80 bridges received condition ratings of “5” on a scale of 0 to 9, which indicates the bridges are in “Fair” condition. The 2014 inspection of the eastbound bridge determined that the substructure was in a condition state of “4” or “Poor.” Subsequent repairs were performed to restore the substructure condition to “5” or “Fair.” A condition state of “4” or “Poor,” if not addressed, would require the use of a Safe Load Capacity Reduction Factor of 0.8 which would reduce the operating load ratings by 20 percent. This reduction in the load rating would require the eastbound bridge to be load posted. Load posting of the interstate bridge would negatively affect commerce and connectivity in the region and is not an acceptable outcome.

The Purpose and Need for tolling these bridges are discussed in Chapter 1.1 of this EA.

### 2.3 Project Setting and Distinct Project Features

The general site topography is forested rural woodlands with rolling terrain. The project is bounded by the White Haven Interchange and the Borough of White Haven to the west, and East Side Borough and the Hickory Run State Park Interchange to the east. The project is surrounded by a mix of residential, commercial, and public use (Sections 6(f) and 4(f)) properties. The public use properties consist of the White Haven Borough owned Lehigh Park in the northwest quadrant and the Commonwealth-owned Lehigh Gorge State Park which encompasses the

Lehigh River and the Lehigh Gorge Trail. The Lehigh River is a State Scenic River and a PA Fish and Boat Commission (PFBC) designated water trail. Near the western extent of the project, I-80 is separated by a grass median. The eastern extent of the project contains a grass median near the existing bridges which transitions to a forested median near the limit of work.

**Describe the involvement with utilities with this project:**

No aerial utilities are present along I-80. Aerial utilities are present along SR 1005 underneath the I-80 structures and along SR 940. Underground utilities have not been identified within the project limits.

**Describe the involvement with any railroad (active or inactive) including all rail lines, crossings, bridges, or signals:**

The bridges carry I-80 over the active Reading Blue Mountain and Northern Railroad line. Coordination with the railroad is required.

**Describe changes to access control:**

No changes to access control are needed.

## 3.0 ALTERNATIVES

### 3.1 No-Build Alternative

Under the no-build alternative, regular maintenance would be assumed to occur. This alternative would fail to address other project needs such as addressing the identified nonredundant critical elements and bridge deterioration. The I-80 Lehigh River Bridges are nearing the end of their useful life. Currently, both the EB and WB bridges are in fair condition and have cracks and delaminated concrete on decks and barriers, rust and section loss in steel superstructure elements, rust, and section loss in floorbeams, and delaminated concrete with exposed reinforcement in abutments, piers, and wingwalls. Without replacement, these bridge structures will need more frequent maintenance and repairs. However, such maintenance can only extend the service life of these bridges for so long before they are at risk of failure.

I-80 is the longest east-west interstate in the Commonwealth of Pennsylvania. Within Pennsylvania, I-80 extends 311 miles across the northern tier of Pennsylvania, providing access to New Jersey, the New York City Metropolitan Area and New England to the east and Ohio and Midwestern states to the west. In the project area, the I-80 corridor is a vital link between two north-south interstates, I-81 to the west and I-476 to the east and is critical for the movement of people and goods through the northern tier of Pennsylvania and beyond. As a critical link in the regional and national highway network, allowing the deterioration of these bridges to reach a level of failure is not reasonable; therefore, due to the project needs, the no-build alternative would not be a reasonable alternative.

The no-build alternative is presented in this EA as a baseline for comparison purposes only.

### 3.2 Proposed Action

#### 3.2.1 Bridge Replacement

The project is located in White Haven Borough, Luzerne County and East Side Borough and Kidder Township, Carbon County, Pennsylvania. The eastbound bridge will be replaced on a new alignment immediately to the south of the existing eastbound bridge. The new eastbound alignment will tie into the existing alignment approximately 1370 feet to the west of the bridge and 1310 feet to the east of the bridge. The westbound bridge will be replaced on the same approximate alignment as the existing westbound bridge. At Exit 273 (SR 940), the eastbound on-ramp will be reconstructed on a new alignment shifted south of existing, and the westbound off-ramp will be reconstructed on the same approximate alignment as the existing ramp.

The project includes the extension and repair of the existing cast-in-place reinforced concrete box culvert which carries an Unnamed Tributary to the Lehigh River under I-80, approximately 1000 feet to the east of the Lehigh River bridges. The existing wing walls and approximately 8 feet of the existing culvert will be removed and replaced with a new section of 39- and 1/2-foot-long reinforced box culvert. Additional information is provided in Table 2 – Construction Station and Length, Appendix A – Engineering Information, Appendix B – Project Design Exhibit, and Appendix C – Preliminary Design Plans.

**Supporting documentation for Chapter 3 includes:**

- [I-80 Lehigh River Bridges Diversion Traffic Evaluation \(March 2022\)](#)

**Table 2  
Construction Station and Length**

Limits of Work (Segment/Offset)		Construction Stations	
Start:	End:	Start:	End:
2720/1097	2743/1236	2720/0997	2743/1136
2721/2330	2731/2253	2721/2430	2731/2153
<b>Total Length:</b>			
98,325.6 ft			

The bridge replacement and roadway work will impact 8 parcels with 2 parcels totally taken, 1 parcel with an aerial easement, 3 parcels with only temporary construction easements (TCEs), 1 parcel with an aerial easement permanent slope easement and TCEs, and 1 parcel with both partial takes and TCEs.

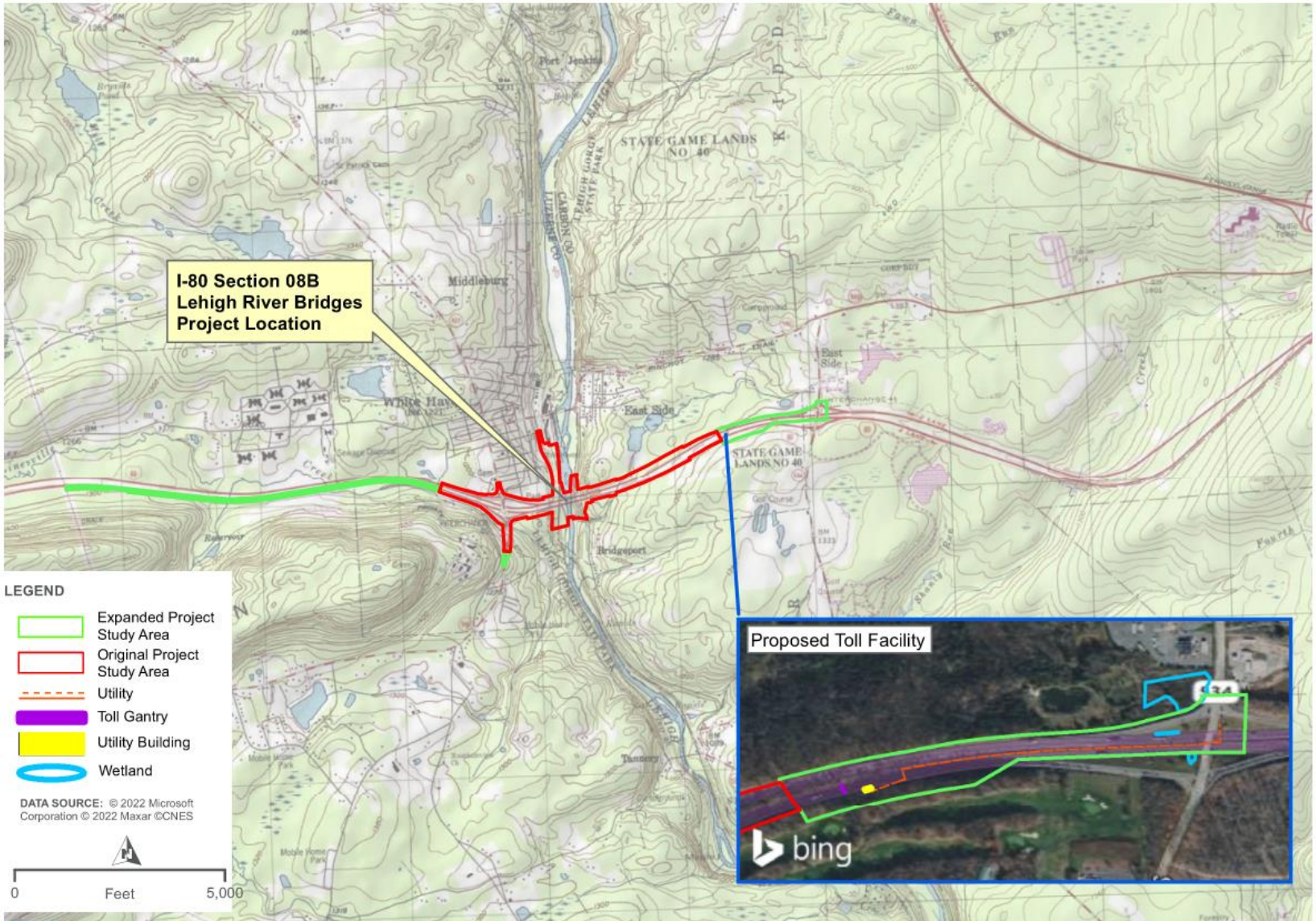
### 3.2.2 Tolling Facility

The I-80 Lehigh River Bridges were identified as a candidate for bridge tolling through PennDOT Pathways Program: The Major Bridge P3 Initiative. With the addition of tolling, the Project Study Area (PSA) was expanded for construction of toll gantries and the associated utility shed, utility connections, and advance “toll ahead” signs. **Figure 1 – Project Location Map** shows the Original PSA for the bridge replacement/roadway improvement project and the Expanded PSA for the toll facility. The Expanded PSA is entirely within Commonwealth-owned existing legal Right-of-Way (ROW). Design plans for the toll facility are in Appendix D.

Based on feedback received from the public and at stakeholder workshops, and because of the close proximity of the two candidate bridges on the western end of the I-80 and the two on the eastern end, PennDOT has decided to pursue one-way tolling on four I-80 bridge projects: Lehigh River, Nescopeck, Canoe Creek, and North Fork bridges. Traffic would be tolled eastbound at Lehigh River, westbound at Nescopeck, and eastbound at Canoe Creek, westbound at North Fork. The one-way tolling will reduce the number of tolls drivers would have to pay on I-80, as well as overall diversions and the need for additional tolling infrastructure.

To implement the toll with All-Electronic Tolling (such as E-ZPass or bill-by-mail), a toll collection facility (gantry, building and utilities) would be constructed for the I-80 eastbound lanes. The tolling facility is proposed to be located east of the bridges over Lehigh River, just before the off-ramp for Exit 274 (SR 534). The All-Electronic cashless tolling facility would not require drivers to stop or slow to pay a toll at the tolling gantry, as the tolling system would record vehicles as they pass under the gantry sensor. The toll gantry location is shown on **Figure 1 – Project Location Map**.

In addition, advanced tolling signs will be placed along I-80 eastbound beginning approximately 2 miles prior to Exit 273 (SR 940). With these advance warning signs located west of the toll bridges, the signage will notify drivers about the upcoming toll bridge and toll collection and give them a chance to exit to avoid the toll. Signs will also be placed along SR 940 to alert drivers entering I-80 eastbound at this exit. The advance tolling signage will consist predominantly of the installation of post-mounted signs or the addition of plaques to pre-existing signs within I-80 and local roadway network. Neither the tolling facility nor its advance tolling signage would require any additional ROW or TCEs.



**I-80 SECTION 08B, LEHIGH RIVER BRIDGES  
PROJECT LOCATION MAP  
EXPANDED PROJECT STUDY AREA**

### 3.2.3 Transportation and Travel Patterns

A primary diversion route consisting of SR 940 and SR 534, north of I-80, was identified. Traffic modeling indicated that with currently proposed eastbound-only tolling, approximately 1,360 vehicles eastbound (5% of I-80 traffic) would divert daily from I-80. It is estimated that 940 of the diverting vehicles would use this primary diversion route. It was originally proposed to provide tolling in both directions of I-80; however, a subsequent decision was made to implement tolling only in the eastbound direction for this bridge. As a result, there are no diversions expected in the WB direction. Highway capacity analysis and crash analysis was conducted to identify areas of existing and anticipated concern. A stakeholder workshop was conducted on August 9, 2021, to gather additional information on potential issues along the diversion routes. From this, a matrix of issues was identified, and further analysis was conducted to determine the impact of toll diversion traffic, and to identify mitigation measures if appropriate.

Based upon this evaluation, the following improvements along the diversion route are proposed to accommodate the effects of tolling diversion:

- Prohibit 102" twins and trailers over 28.5 feet along SR 940 from SR 534 to I-80 in White Haven, similar to the existing restriction on SR 437, north of SR 940.
- Review the existing speed limit signage along SR 940 between the I-476 interchange and the Lehigh River, and supplement in appropriate locations, provide paint striping and radar speed signs emphasizing speed limit.
- Improve the signage on eastbound SR 940 approaching East Side Borough to clarify that vehicles continuing east on SR 940 must stay to the left and that trucks are prohibited on Bridge Street.
- Install a flashing beacon for crossing SR 940 at Main Street to improve the visibility of the crosswalk and repaint crosswalk at the existing crossing of the D&L Trail at SR 940.
- Install curb bulb outs along SR 940 at both Main Street and Towanda Street in order to reduce the width of roadway that needs to be crossed by pedestrians and improve the visibility of people waiting to cross SR 940.
- Mill and repave SR 940 (Berwick Street) between Church Street and Towanda Street, including base repairs at the multiple areas of significant pavement shoving/rutting.
- Improve the overall vertical alignment of SR 940 in the immediate area of the railroad tracks in White Haven to minimize some of the steep grade differences.
- Shift the double yellow lane line, on the westbound SR 940 approach to the intersection with SR 437 by approximately 5 feet to the north, to allow the northbound right-turn movement to occur without encroachment into the adjacent travel lane.
- Mill the existing pavement and repave the intersection of SR 940 and SR 437, to reestablish the curb line and update the ADA ramps at the intersection to meet the new pavement elevation.
- In order to calm traffic traveling on SR 940, east of the Lehigh River, reduce the lane width along SR 940, between Springhill Road and Centre Street, from 11 feet to 10 feet through modified paint striping and the addition of 3 center islands to emphasize the cross-section change and install a gateway treatment at the eastern end of this corridor.
- Modify the existing pavement striping on SR 534 at SR 940 to bring left-turning SR 534 vehicles closer to a 90-degree angle at the intersection.



- Repair/replace the gabion retaining wall, adjacent pavement, and guide rail along SR 940 along the curve to the east of the Lehigh River.

The locations of these proposed traffic improvements are shown on **Figure 2 – Proposed Traffic Improvements**.

Additionally, a before/after study to evaluate actual toll diversion volumes and roadway performance was recommended and will be conducted to evaluate actual volumes, including truck traffic, compare to the projections in this study, and, if appropriate, identify and evaluate additional mitigation measures.

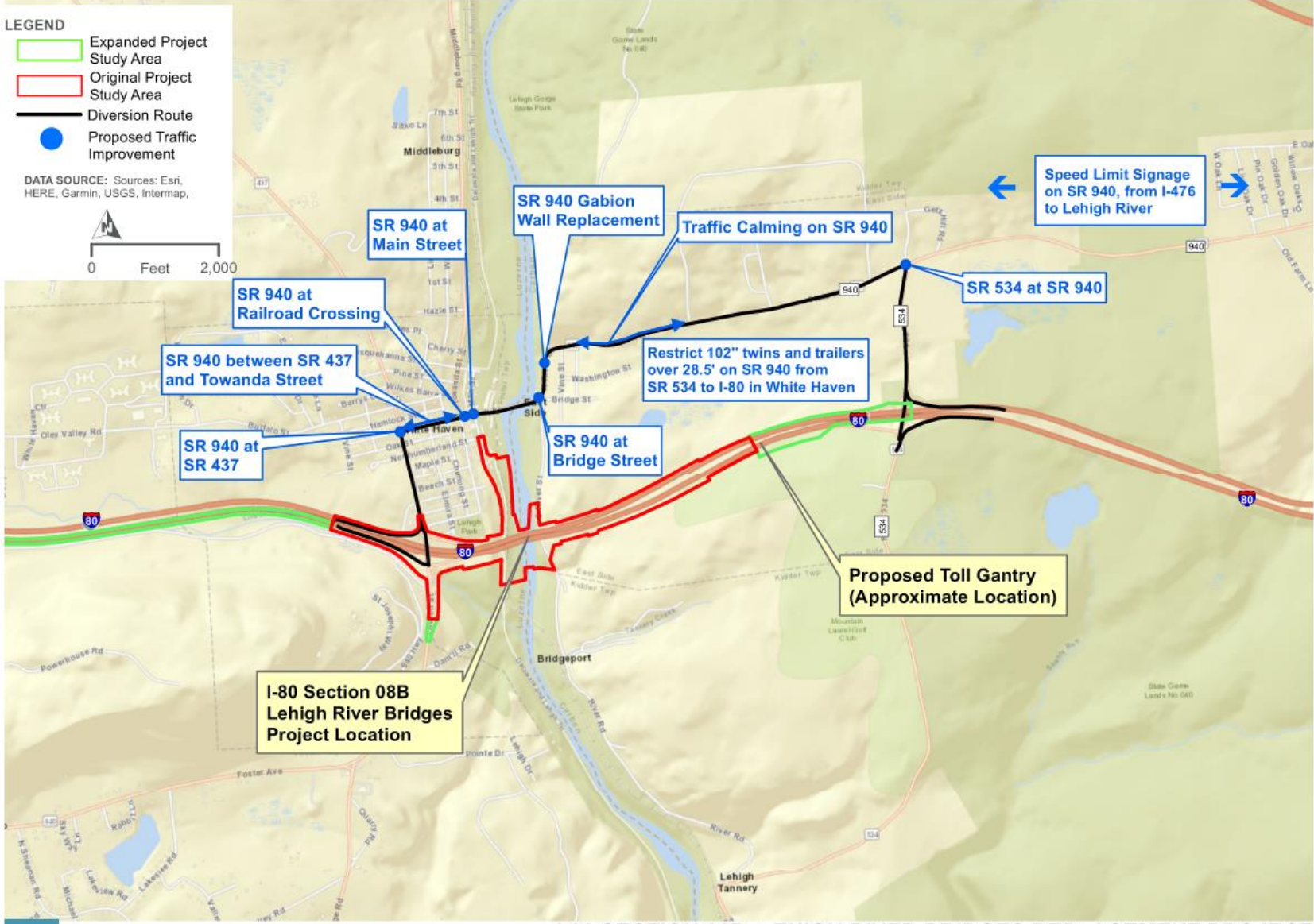
Of the proposed traffic improvements, three involve signage and restrictions on SR 940 and two will improve crosswalks in White Haven Borough. Also in White Haven, four improvements on SR 940 will mill/re-pave existing roadway, address vertical grade and sight distance issues, and improve SR 940/437 intersection operation, all of which will be accomplished within Commonwealth-owned existing legal ROW. Additionally, the traffic improvements on SR 940 in White Haven will not encroach toward adjacent properties beyond the existing roadway pavement and sidewalks.

In East Side Borough, two traffic improvements will also be accomplished within PennDOT’s existing legal ROW: traffic calming and gateway improvements between Springhill Road and Centre Street (**Figure 3 – SR 940 Traffic Calming in East Side Borough**), and pavement re-striping at SR 534/SR 940 intersection (**Figure 4 – SR 534 & SR940 Intersection Pavement Restriping**). It is anticipated that due to the limited scope of work, these improvements will not have a “design footprint” (or limit of disturbances) with potential to impact environmental resources.

The remaining proposed traffic improvement, replacing the existing gabion wall on SR 940, will encroach on land that is not already incorporated into the existing roadway/shoulder and is expected to extend outside of PennDOT’s legal ROW. A conceptual design of the improvement is shown on **Figure 5 – SR 940 Replace Gabion Wall**. Based on conceptual design, replacement of the gabion wall will require approximately 0.1 acre of required ROW from Commonwealth property that is part of Lehigh Gorge State Park.

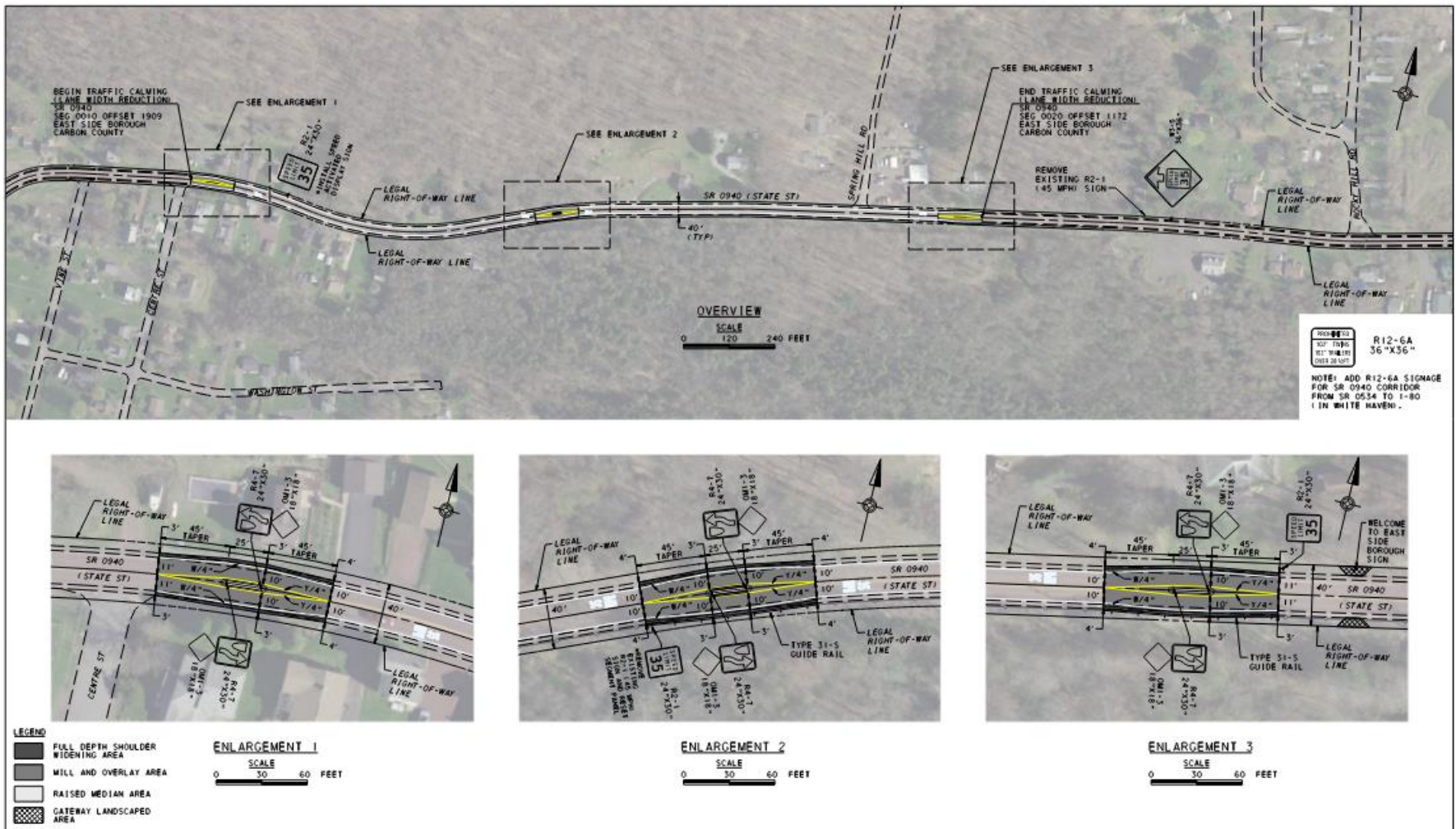
The I-80 Lehigh River Bridges Diversion Route Traffic Evaluation, March 2022, is included in the project technical files and is incorporated by reference to this EA.

In summary, as a result of the addition of tolling per the Major Bridge P3 Initiative, the I-80 Lehigh River Bridges Project encompasses the bridge replacements, the tolling facility and associated infrastructure, and the diversion route improvements, as identified in the sections above.



I-80 SECTION 08B, LEHIGH RIVER BRIDGES REPLACEMENT PROJECT  
 PROPOSED TRAFFIC IMPROVEMENTS



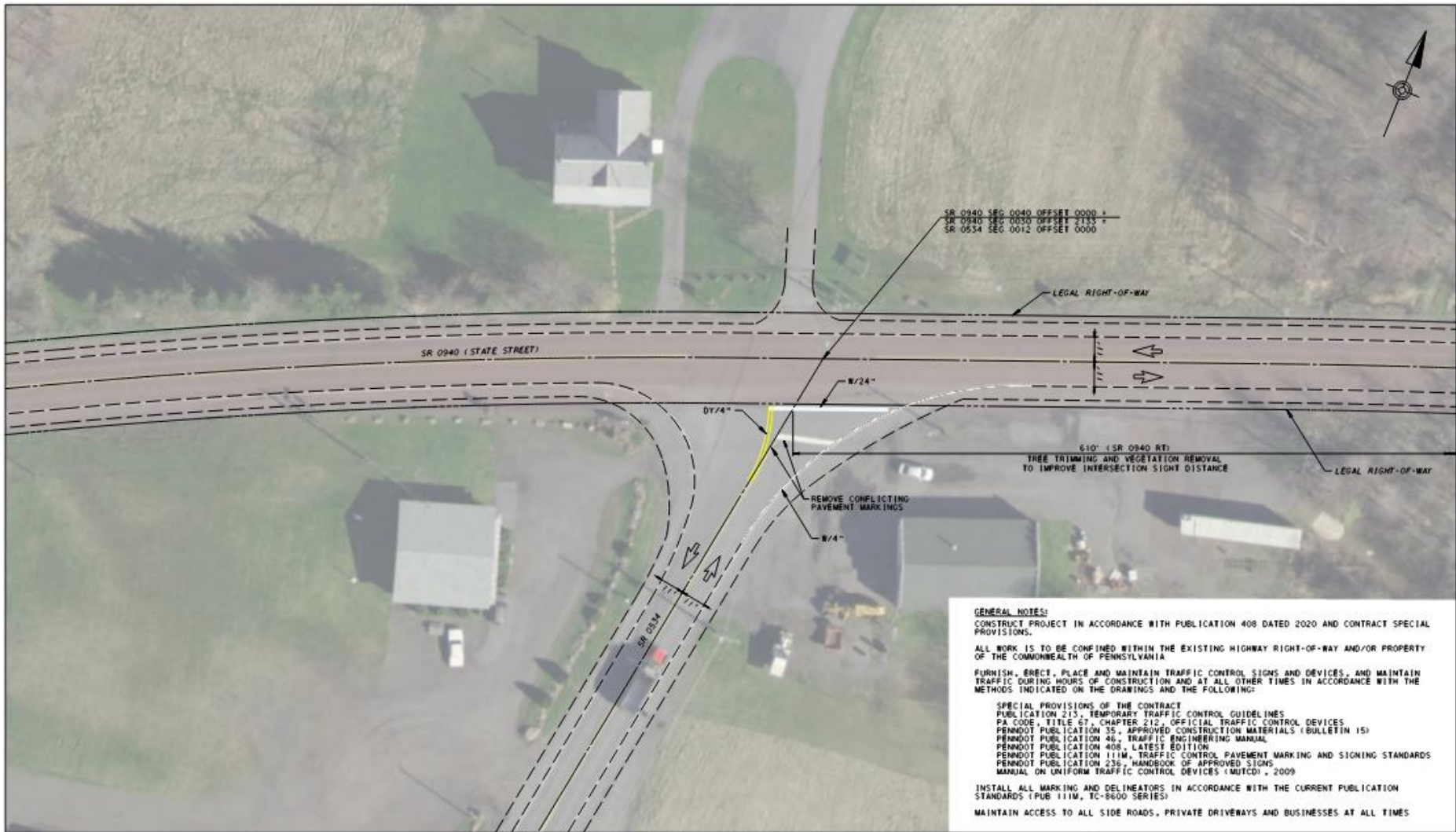


**PROPOSED LANE WIDTH REDUCTIONS  
SR 0940 (STATE STREET)  
EAST SIDE BOROUGH, CARBON COUNTY**

CONCEPTUAL PLAN  
WHITE HAVEN BRIDGE PACKAGE

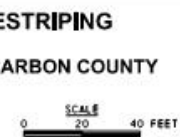
DATE: FEBRUARY 2022  
FIGURE: FIGURE (SHEET 2)

Figure 3 – SR 940 Traffic Calming in East Side Borough



**PROPOSED INTERSECTION RESTRIPIING**  
**SR 0534 & SR 0940**  
**EAST SIDE BORO & KIDDER TWP, CARBON COUNTY**

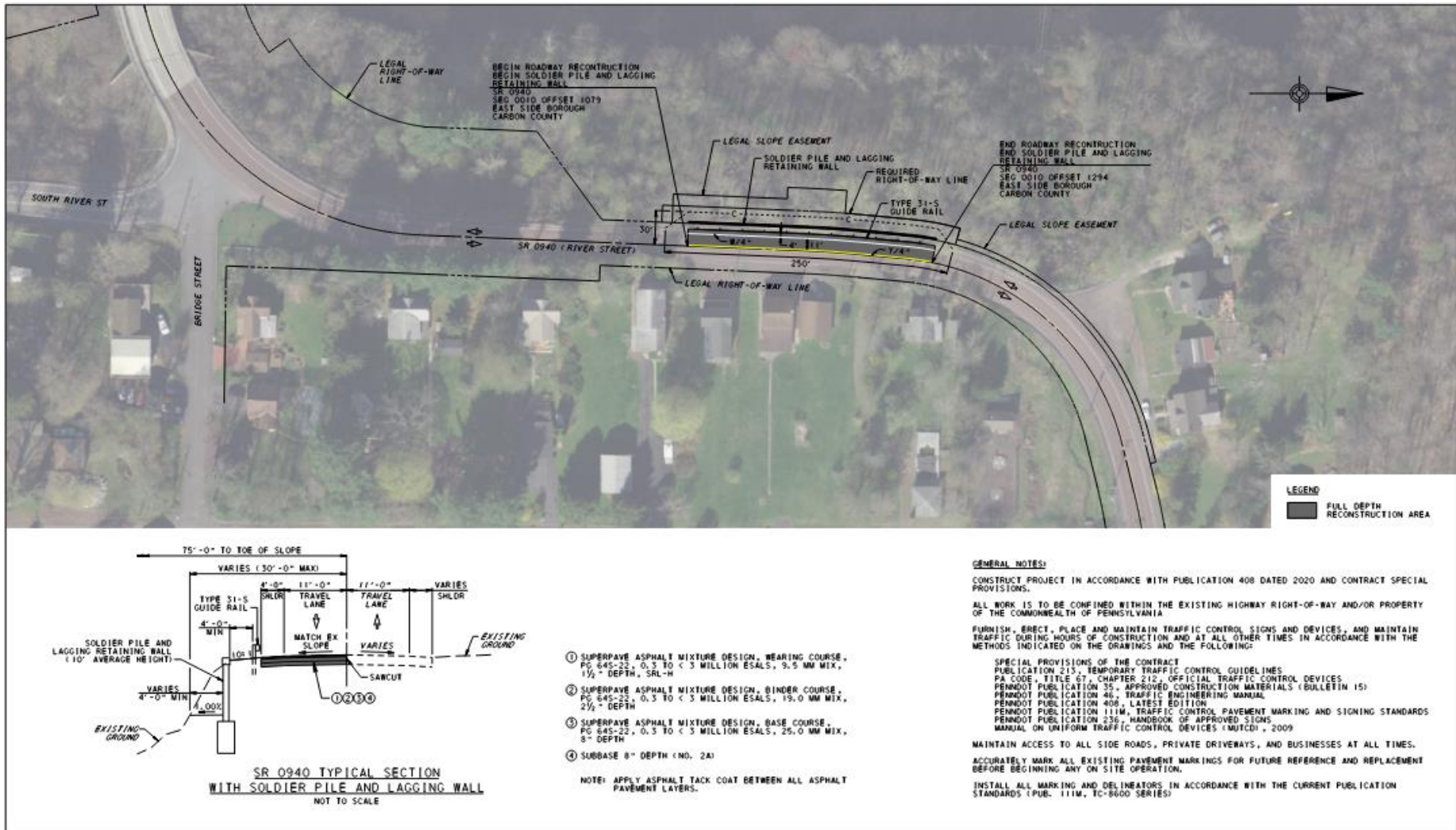
CONCEPTUAL PLAN  
 WHITE HAVEN BRIDGE PACKAGE



DATE  
 DECEMBER 2021

FIGURE  
 FIGURE

Figure 4 – SR 534 & SR940 Intersection Pavement Restriping



**PROPOSED SOLDIER PILE & LAGGING WALL  
 SR 0940 (RIVER STREET)  
 EAST SIDE BOROUGH, CARBON COUNTY**

CONCEPTUAL PLAN  
 WHITE HAVEN BRIDGE PACKAGE



DATE  
 FEBRUARY 2022

FIGURE  
 FIGURE

Figure 5 – SR 940 Replace Gabion Wall

### 3.3 Impact Summary Table

**Table 3  
Impact Summary Table**

<b>Environmental Resource Category</b>	<b>No-Build Alternative<sup>1</sup></b>	<b>Proposed Action</b>	<b>Proposed Mitigation Summary</b>
<b>Aquatic Resources</b>			
<b>Streams, Rivers, &amp; Watercourses</b>	No Impact	Streams: HQ-CWF, MF; stocked trout and wild trout  79 linear feet permanent impact 578 linear feet temporary impact	Stream mitigation details will be determined in final design through consultation with permitting agencies, in accordance with current regulations and practices, and incorporated into the waterway permit application.
<b>Wild &amp; Scenic Rivers and Streams</b>	No Impact	Pennsylvania Scenic River  - 230 square feet permanent impact for piers  - 500 linear feet (1.5 ac) temporary impact for causeways	-Vegetative buffers will be provided to the extent practicable. -Piers will not be allowed in the middle portion of the river. -Steel girders, if used on the new bridge, will be weathering steel or painted a “natural” color to blend with the setting. -Where possible, riprap will be covered with soil and vegetation. -Scenic river signs will be placed at the leading ends of each bridge. Signs will also be placed on the bridge and/or piers for water trail users
<b>Navigable Waterways</b>	No Impact	US Coast Guard Navigable, PFBC Water Trail, Recreational Boating Waterway  - 230 square feet permanent impact for piers	Aids to Navigation (ATON) Plan to be implemented during construction; United States Coast Guard (USCG) permit not required

<b>Environmental Resource Category</b>	<b>No-Build Alternative<sup>1</sup></b>	<b>Proposed Action</b>	<b>Proposed Mitigation Summary</b>
		<p>- 500 linear feet (1.5 ac) temporary impact for causeways</p> <p>The existing boat launch directly under the bridges will be closed during construction; however, three additional boat launches are being constructed by DCNR south of the project area and will remain open during I-80 bridge construction.</p>	
<b>Groundwater</b>	Not Present	Not Present	None
<b>Wetlands</b>	No Impact	Wetlands: 0.11 acre permanent impact	<p>Through consultation with permitting agencies, mitigation details will be determined in Final Design and incorporated into the waterway permit application.</p> <p>Protective fencing will be placed to prohibit contractor access in Wetland 2.</p>
<b>Floodplains</b>	No Impact	No significant floodplain encroachment would occur	None
<b>Soil Erosion and Sedimentation</b>	No Impact	<p>E&amp;S Control Plan and Post Construction Stormwater Management Plan will be prepared</p> <p>NPDES permit application will need to be submitted prior to construction</p>	<p>- Best Management Practices (BMPs) will be defined and implemented as a component of the erosion and sedimentation plan and waterway encroachment permit.</p> <p>- The E&amp;S Control Plan will be reviewed by the Pennsylvania Department of Environmental Protection (PADEP) and</p>

Environmental Resource Category	No-Build Alternative <sup>1</sup>	Proposed Action	Proposed Mitigation Summary
			<p>coordination will be conducted to ensure the selected BMPs are adequate for the project.</p> <ul style="list-style-type: none"> <li>- The approved E&amp;S Control Plan will be implemented prior to any earth disturbance, during construction.</li> <li>- Installed BMPs will be inspected and maintained throughout the duration of construction.</li> <li>- All areas of earth disturbance will be stabilized immediately following completion of earthwork.</li> <li>- Post Construction Stormwater Controls (PCSMs) will be evaluated in final design and included in the NPDES permit application, if required.</li> </ul>
<b>Land Use</b>			
<b>Agricultural Resources</b>	No Impact	<p>No active agricultural land</p> <p>Impacts to farmland soils are exempt from FPPA provisions</p>	None
<b>Vegetation</b>	No Impact	<p>Forested land, riparian vegetation and roadside vegetation are present and will be impacted</p>	<p>In accordance with PennDOT's invasive species guidance (Publication 756, 2014), care will be taken not to transplant roots or seeds of noted invasive, non-native plants during earth moving operations. Re-vegetation of impacted areas will be implemented through the E&amp;S plan. Prior to completion of construction, all remaining areas of earth disturbance will</p>



<b>Environmental Resource Category</b>	<b>No-Build Alternative<sup>1</sup></b>	<b>Proposed Action</b>	<b>Proposed Mitigation Summary</b>
			be restored by re-seeding with standard PennDOT seed formulas. These seed formulas may contain native plant species; but per Executive Order 13112, will avoid those plant species that are listed on the Noxious Weed Control List.
<b>Geologic Resources</b>	No Impact	Lehigh Gorge is an Outstanding Scenic Geological Feature of Pennsylvania, no impact	None
<b>Parks and Recreation Facilities</b>	No Impact	Lehigh Gorge State Park – Section 6(f): approximately 6 acres <ul style="list-style-type: none"> <li>- 0.1 acre of ROW</li> <li>- 0.4 acre of Proposed Slope Easement</li> <li>- 0.5 acre Proposed Aerial Easements</li> <li>-4.9 acre of TCE within LGSP</li> </ul>	To comply with Section 6(f) of the Land and Water Conservation Fund, approximately 6 acres of replacement lands will be provided.
<b>State Forest and Gamelands</b>	Not Present	Not Present	None
<b>Wilderness, Natural, &amp; Wild Areas</b>	Not Present	Not Present	None
<b>Hazardous or Residual Waste Sites</b>	No Impact	Phase I has been completed and identified 2 potential sites	Phase II and Phase III investigations are ongoing, and recommendations will be incorporated into construction contract, as appropriate.
<b>Wildlife</b>			

Environmental Resource Category	No-Build Alternative <sup>1</sup>	Proposed Action	Proposed Mitigation Summary
Wildlife Refuges & Critical Habitat	Not Present	Not Present	None
Threatened & Endangered Species	No Impact	No Impact	None
Cultural Resources			
Archaeological Resources	Not Present	Not Present	None
Historic Resources	No Historic Properties Affected	No Historic Properties Affected	None
Section 4(f) Resources	No Impact	<p><u>Lehigh Gorge State Park – De Minimis Use</u></p> <p>- 0.1 acre of required ROW - 0.4 acre of Proposed Slope Easement</p> <p><u>Lehigh River Water Trail / PA Scenic River – De Minimis Use</u></p> <p>Closure of one of four boat launches during construction</p>	<p>Mitigation measures for Lehigh Gorge State Park will include:</p> <p>- Access to and use of the Lehigh Gorge Trail will be maintained to the extent possible during construction:</p> <ul style="list-style-type: none"> <li>• <i>April through October: Overnight closure of the trail. Short-term closure of the trail during daylight hours Monday through Friday.</i></li> <li>• <i>November through March: Long-term closure of the trail for certain construction activities.</i></li> <li>• <i>Advance notice of trail closures will be provided.</i></li> <li>• <i>The contractor may temporarily realign the trail through the project site. A 10' wide trail will be maintained except as needed for closures.</i></li> </ul>

Environmental Resource Category	No-Build Alternative <sup>1</sup>	Proposed Action	Proposed Mitigation Summary
			<p>- If necessary due to pier locations, reconfiguration of the driveway, parking, or boat launches will be coordinated with DCNR.</p> <p>- An ATON Plan will be implemented and will include advance signing. Signs will be placed at boat launches upstream of the project. Additional advanced notification will be provided.</p> <p>- The contractor will restore disturbed areas, and full access to the park amenities will be provided upon completion. A planting plan will be prepared during Final Design.</p> <p>- PennDOT will coordinate with DCNR and White Haven Borough to provide wayfinding signs.</p> <p>Mitigation measures for the Lehigh River Water Trail / PA Scenic River will include:</p> <p>- An ATON Plan will be implemented and will include advance signing. Signs will be placed at boat launches upstream of the project. Additional advanced notification to PFBC, DCNR and the public will be provided.</p> <ul style="list-style-type: none"> <li>• <i>Temporary closures of boat traffic will be allowed for certain activities.</i></li> </ul>

Environmental Resource Category	No-Build Alternative <sup>1</sup>	Proposed Action	Proposed Mitigation Summary
Air Quality and Noise			<ul style="list-style-type: none"> <li>• <i>If necessary due to pier locations, reconfiguration of the driveway, parking, or boat launches will be coordinated with DCNR.</i></li> <li>• <i>The contractor will restore disturbed areas.</i></li> </ul> <p>To mitigate for the PA Scenic Rivers designation, PennDOT has agreed to incorporate the following actions:</p> <ul style="list-style-type: none"> <li>• <i>Vegetative buffers for construction staging areas will be provided to the extent practicable.</i></li> <li>• <i>Piers will not be allowed in the middle portion of the river.</i></li> <li>• <i>Steel girders, if used on the new bridge, will be weathering steel or painted a “natural” color to blend with the setting.</i></li> <li>• <i>Where possible, riprap will be covered with soil and vegetation.</i></li> <li>• <i>Scenic river signs will be placed at the leading ends of each bridge. Signs will be placed on the bridge and/or piers for water trail users.</i></li> </ul>
	Air Quality	No Impact	Exempt; no impact
Noise	No Impact	Type III Project; noise analysis not required	None

<b>Environmental Resource Category</b>	<b>No-Build Alternative<sup>1</sup></b>	<b>Proposed Action</b>	<b>Proposed Mitigation Summary</b>
<b>Socioeconomic Areas</b>			
<b>Regional &amp; Community Growth</b>	No Impact	No Impact	None
<b>Public Facilities &amp; Services</b>	No Impact	<p>Positive Impacts:</p> <p>Access for public facilities and services will be improved due to design improvements resulting from the project.</p>	<p>Proposed traffic improvements, including traffic signal, sidewalk, and crosswalk improvements are intended to enhance safety and mobility of bicyclists and pedestrians.</p> <p>Emergency services vehicles will be exempt from paying toll. Emergency responders traveling to an incident in their own vehicles will be reimbursed for tolling.</p>
<b>Community Cohesion</b>	No Impact	No impact	No Impact
<b>ROW Acquisitions</b>	No Impact	<p>9 parcels impacted:</p> <ul style="list-style-type: none"> <li>- 2 parcels totally taken;</li> <li>- 1 parcel with an aerial easement;</li> <li>- 1 parcel with partial take (required ROW);</li> <li>- 3 parcels with only TCEs;</li> <li>- 1 parcel with an aerial easement, permanent slope easement and TCEs; and</li> <li>- 1 parcel with both partial takes and TCEs.</li> </ul>	<p>Property acquisitions will be conducted in accordance with the Uniform Relocation Assistance and Real Property Acquisitions Policies Act of 1970, as amended; Title VI of the Civil Rights Act of 1964; and the Pennsylvania Eminent Domain Code of 1964.</p> <ul style="list-style-type: none"> <li>• While no residential relocations are anticipated, any individual or family displaced by the project would be offered the full extent of benefits and payments.</li> </ul>

<b>Environmental Resource Category</b>	<b>No-Build Alternative<sup>1</sup></b>	<b>Proposed Action</b>	<b>Proposed Mitigation Summary</b>
		No commercial or residential displacements are anticipated.	<ul style="list-style-type: none"> <li>Provisions would be made to ensure that any person with a disability who is displaced is offered replacement housing that meets any special needs. Based on current design plans, no displacements are anticipated.</li> </ul>
<b>Displacements</b>	No Impact	No Impact	None
<b>Aesthetics</b>	No Impact	No impact	None
<b>Energy</b>	Higher energy usage	Reduced energy usage	None
<b>Cumulative Impacts</b>	No Impact	No significant cumulative effects	None
<b>Environmental Justice</b>	No Impact	No disproportionately high and adverse effects on low-income or minority populations have been identified.	PennDOT will implement toll-free bridge access to low-income persons and reassess program 5 years after completion of project.

**Footnote:**

<sup>1</sup>While the No-Build Alternative would not directly affect resources, should the bridge deteriorate to the point where it would have to be weight-posted, closed, or should it experience a partial collapse, there would be impacts to the resources below the bridge. A full or partial closure would have a profound effect on commerce reliant on I-80 and would detour vehicles onto the toll diversion route. With the No-Build Alternative, the traffic improvements proposed to enhance safety and mobility along the toll diversion route would not be completed.

## 4.0 AFFECTED ENVIRONMENT AND ENVIRONMENTAL CONSEQUENCES

### 4.1 Aquatic Resources

	PRESENCE	IMPACTS
<b>STREAMS, RIVERS &amp; WATERCOURSES</b>	<input type="radio"/> Not Present <input checked="" type="radio"/> Present	
Intermittent (streams only)	<input checked="" type="radio"/> Not Present <input type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes
Perennial	<input type="radio"/> Not Present <input checked="" type="radio"/> Present	<input type="radio"/> No <input checked="" type="radio"/> Yes
Wild trout streams	<input type="radio"/> Not Present <input checked="" type="radio"/> Present	<input type="radio"/> No <input checked="" type="radio"/> Yes
Stocked trout streams	<input type="radio"/> Not Present <input checked="" type="radio"/> Present	<input type="radio"/> No <input checked="" type="radio"/> Yes

#### Identify all streams and their classifications per Chapter 93 of 25 PA Code (e.g. CWF, WWF, HQ, EV)

Field investigations conducted November 12, 2018 to November 15, 2018, December 19, 2018 to December 20, 2018, January 16, 2019, March 13, 2019, and November 20, 2020 identified ten stream channels totaling 4.946 acres (4,665 linear feet) within the project study area. These streams included the Lehigh River, Linesville Creek, six Unnamed Tributaries (UNTs) to Lehigh River, and two UNTs to Linesville Creek.

The Lehigh River Basin, Tobyhanna Creek to Buck Mountain Creek, is listed in Pennsylvania Code Title 25, Chapter 93 Water Quality Standards, as High Quality-Cold Water Fishes, Migratory Fishes (HQ-CWF, MF). All tributaries within this section are within the basin and, therefore, also HQ-CWF, MF.

**Linear feet of Streams permanently impacted:** 79

#### Describe Any Permanent Impacts

Approximately 79 linear feet of permanent impacts are anticipated due to new pier locations and proposed embankment along the Lehigh River and extension of a culvert carrying a UNT to Lehigh River.

#### Describe Any Temporary Impacts

Approximately 578 linear feet of temporary impacts are anticipated due to causeways in the Lehigh River needed to construct the bridges and along one UNT to Lehigh River for placement of temporary bypass measures.

#### Supporting documentation for Chapter 4.1 includes:

- [I-80 Lehigh River Bridges Final Wetland ID and Delineation Report \(February 2021, revised February 2022\)](#)
- [I-80 Lehigh River Bridges H&H Study \(November 2021\)](#)

**Is mitigation incorporated?**  No  Yes

#### Mitigation Remarks

Stream mitigation details will be determined in final design through consultation with permitting agencies, in accordance with current regulations and practices, and incorporated into the waterway permit application.

The Lehigh River is listed as a natural reproducing trout and stocked trout stream and Linesville Creek is listed as a natural reproducing trout stream. Construction work within the streams will be prohibited from February 15 to June 1 (stocked trout) and October 1 to December 31 (wild trout).

#### Remarks

Based on Wetland and Watercourses Presence/Absence Survey, no additional aquatic resource impacts were identified that are attributable to the design footprint of the toll facility and proposed traffic improvements to the diversion routes. The Wetland and Watercourses Presence/Absence Survey is appended to the Wetland ID and Delineation Report (February 2021, revised February 2022), which is included in the project technical files.

<b>FEDERAL WILD &amp; SCENIC RIVERS &amp; STREAMS</b>	<b>PRESENCE</b>	<b>IMPACTS</b>
	<input checked="" type="radio"/> Not Present <input type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes

**Remarks**

Review of the USGS Quadrangle and Federal Wild and Scenic Rivers System website has confirmed there are no Federal Wild and Scenic Rivers and Streams within the project area.

<b>STATE SCENIC RIVERS &amp; STREAMS</b>	<b>PRESENCE</b>	<b>IMPACTS</b>
	<input type="radio"/> Not Present <input checked="" type="radio"/> Present	<input type="radio"/> No <input checked="" type="radio"/> Yes

**Documentation**

DCNR Coordination Letter

**Describe Any Permanent and Temporary Impacts**

The stretch of the Lehigh River within the project study area is designated as a Pennsylvania Scenic River by Pennsylvania Department of Conservation and Natural Resources (DCNR), with the scenic designation from the Francis E. Walter Dam north of the project to Jim Thorpe to the south. This designation encompasses a 32-mile upper section of the Lehigh River and is intended to help preserve the primitive qualities, as well as the natural aesthetic values, of our rivers and adjacent land environment. Designated rivers shall be free-flowing and capable of supporting both aquatic life and water-based recreation.

The proposed replacement I-80 Lehigh River Bridges would place two piers within the Lehigh River delineated ordinary high water mark. One pier of each bridge is anticipated to be placed along the western edge of and extending into the river (approximately 230 square feet total within the river). The existing bridges have 4 piers in the river (approximately 600 square feet total). The final determination of the pier locations will be dependent on the contractor’s selection of bridge structure type.

Approximately 500 linear feet (1.5 acres) of temporary impact to the Lehigh River is anticipated due to temporary causeways needed to construct the bridges. Temporary causeways will extend from both riverbanks but will not extend completely across the river allowing for safe passage of recreational boat traffic through the project construction site. A series of causeways is proposed for staged construction lasting approximately four years.

**Is mitigation incorporated?**       No  Yes

**Describe Mitigation**

Through coordination with PA DCNR, PennDOT agreed to incorporate the following mitigation measures into the project. The DCNR coordination letter is provided in Appendix E – Agency Coordination.

- Vegetative buffers will be provided to the extent practicable.
- Piers will not be allowed in the middle portion of the river.
- Steel girders, if used on the new bridge, will be weathering steel or painted a “natural” color to blend with the setting.
- Where possible, riprap will be covered with soil and vegetation.
- Scenic river signs will be placed at the leading ends of each bridge. Signs will also be placed on the bridge and/or piers for water trail users.



**PRESENCE**

**IMPACTS**

**NAVIGABLE WATERWAYS**

Coast Guard Navigable	<input type="radio"/> Not Present <input checked="" type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes
PFBC Water Trail	<input type="radio"/> Not Present <input checked="" type="radio"/> Present	<input type="radio"/> No <input checked="" type="radio"/> Yes
Recreational Boating Waterway	<input type="radio"/> Not Present <input checked="" type="radio"/> Present	<input type="radio"/> No <input checked="" type="radio"/> Yes

**Documentation**

- PFBC Aids to Navigation Plan
- Coast Guard Coordination

**Describe Any Permanent and Temporary Impacts**

The stretch of the Lehigh River within the project study area is a water trail designated by PFBC. Water based recreation, including recreational boating, is conducted along the 72-mile-long Lehigh River Water Trail, which emphasizes low-impact use and promotes stewardship of water resources. The 41-mile Northern section of the Lehigh River Water Trail begins at the White Haven boat access on the west shore of the Lehigh River underneath the I-80 bridges.

Approximately 500 linear feet (1.5 acres) of temporary impact to the Lehigh River is anticipated due to temporary causeways needed to construct the bridges. Temporary causeways will extend from both riverbanks but will not extend completely across the river allowing for safe passage of recreational boat traffic through the project construction site. A series of causeways is proposed for staged construction lasting approximately four years.

The existing boat launch directly under the bridges will be closed during construction; however, three additional boat launches are being constructed by DCNR south of the project area and will remain open during I-80 bridge construction.

**Is mitigation incorporated?**      No  Yes

**Describe Mitigation**

A preliminary ATON Plan was prepared and submitted to PFBC for review. PFBC approved the ATON Plan on August 16, 2021. The ATON Plan and approval letter are in the project technical files. The ATON Plan shows the proposed causeways and staging, and sign locations in the vicinity of the bridges. Signs will also be placed at boat launches upstream of the project. Additional advanced notification to PFBC, DCNR and the public will be provided. The P3 Development Entity will be responsible for implementation of the ATON plan during construction.

Detailed information regarding impacts and mitigation for the Lehigh River Water Trail is provided in the attached Section 4(f) Evaluation prepared for the Lehigh River Water Trail / PA Scenic River (see Chapter 4.5 of this EA).

**Remarks**

Based on the Navigable Waters of the US Section 10, the Lehigh River is navigable 72 miles from the confluence with the Delaware River to the downstream side of the PA Route 940 bridge, which includes the I-80 Lehigh Bridges project study area. PennDOT coordinated with the USCG, which determined that a Coast Guard bridge permit will not be required for the proposed bridge replacements over the Lehigh River. The project was placed in the Coast Guard Authorization Act of 1982 exemption category, and this determination is valid for five years from the date of the USCG letter, November 18, 2021. If the construction project does not commence within this

time, USCG must be contacted for reaffirmation of this determination. The USCG Coordination letter is projected in Appendix E – Agency Coordination.

**PRESENCE**

**OTHER SURFACE WATERS**

Not Present  Present

**Remarks**

Based on secondary source aerial views and field investigations, no other surface waters are present in the project area.

**PRESENCE**

**GROUNDWATER RESOURCES**

Not Present  Present

**Remarks**

There are no recorded wells in the project study area. According to PADEP's eMapPA online tool that uses PA Groundwater Information System data, there are 4 wells located within 0.1 miles to 0.3 miles of the project bridge.

**PRESENCE**

**IMPACTS**

**WETLANDS**

Open Water

Not Present  Present

No  Yes

Vegetated

Not Present  Present

Emergent

Not Present  Present

No  Yes

Scrub Shrub

Not Present  Present

No  Yes

Forested

Not Present  Present

No  Yes

Exceptional Value

Not Present  Present

No  Yes

**Documentation**

- Data Forms
- Wetland Identification and Delineation Report
- Conceptual Mitigation Plan
- 404 (b)(1) Alternative Analysis
- Jurisdictional Determination Functional
- Assessment Analysis

**Methodology**

Field investigations conducted November 12, 2018 to November 15, 2018, December 19, 2018 to December 20, 2018, January 16, 2019, March 13, 2019, and November 20, 2020 identified eight wetland areas, totaling 0.914 acres, within the project study area. Wetlands were classified as Palustrine Forested (PFO) and Palustrine Scrub-Shrub (PSS) wetlands.

Field investigations were conducted in accordance with the methodology described in the US Army Corps of Engineers (USACE Corp of Engineers Wetland Delineation Manual (Technical Report Y-81-1) and the USACE Regional Supplement to the Corps of Engineers Wetland Delineation Manual: Eastern Mountains and Piedmont Region Version 2.0), PADEP, Chapter 105 regulations, Section 404 of the Federal Clean Water Act, and the Pennsylvania Department of Transportation (PennDOT) Publication No. 325.

The wetlands within the project study area are associated with a naturally reproducing trout water and PA

Scenic River, and therefore are designated as Exceptional Value (EV).

**Number of Wetlands permanently impacted:** 3

**Acreage of Wetlands permanently impacted:** 0.11

**Describe Any Permanent Impacts**

Approximately 0.11 acre of permanent impacts to wetlands are anticipated: 0.075 acre Palustrine Forested wetlands (Wetland 1 and Wetland 7) and 0.035 Palustrine Scrub-Shrub (Wetland 6). The wetlands are located in the Lehigh River floodplain and will be impacted due to crane placement and contractor access necessary to construct the bridges.

**Describe Any Temporary Impacts**

Temporary impacts to wetlands are not anticipated.

**Is mitigation incorporated?**  No  Yes

**Mitigation Remarks**

Wetland mitigation details will be determined in final design through consultation with permitting agencies, in accordance with current regulations and practices, and incorporated into the waterway permit application. Protective fencing will be placed to prohibit contractor access in Wetland 2.

**Executive Order 11990 Compliance**

**Compliance requires the determination that there is no practicable alternative to the proposed construction in wetlands and the proposed action includes all practicable measures to minimize harm to wetlands which may result from such use.**

**Options/design modifications were investigated to avoid impacts to wetlands:**  Yes  No  N/A

**There are no practicable alternatives to construction within the wetlands:**  Yes  No  N/A

**Alternative chosen (proposed project) includes all practicable measures to minimize harm to wetlands:**  
 Yes  No  N/A

**Remarks**

All practicable measures to avoid and minimize impact to wetlands were taken as part of project design activities.

Based on Wetland and Watercourses Presence/Absence Survey, no additional wetland impacts were identified that are attributable to the design footprint of the toll facility and proposed traffic improvements to the diversion routes. The Wetland and Watercourses Presence/Absence Survey is appended to the Wetland ID and Delineation Report (February 2021, revised February 2022), which is included in the project technical files.

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	<b>PRESENCE</b>	<b>IMPACTS</b>
<b>COASTAL ZONE</b>	<input checked="" type="radio"/> Not Present <input type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes

**Remarks**

There are no coastal zones located within the project area.

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	<b>PRESENCE</b>	<b>IMPACTS</b>
<b>FLOODPLAINS</b>	<input type="radio"/> Not Present <input checked="" type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes

No significant floodplain encroachment would occur.

**Describe Any Permanent and Temporary Impacts**

The Lehigh River is the borderline between Carbon and Luzerne counties. Both counties have FEMA studies for the Lehigh River. The project is located in a detailed FEMA study area for Luzerne County. The I-80 bridges over Lehigh River are in an area studied by detailed methods (Zone AE with floodway area) with published peak flows and base flood (100-year) elevations. For the Carbon County FEMA study, the I-80 bridges are in an area studied by approximate methods (Zone A area). The UNT to Lehigh River in Carbon County is in FEMA Zone X, which is an area that was not studied by FEMA.

Based on preliminary Hydrology and Hydraulics (H&H) analysis, the proposed bridge replacement will not increase the risk of flooding on the Lehigh River upstream or downstream of the proposed bridges for the FEMA 100-year flood event. The proposed culvert extension under I-80 also will not increase the risk of flooding upstream or downstream of the culvert for the 100-year event.

The project will have no significant floodplain encroachment, as defined in 23 CFR Part 650, Subpart A, Section 650.105(q), since the project will not: 1. Have a significant potential for interruption or termination of a transportation facility which is needed for emergency vehicles or provides a community's only evacuation route, 2. Have a significant risk, 3. Have a significant adverse impact on natural and beneficial flood plain values.

Is mitigation incorporated?       No    Yes

**SOIL EROSION & SEDIMENTATION**

**Are there activities that could cause erosion or sedimentation and would require E&S Controls?**

Yes    No    N/A

**Documentation**

- Coordination w/County Conservation
- District E&S Control Plan
- NPDES Stormwater Construction Permit

Is mitigation incorporated?       No    Yes

**Remarks**

Preliminary E&S Control Plan and PCSM Plan have been prepared for the project and coordination with Luzerne and Carbon County Conservation Districts, PADEP, and USACE has been initiated. Final plans will be prepared during Final Design and will be incorporated into the construction contract. The P3 Development Entity will prepare and submit the NPDES permit application and secure necessary permit authorizations prior to construction.

**Mitigation:**

- BMPs will be defined and implemented as a component of the erosion and sedimentation plan and waterway encroachment permit.
- The E&S Control Plan will be reviewed by the PADEP and coordination will be conducted to ensure the selected BMPs are adequate for the project.
- The approved E&S Control Plan will be implemented prior to any earth disturbance, during construction.
- Installed BMPs will be inspected and maintained throughout the duration of construction.
- All areas of earth disturbance will be stabilized immediately following completion of earthwork.
- PCSMs will be evaluated in final design and included in the NPDES permit application, if required.

## 4.2 Land

	<b>PRESENCE</b>	<b>IMPACTS</b>
<b>AGRICULTURAL RESOURCES</b>	<input type="radio"/> Not Present <input checked="" type="radio"/> Present	
Productive Agricultural Land	<input checked="" type="radio"/> Not Present <input type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes
Agricultural Security Areas	<input checked="" type="radio"/> Not Present <input type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes
Prime Agricultural Land	<input checked="" type="radio"/> Not Present <input type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes
Agricultural Conservation Easement	<input checked="" type="radio"/> Not Present <input type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes
Farmland Enrolled in Preferential Tax Assessments	<input checked="" type="radio"/> Not Present <input type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes
Agricultural Zoning	<input checked="" type="radio"/> Not Present <input type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes
Soil Capability Classes I, II, III, IV	<input type="radio"/> Not Present <input checked="" type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes
Prime or Unique Soil	<input type="radio"/> Not Present <input checked="" type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes
Statewide or Locally Important Soils	<input type="radio"/> Not Present <input checked="" type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes

### Describe Any Permanent and Temporary Impacts

No impacts to agricultural resources are anticipated.

Is mitigation incorporated?  No  Yes

### Remarks

Examination of USDA NRCS web soil survey mapping for the project area identified one Prime Farmland soil type and three Farmland of Statewide Importance soil types within the project study area. These soil classifications are protected under the Farmland Protection Policy Act (FPPA). However, bridge replacements are exempt from FPPA provisions as per Farmland Protection Policy Manual, 523.11, C. Activities Not Subject to Provisions of FPPA, (10) Restoration, maintenance, renovation, or replacement of existing structures prior to the time of Federal Assistance.

Site visits have confirmed that no active agricultural land is present within the project study area; therefore, there will be no impact to Agricultural Land Preservation Policy (ALPP) Prime Agricultural Land. This project is in conformance with 4 Pa Code Chapter 7, Section 7.301 et seq., ALPP. Additionally, this project is an upgrade of existing transportation facility and is exempt from Acts 43 and 100.

	<b>PRESENCE</b>	<b>IMPACTS</b>
<b>VEGETATION</b>	<input type="radio"/> Not Present <input checked="" type="radio"/> Present	
Landscaped	<input checked="" type="radio"/> Not Present <input type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes
Agricultural	<input checked="" type="radio"/> Not Present <input type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes
Forest Land	<input type="radio"/> Not Present <input checked="" type="radio"/> Present	<input type="radio"/> No <input checked="" type="radio"/> Yes
Rangeland	<input checked="" type="radio"/> Not Present <input type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes
Other (describe in remarks)	<input type="radio"/> Not Present <input checked="" type="radio"/> Present	<input type="radio"/> No <input checked="" type="radio"/> Yes

### Describe Any Permanent and Temporary Impacts

Vegetation within the project corridor primarily consists of deciduous forest land, riparian vegetation, and roadside vegetation along I-80. Permanent and Temporary impacts will occur to the project corridor vegetation to construct project improvements. This includes roadside vegetation as well as land below and adjacent to the

I-80 bridges for crane placement and contractor access.

Invasive Non-Native Plants are Present

**Mitigation:**

Are measures being taken to minimize movement of invasive plant parts (roots, tubers, seeds)?  Yes  No

Will native plants be used in project landscaping or mitigation?  Yes  No

Other?  Yes  No

**Describe Mitigation**

In accordance with PennDOT’s invasive species guidance (Publication 756, 2014), care will be taken not to transplant roots or seeds of noted invasive, non-native plants during earth moving operations. Re-vegetation of impacted areas will be implemented through the E&S plan. Prior to completion of construction, all remaining areas of earth disturbance will be restored by re-seeding with standard PennDOT seed formulas. These seed formulas may contain native plant species; but per Executive Order 13112, will avoid those plant species that are listed on the Noxious Weed Control List.

	PRESENCE	IMPACTS
<b>GEOLOGIC RESOURCES</b>	<input type="radio"/> Not Present <input checked="" type="radio"/> Present	
DCNR Heritage Geology Site	<input type="radio"/> Not Present <input checked="" type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes
Other (describe in remarks)	<input checked="" type="radio"/> Not Present <input type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes

**Describe Any Permanent and Temporary Impacts**

None.

Is mitigation incorporated?  No  Yes

**Remarks**

Lehigh Gorge is an Outstanding Scenic Geological Feature of Pennsylvania. Coordination with PA DCNR has determined that the I-80 Lehigh River Bridges project will not impact the Lehigh Gorge geoheritage site (Appendix E).

	PRESENCE	IMPACTS
<b>PARKS &amp; RECREATION FACILITIES</b>	<input type="radio"/> Not Present <input checked="" type="radio"/> Present	
National	<input checked="" type="radio"/> Not Present <input type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes
State	<input type="radio"/> Not Present <input checked="" type="radio"/> Present	<input type="radio"/> No <input checked="" type="radio"/> Yes
Local	<input type="radio"/> Not Present <input checked="" type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes
Other (describe in remarks)	<input checked="" type="radio"/> Not Present <input type="radio"/> Present	<input checked="" type="radio"/> No <input type="radio"/> Yes

**Were any of the impacted properties acquired through the use of :**

- Land and Water Conservation Fund (Section 6(f))
- Project 70 Fund
- Project 500 Fund
- Recreational Improvement and Rehabilitation Act (RIRA)
- Growing Greener Bond Fund (GG2)
- Keystone Recreation, Park and Conservation Fund (Key 93)
- Environmental Stewardship Fund Act (ESF)

**Documentation**

- Coordination with DCNR as representative for NPS (LWCF)
- Coordination with DCNR and PA General Assembly (Project 70)
- Coordination with DCNR (Project 500, GG2, Key93, RIRA or ESF)

**Describe Any Permanent and Temporary Impacts**

Within the project area, Lehigh Gorge State Park (LGSP) extends on both the east and west sides of the Lehigh River on land owned by the Commonwealth of Pennsylvania and administered by PA Department of Conservation and Natural Resources (DCNR). According to DCNR’s website, the 6,107-acre park follows the Lehigh River from Francis E. Walter Dam north of I-80 to Jim Thorpe to the south. DCNR has confirmed that LWCF funds (42-00081) were used to purchase land for LGSP. The entire state park is afforded protection under Section 6(f). As such, any land necessary to be acquired for construction of the project would be considered a conversion from public outdoor recreation use to transportation use. Temporary non-conforming use of Section 6(f) protected land (i.e., TCEs) lasting longer than 180 days is considered a permanent conversion also requiring replacement of property according to the National Park Service’s policy. The temporary impacts for this project will be longer than 180 days. The National Park Service can approve such conversion only if it is found to be in accordance with the comprehensive statewide outdoor recreation plan and with the replacement of property of at least equal fair market value and of reasonably equivalent usefulness and location.

The project will result in the following impacts to LGSP:

Permanent Impacts:

- 0.1 acre of ROW for replacement of a gabion wall along SR 940 (there are no park amenities at this location);
- 0.4 acre of Proposed Slope Easement along the eastbound on-ramp at SR 940 due to shift to the south (there are no park amenities at this location);

Temporary Impacts:

- 0.5 acre Proposed Aerial Easements for new eastbound bridge due to shift to the south, which will also include temporary use for construction staging in excess of 180 days.
- 4.9 acre of TCE within LGSP for crane placement, causeway construction, and contractor access

The area of proposed aerial easements and TCEs will impact LGSP facilities. Temporary impacts to LGSP include closure of the Lehigh Gorge Trail during specified times and durations, driveway and parking facility closures, and closure of one boat launch under the I-80 bridges (three boat launches being constructed by DCNR south of the project area will remain open).

Approximately 6 acres of land will be replaced to satisfy Section 6(f). The concurrence letter from DCNR and NPS is provided in Appendix E.

**Is mitigation incorporated?**       No  Yes

**Mitigation:**

Approximately 6 acres of land will be replaced for permanent and temporary impacts within LGSP. LGSP is also protected under Section 4(f) of USDOT Act of 1966. Additional mitigation measures are proposed for Section 4(f) uses within LGSP (see Chapter 4.5 of this EA).

**Remarks**

Lehigh Park, owned by the Borough of White Haven, is adjacent to the I-80 westbound off-ramp at SR 940 and would be protected under Section 4(f). The proposed project will have no temporary or permanent impacts to Lehigh Park.

**PRESENCE**

**FOREST & GAMELANDS**

Not Present  Present

**Remarks**

A review of Google Maps, the PA Gazateer (DeLorme 2012), aerial imagery, PennDOT One Map (formerly MPMS IQ), and the results of the field reconnaissance did not identify any State Forests or State Gamelands within the project study area.

**PRESENCE**

**WILDERNESS, NATURAL & WILD AREAS**  Not Present  Present

**Remarks**

Review of USGS mapping, PADEP eMap, and site investigations confirmed there are no Federal and/or State Wilderness, Natural or Wild Areas within the project study area.

**PRESENCE**

**NATIONAL NATURAL LANDMARKS**  Not Present  Present

**IMPACTS**

No  Yes

**Remarks**

No National Natural Landmarks are present within project limits.

**PRESENCE**

**HAZARDOUS OR RESIDUAL WASTE SITES**  Not Present  Present

**IMPACTS**

No  Yes

**Documentation**

- Phase I
- Phase II
- Phase III
- Other
- No Documentation Required

**Supporting documentation for Chapter 4.2 includes:**

- [I-80 Lehigh River Bridges Phase I ESA report \(April 2021\)](#)
- [I-80 Lehigh River Bridges Expanded Phase I ESA \(November 2021\)](#)

**Describe Any Permanent and Temporary Impacts**

A Phase I Environmental Site Assessment (ESA) was conducted in accordance with PennDOT Publication 281, "Waste Site Evaluation Procedures for the Highway Development Process" to determine if hazardous, residual, or municipal waste sites exist within the study area. The Phase I ESA included site reconnaissance on November 19, 2020, environmental database review, historical data review, and personal interviews. Three potential sites were identified. The Phase I ESA findings and conclusions resulted in the following recommendations:

RECOMMENDATION 1 – ACTION REQUIRED

The abandoned municipal White Haven Landfill was identified as a Recognized Environmental Condition (REC). Conducting a Phase II geophysical survey in the area of proposed excavation that intercepts the historic landfill is recommended. The geophysical survey would be helpful to determine the location of bedrock and determine the



extent of landfill material if significant trash or debris exists. If the geophysical survey or geotechnical borings being performed for the project identify trash, municipal waste or inappropriate material, a Phase III investigation is recommended to understand the nature of the material and provide verification of its vertical and horizontal extents for the purpose of excavation on the project. Municipal waste found within excavation areas will need to be managed or disposed of properly during construction.

**RECOMMENDATION 2 – ACTION REQUIRED**

A Phase III investigation to characterize historic fill material in the areas of pier replacement along the Lehigh Gorge Trail/D&L Trail (former railroad) is recommended. The characterization data would be used to prepare a Soil Management Plan (SMP) that would be required to dispose of waste that cannot be managed on site.

**RECOMMENDATION 3 – ACTION REQUIRED**

Development of a SMP that will be provided in the bid specification is recommended. The SMP will provide the contractor with instruction and methods to identify unsuitable fill material from an environmental perspective and instruction on how to address impacted historic fill material that may be identified in the area of pier replacement.

A Phase I ESA was completed for the expanded PSA for the toll facility and documented in an addendum to the Phase I report for the bridge replacement project. Field reconnaissance was conducted May 21, 2021. Based on the Phase I ESA for the toll facility, one Activity and Use Limitation (AUL) was identified for a property located at 4700 SR 534 due to a diesel fuel spill on the property. An environmental covenant prohibits groundwater use on the property and requires a Soil Management Plan (SMP) be in place if soil is excavated in an area where soil was affected by the diesel spill. Preliminary toll facility design plans do not include excavations at this property. If a design change requires disturbance of soil at 4700 SR 534, a SMP should be developed. The SMP will provide the contractor with instruction and methods to identify and manage unsuitable fill material.

**Remarks**

Phase II and Phase III investigations are ongoing

**Is remediation/mitigation incorporated?**  No  Yes  Unknown at this time

**Describe Remediation/Mitigation**

Phase II and Phase III investigations are ongoing for two potential sites. Impacts, remediation, and mitigation will be determined upon completion of these investigations.

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**4.3 Wildlife**

**PRESENCE**

**WILDLIFE & HABITAT**

Not Present  Present

**Remarks**

The results of the field reconnaissance and review of the Pennsylvania Gazetteer (DeLorme 2015), US Fish and Wildlife Service and Nature Conservancy Map Portals did not identify any wildlife sanctuaries, wildlife refuges, unique or critical habitat, or wildlife preserves in the vicinity of the project study area.

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**THREATENED & ENDANGERED  
PLANTS & ANIMALS**

**PRESENCE**

- Not Present
- Present
- No Coordination Needed

**IMPACTS**

- No Potential Impacts
- Potential Impacts with Avoidance Measures
- Potential Impacts with Conservation Measures
- Potential Impacts

**Documentation**

- PNDI ER Receipt

**Agency Documentation**

- PFBC Correspondence
- PGC Correspondence
- DCNR Correspondence
- USFWS Correspondence

**Remarks**

The Pennsylvania Natural Diversity Inventory (PNDI) review was conducted for the bridge/roadway project study area, including the toll facility. The PNDI returned a potential impact with a species of special concern under the jurisdiction of PFBC. Further consultation with PFBC determined that given the nature of the proposed project, the immediate location or current status of the species occurrence, no adverse impacts are expected as a result of the project. A second PNDI review was conducted for the proposed diversion route improvement to replace a failing gabion wall along SR 940 and determined that there are no known impacts anticipated to threatened and endangered species and/or special concern species and resources at this location. No further review is required at this time. These determinations are valid for two years. The PNDI receipts and PA Fish & Boat Commission letter are provided in Appendix F.

- PNDI 739744, 10/13/2021. I-80 Lehigh River Bridges Project (including expanded PSA for toll facility)
- PNDI 753893, 3/3/2022. SR 940 Gabion Wall Replacement

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#### 4.4 Cultural Resources

Were Cultural Resource Professionals (CRPs) needed for project scoping?  Yes  No

CRP Scoping Field View Date: 03/29/18

CRP Architectural Historian in Attendance: Kristina Thompson (field viewed 5/10/18)

CRP Archaeologist in Attendance: Kevin Mock

Was a Project Early Notification / Scoping Results Form completed?  Yes  No

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Is the project exempted from review by the District Designee or CRP as per Appendix C of the Statewide Section 106 Programmatic Agreement?  Yes  No

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Is the project exempted from review by the District Designee or CRP as per Stipulation III of the Emergency Relief Projects Programmatic Agreement (2005)?  Yes  No

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	<u>PRESENCE</u>				<u>LEVEL OF EFFECTS</u>		
	Not Present	Potentially Eligible Resource Present	Eligible Resource Present	Listed Resource Present	No Historic Properties Affected	No Adverse Effect	Adverse Effect
CULTURAL RESOURCES	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<u>Archaeology</u>							
Pre-Contact:	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Contact Native American:	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Historic:	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<u>Above-Ground Historic Properties</u>							
Structure/Building:	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
District:	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

**Documentation**

For projects *not having a known adverse effect*, one from *each* column:

**Above-Ground Historic Properties**

- Above-Ground Historic Properties Field Assessment and Finding
- Above-Ground Historic Properties Finding Letter
- Section 106 (Above-Ground Historic Properties) Effect Concurrence Letter
- TE Project Field Assessment and Finding Checklist

**Archaeology**

- Archaeology Field Assessment and Finding
- Archaeology Finding Letter
- Section 106 (Archaeology) Effect Concurrence Letter
- TE Project Field Assessment and Finding Checklist
- Deferred Archaeological Testing Form
- Project Specific Programmatic Agreement

**Supplemental documentation should be completed as warranted:**

- Historic Structures Survey / Determination of Eligibility Report
- Phase Ia Archaeological Sensitivity Report
- Geomorphological Survey Report
- Archaeological Disturbance Report
- Archaeology Identification (Phase I) Report
- Archaeology Negative Survey Form
- Archaeology Evaluation (Phase II) Report
- Combined Archaeology Identification/Evaluation Report
- Determination of Effects Report
- (Bridge) Feasibility Report
- Other

**Describe Any Permanent and Temporary Impacts**

None.

Are mitigation and/or standard treatments required?  No  Yes

## Describe Mitigation/Standard Treatments

PennDOT tested the single area within the Limits of Disturbance (LOD) featuring intact soils. No archaeological sites were identified. The following two historic districts were located in the vicinity of the project study area: National Register eligible Lehigh Valley Railroad and the potentially eligible Central Railroad of New Jersey. The Lehigh Valley Railroad does not have any contributing resources within the Area of Potential Effect (APE), nor does the Central Railroad of New Jersey. No other historic properties are in the APE. PennDOT posted a finding of No Historic Properties Affected on PATH (8/16/21). Section 106 cultural resource documentation is located in Project Path at <https://path.penndot.gov/ProjectDetails.aspx?ProjectID=55546>.

After addition of diversion route traffic improvements to the project, a revised finding was posted to PATH 3/15/2022, with the same result of No Historic Properties Affected for above and below ground resources. Section 106 documentation is posted to PATH and in the project technical files.

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## 4.5 Section 4(f) Resources

	PRESENCE	USE
<b>SECTION 4(f) RESOURCES</b>	<input type="radio"/> Not Present <input checked="" type="radio"/> Present	<input type="radio"/> No <input checked="" type="radio"/> Yes

### Documentation

- Individual Section 4(f) Evaluation
- Programmatic Section 4(f) Evaluation
- Section 2002 Evaluation
- De Minimis Use/No Adverse Use Checklist
- Non-Applicability/No Use Checklist
- Temporary Use Checklist
- FHWA Coordination Documents

Will temporary easements during construction be necessary from Section 4(f) resources?  No  Yes

### Describe Any Permanent and Temporary Impacts

The project bridge spans the Lehigh River, Lehigh Gorge State Park, and the Lehigh Gorge Trail (a section of the Delaware and Lehigh National Heritage Corridor Trail). The portion of the Lehigh River within the project area is part of the PFBC designated Lehigh River Water Trail with boating access near the project bridges. The Lehigh River is also a state designated Scenic River.

Permanent and temporary impacts to Lehigh Gorge State Park (LGSP) and the Lehigh River Water Trail / PA Scenic River are described in detail in the Section 4(f) Evaluation documents provided in Appendix G. A separate *De Minimis Use/No Adverse Use* checklist is provided for LGSP and the Lehigh River Water Trail / PA Scenic River to satisfy Section 4(f) and Section 2002 of Pa Act 120.

- Lehigh Gorge State Park: The proposed bridge replacement project will acquire approximately 0.1 acre Required ROW, 0.4 acre of Proposed Slope Easement, 0.5 acre Proposed Aerial Easements, 4.9 acres of TCE within LGSP, and 0.3 acres of TCE within DCNR Easement. Proposed Slope Easement is necessary to replace the eastbound bridge on an alignment south of the existing bridge. Proposed Aerial Easement (which is not a Section 4(f) use) is needed for future access, maintenance, and construction. TCE is necessary to provide contractor access and staging. The 4.9 acres and 0.3 acres of temporary impacts are not a Section 4(f) use because the scope of the work in these areas are minor, the area will be restored with no permanent impacts and DCNR agrees that the impacts are temporary. Required ROW is for

replacement of the failing gabion wall along SR 940 in East Side Borough. Additional temporary impacts to LGSP include closure of the Lehigh Gorge Trail during specified times and durations, driveway and parking facility closures, and closure of one boat launch under the I-80 bridges (three boat launches being constructed by DCNR south of the project area will remain open).

- Lehigh River Water Trail / PA Scenic River: The proposed replacement I-80 Lehigh River Bridges would place two piers within the Lehigh River delineated ordinary high water mark. One pier of each bridge is anticipated to be placed along the western edge of and extending into the river (approximately 230 square feet total within the river). The existing bridges have 4 piers in the river (approximately 600 square feet total). A Section 4(f) use would not occur due to pier placement since there is no additional land being converted to transportation use; in fact, there would be a reduction. The final determination of the pier locations will be dependent on the contractor's selection of bridge structure type.

Approximately 500 linear feet (1.5 acres) of temporary impact to the Lehigh River is anticipated due to temporary causeways needed to construct the I-80 bridges. Temporary causeways will extend from both riverbanks but will not extend completely across the river allowing for safe passage of recreational boat traffic through the project construction site. A series of causeways is proposed for staged construction over a four year period. Again, the temporary impacts to the water trail are not a Section 4(f) use. Boat traffic through the bridge construction site will be maintained for most of construction; however, temporary closures of boat traffic will be allowed for certain activities such as bridge demolition, construction of piers, and setting of beams. For the safety of boaters traveling through the construction site, an ATON Plan will be implemented.

The existing boat launch under I-80 bridges on the west shore of the river will be closed during construction. Three boat launches being constructed by DCNR south of the project area will remain open. Because the boat launch will be closed for the duration of construction, this was considered a de minimis use rather than temporary occupancy under Section 4(f).

### Summary of Section 4(f) Uses

Within Lehigh Gorge State Park, the proposed bridge replacement project will acquire approximately 0.1 acre Required ROW for replacement of the failed gabion wall along SR 940, 0.4 acre of Proposed Slope Easement to replace the eastbound bridge on an alignment south of the existing bridge, 0.5 acre Proposed Aerial Easements (which is not a Section 4(f) use) necessary for bridge construction, contractor access and future access, maintenance, and construction. The 4.9 acres of TCE within LGSP, and 0.3 acres of TCE within DCNR Easement and temporary impacts to the Lehigh Gorge Trail, PA Scenic River and PFBC Water Trail, are not considered a Section 4(f) use because the uses are temporary and minor, there is no anticipated permanent impacts to these areas, the areas will be restored upon completion of construction, and DCNR and PFBC agreed that these impact to LGSP, the Scenic River and Water Trail will be temporary.

Is mitigation incorporated?       No    Yes

### Describe Mitigation

Mitigation measures for Lehigh Gorge State Park will include:

- Access to and use of the Lehigh Gorge Trail will be maintained to the extent possible during construction:
  - April through October: Overnight closure of the trail (during hours that the state park is closed) will be allowed. Short-term closure of the trail (up to 15 minutes) will be allowed during daylight hours Monday through Friday using flaggers and advance signage.

- November through March: Long-term closure of the trail will be allowed for certain construction activities including bridge superstructure demolition, pier demolition, pier construction, and setting of beams and deck forms, with consideration of working room and safety.
- Advance notice of trail closures will be provided to DCNR, D&L National Heritage Corridor, the Borough of White Haven, and the public.
- The contractor may temporarily realign the trail through the project site if approved by DCNR. A 10' wide trail will be maintained except as needed for closures noted above.
- If necessary due to pier locations, reconfiguration of the driveway, parking, or boat launches will be coordinated with DCNR.
- An ATON Plan will be implemented and will include advance signing. Signs will be placed at boat launches upstream of the project. Additional advanced notification to PFBC, DCNR, and the public will be provided.
- The contractor will restore disturbed areas, and full access to the park amenities will be provided upon completion. As part of restoring the disturbed areas, a planting plan will be prepared in coordination with DCNR during Final Design.
- PennDOT will coordinate with DCNR and White Haven Borough to provide wayfinding signs.

LGSP is also protected under Section 6(f) of the Land and Water Conservation Fund Act. PennDOT will provide replacement land for Section 6(f) conversion within LGSP totaling approximately 6 acres. Although not required for Section 4(f) mitigation, this replacement land will also serve as mitigation of the Section 4(f) use of LGSP. The replacement land being provided is forested land; therefore, the replacement land offsets the removal of trees in the construction staging area, and in the area where the existing gabion wall along SR 940 will be replaced.

Mitigation measures for the Lehigh River Water Trail / PA Scenic River will include:

- An ATON Plan will be implemented and will include advance signing. Signs will be placed at boat launches upstream of the project. Additional advanced notification to PFBC, DCNR and the public will be provided.
- Temporary closures of boat traffic will be allowed for certain activities, including bridge demolition, construction of piers near or within the river, and setting of beams.
- If necessary due to pier locations, reconfiguration of the driveway, parking, or boat launches will be coordinated with DCNR.
- The contractor will restore disturbed areas.

In addition, to mitigate for the PA Scenic Rivers designation, PennDOT has agreed to incorporate the following actions into the project:

- Vegetative buffers for construction staging areas will be provided to the extent practicable.
- Piers will not be allowed in the middle portion of the river.
- Steel girders, if used on the new bridge, will be weathering steel or painted a “natural” color to blend with the setting.
- Where possible, riprap will be covered with soil and vegetation.
- Scenic river signs will be placed at the leading ends of each bridge. Signs will also be placed on the bridge and/or piers for water trail users. PA Scenic Rivers coordination letters are provided in Appendix E.

#### **Remarks**

A local park and baseball field, Lehigh Park, is located adjacent to the I-80 westbound off-ramp to SR 940 in White Haven Borough. No Section 4(f) use of Lehigh Park is anticipated.

The project bridges span the National Register eligible Lehigh Valley Railroad (Allentown to Wilkes-Barre) and Central Railroad of New Jersey/Lehigh Valley Railroad. Through Section 106 consultation, it was determined that there are no contributing resources for either historic district within the Area of Potential Effect (APE), therefore no Section 4(f) use of these historic railroads.

## 4.6 Air Quality and Noise

### AIR QUALITY

Is the project exempt from regional ozone conformity analysis and a CO, PM10 & PM2.5 Hot-Spot analysis?  Yes  No

#### Mobile Source Air Toxics (MSATs)

Is the project exempt from an analysis for MSATs based on Pub #321?  Yes  No

#### Remarks

Per the PennDOT "Project Level Air Quality Handbook" Publication 321, this project is classified as exempt from air quality analysis because it is a bridge replacement project. The project will not add travel lanes and will not result in changes in traffic volumes, vehicle mix, location of existing facility or other factor that would cause an increase in emissions relative to existing conditions.

The cashless tolling facility will not require drivers to stop or slow to pay a toll at the tolling gantry. The tolling system will record vehicles as they pass under the gantry sensor. Therefore, additional air quality impacts due to tolling are not anticipated.

### NOISE

Is the project a:

- A. Type I Project?  Yes  No  
B. Type II Project?  Yes  No  
C. Type III Project?  Yes  No

#### Supporting documentation for Chapter 4.6 includes:

- [I-80 Lehigh River Toll Diversion Noise Analysis Report \(January 2022\)](#)

The project meets the criteria for a Type III project established in 23 CFR 772. Therefore, the project requires no analysis for highway traffic noise impacts. Type III projects do not involve added capacity, construction of new through lanes or auxiliary lanes, changes in the horizontal or vertical alignment of the roadway or exposure of noise sensitive land uses to a new or existing highway noise source. PennDOT acknowledges that a noise analysis is required if changes to the proposed project result in reclassification to a Type I project.

#### Noise Remarks

In accordance with the Federal Noise regulations in 23 CFR 772 and PennDOT Publication #24, this project is a Type III noise project, and therefore, noise analysis is not required and abatement is not eligible for federal funding. As a result of proposed tolling, traffic studies suggest that portions of the local traveling public would seek alternate travel routes to reach destinations in order to avoid toll facilities. In response to the unusual circumstance of likely increases in diversion route traffic volumes due to traffic diversion, PennDOT and FHWA agreed to perform a qualitative assessment of noise to inform the public of the potential effects. A total of 252 noise sensitive land uses were identified within a 500-foot buffer on either side of the diversion route roadways, through the length of the corridor. Noise modeling of peak-hour, worst-case noise levels on the local roadway network indicates that traffic noise levels are not anticipated to exceed PennDOT/FHWA Noise Abatement Criteria (NAC) in the existing condition. In the future no-toll condition, noise levels remain almost unchanged

compared to the existing condition with no receptors anticipated to exceed the NAC. In the future toll condition, 9 receptors are anticipated to exceed the NAC. Differences in traffic noise levels between the future no-toll and future toll conditions are predicted to be no greater than 5 dBA, with the majority of receptors experiencing a 1-3 dBA increase. An increase of 3 dBA or more is considered discernable to a person with normal hearing.

The I-80 Lehigh River Bridges Toll Diversion Noise Analysis Report, January 2022, is included in the project technical files and incorporated by reference to this EA.

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## 4.7 Socioeconomic Areas

### REGIONAL & COMMUNITY GROWTH

**Will the project induce impacts (positive and negative) on planned growth, land use, or development patterns for the area?**  Yes  No

**Is the project consistent with planned growth?**  Yes  No

**Basis of this determination:**

Maintains transportation system. The project is listed on the Interstate Transportation Improvement Program (TIP) program years 2022, 2023, 2024, and 2nd 4 years.

**Will the project induce secondary growth?**  Yes  No

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### PUBLIC FACILITIES & SERVICES

**Will the project induce negative impacts on health and educational facilities; public utilities; fire, police, and emergency services; civil defense; religious institutions; or public transportation?**  Yes  No

Emergency medical services (EMS) representatives were at the traffic diversion workshop. Emergency services vehicles will be exempt from paying toll, thus no reason to use diversion route. EMS dispatchers are accustomed to assigning response teams based on current roadway conditions and other factors. Traffic modeling projects that only 5% of traffic will divert and that the primary diversion route is capable of absorbing the additional traffic. Improvements proposed along the diversion route would also mitigate for increased traffic due to diversion. Emergency response vehicles, such as ambulances and fire trucks, will be exempt from paying the toll. Emergency responders traveling to an incident in their own vehicle will be reimbursed for tolling

**Does the project incorporate bicycle or pedestrian facilities into the overall design or operations (including construction)?**  Yes  No

Increased traffic diverting to avoid the tolls is not anticipated to interfere or negatively impact the safety or mobility of bicyclists and pedestrians along these routes. Proposed traffic improvements along SR 940 in White Haven Borough are intended to enhance safety for bicyclists and pedestrians.

PA Bike Route L will be detoured during construction. The detour will follow SR 940 east to SR 534 south. A bicycle/pedestrian checklist is included in Appendix H.

Traffic improvements in regard of to the diversion route are discussed in Section 3.2.3 above in this EA and in the I-80 Lehigh River Bridges Diversion Route Traffic Evaluation report, February 2022, which is included in the project technical files and is incorporated by reference to this EA.



**Will the project have a positive impact to the public facilities and services listed above?**  Yes  No

The proposed bridge replacement project will maintain a reliable crossing over the Lehigh River and SR 1005, while also widening bridge curb-to-curb width and roadway shoulders, having a positive impact to public facilities and services.

Proposed traffic improvements along SR 940 in White Haven Borough and East Side Borough were determined through coordination with the local officials and are intended to enhance safety and mobility for the local community.

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## COMMUNITY COHESION

**Will the project induce impacts to community cohesion?**  Yes  No

**Will the project induce impacts to the local tax base or property values?**  Yes  No

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## ENVIRONMENTAL JUSTICE *(see Chapter 6 of this EA)*

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### RIGHT-OF-WAY ACQUISITIONS OR DISPLACEMENTS OF PEOPLE, BUSINESSES OR FARMS

**How many parcels require right-of-way acquisition, either partial or total?**

9 parcels impacted with 2 parcels totally taken, 1 parcel with an aerial easement, 1 parcel with partial take (required ROW), 3 parcels with only TCEs, 1 parcel with an aerial easement, permanent slope easement and TCEs, and 1 parcel with both partial takes and TCEs.

**Describe the extent and locations of acquisitions. Indicate for each acquisition whether it is temporary or permanent.**

- White Haven Borough: Total take of existing Service Road ROW along I-80 EB ramp
- RBMN RR: Aerial Easement along south side of I-80 EB bridge
- Commonwealth of Pennsylvania (Lehigh Gorge State Park): Slope Easement along I-80 EB on ramp, Aerial Easement along south side of I-80 EB bridge, TCEs at all four quadrants of bridges
- Commonwealth of Pennsylvania (Lehigh Gorge State Park): required ROW along SR 940 for replacement of failing gabion wall
- Residential property at South East quadrant of I-80 EB bridge: Partial Take and TCEs
- Unknown Owner at South East quadrant of I-80 EB bridge: Total Take, vacant forested land
- Commercial property north of bridge: TCE
- Utility property north of bridge: TCE
- Commercial property north of bridge: TCE

**Will the project require the relocation of people, businesses, or farms?**  Yes  No

**Will the project induce impacts to economic activity, including employment gains and losses?**  Yes  No

## Mitigation

Property acquisitions will be conducted in accordance with the Uniform Relocation Assistance and Real Property Acquisitions Policies Act of 1970, as amended; Title VI of the Civil Rights Act of 1964; and the Pennsylvania Eminent Domain Code of 1964.

- While no residential relocations are anticipated, any individual or family displaced by the project would be offered the full extent of benefits and payments.
- Provisions would be made to ensure that any person with a disability who is displaced is offered replacement housing that meets any special needs. Based on current design plans, no displacements are anticipated.

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## MAINTENANCE AND OPERATING COSTS OF THE PROJECT AND RELATED FACILITIES

Will the project induce increases of operating or maintenance costs?  Yes  No

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## AESTHETIC AND OTHER VALUES

Will the project be visually intrusive to the surrounding environment?  Yes  No

Will the project include “multiple use” opportunities?  Yes  No

Will the project involved “joint development” activities?  Yes  No

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### 4.8 Energy

Energy usage is generally a function of vehicle miles traveled (VMT) (volume x distance traveled), speed, vehicle mix, and congestion. Vehicles consume greater amounts of energy in congested, stop and go, and idling conditions. Additional energy use is associated with lighting, and operations and maintenance activities. Indirect energy use can be attributable to out-of-direction travel associated with toll diversion or detours for incidents, bridge inspections, maintenance, and repairs. A Transportation Research Board study found that traffic congestion typically led to an increase of fuel consumption on the order of 80 percent, and a study by Massachusetts Institute of Technology (MIT) indicates that for congested traffic conditions, fuel consumption is up to 3.5 times higher than in free-flowing traffic.

Absent traffic incidents, construction, or closing of lanes for maintenance activities and repairs, the interstate is typically free flowing and relatively energy-efficient. The cashless tolling facility will not require drivers to stop or slow to pay a toll at the tolling gantry. The tolling system will record vehicles as they pass under the gantry sensor.

Under the no build alternative, more frequent bridge inspections and more frequent maintenance and repairs would result in lane closures and associated detours and congested conditions leading to higher energy expenditures. Should the bridge have to be weight-posted or closed, the associated detours would result in longer travel routes, congestion, and higher expenditures of energy.

The build alternative would require expenditures of energy for the construction of the project but would result in a new bridge requiring less frequent bridge inspections and maintenance/repairs. Proposed tolls assessed on the bridge would cause some traffic to divert, which would increase the VMT for those diverting drivers and increase energy use. However, traffic modeling conducted for the bridge project predicts that only 5% of traffic is anticipated to divert, a relatively small percentage of the overall traffic. The primary diversion route anticipated to be used runs parallel to the interstate and would not add substantial VMT.

## 4.9 Cumulative Effects

Cumulative effects include “the proposed project’s direct and indirect effects in combination with the effects due to past, present, and reasonably foreseeable future activities or actions of Federal, non-federal, public, and private entities” (PennDOT 2008). This analysis was conducted in accordance with Pennsylvania Department of Transportation (PennDOT) Publication 640 Indirect and Cumulative Effects (ICE) Desk Reference (PennDOT, March 2008).

The first step in performing the cumulative effects analysis is to identify which resources to consider in the analysis. The no-build alternative would not contribute to cumulative effects and is not discussed. Cumulative effects are considered only for resources with a direct or indirect effect from the I-80 Lehigh River Bridges Project. Resources not evaluated within the Lehigh River Bridges Environmental Assessment (EA) are not included in the cumulative effects analysis because they are not present. Similarly, resources that are present, but not affected either directly or indirectly by the proposed project, are also not included in the cumulative effects analysis. Because of the potential for direct or indirect effects that could contribute to cumulative impacts, the following resources are evaluated: wetlands, streams, rivers, and watercourses; vegetation ; hazardous materials; park and recreation facilities (including Section 4(f) and 6(f) resources); ; environmental justice, and indirect traffic impacts.

### 4.9.1 Boundaries and Time Frame

Cumulative effects are considered within geographic boundaries that provide context to help understand the health of the resource. The following summarizes the areas used in assessing cumulative effects on resources impacted by the project:

- Aquatic Resources (streams, wetlands, waters) – Lehigh River watershed
- Vegetation – Project area municipalities
- Park and recreation facilities (including Section 4(f) and 6(f) resources) - Project area municipalities
- Hazardous Waste – Project area municipalities
- Environmental Justice - A one hour drive time from the bridge
- Indirect Traffic Impacts – Traffic Diversion Route Area

The time frame for analysis goes back to approximately 1960, just prior to the initial construction of the I-80 corridor and the bridges over the Lehigh River. The following section provides information on the past, present, and reasonably foreseeable future conditions and provides context for understanding the potential cumulative effects.

### 4.9.2 Past

Early development of the borough of White Haven, Pennsylvania is described as follows:

*Throughout the 1800s, canals and railroads were constructed to aid in the mining and transportation of coal in Northeastern Pennsylvania. The County of Luzerne witnessed a population boom with the expansion of the coal mining industry. The Lehigh Coal and Navigation Company chartered the Lehigh and Susquehanna Railroad on March 31, 1837, in order to link Wilkes-Barre to White Haven. Construction of the railroad began in 1839 and was completed in the 1840s.*

*With the completion of the Lehigh and Susquehanna Railroad, the canal industry, which had existed for no more than one generation, faced a rapid extinction. The Lehigh and Susquehanna Railroad moved Wyoming*

*Valley anthracite from its various coal fields to White Haven; over the Appalachian Mountains that had posed such a challenge to settlers for 150 years. Once the L&SR reached White Haven, an extensive transportation network expeditiously and economically delivered the Wyoming Valley's "black diamonds" to a waiting market. By 1867, the Lehigh Valley Railroad, which was first established in the Hazleton area's coal fields, was linked to Wilkes-Barre, and then in 1869, the Lehigh Valley Railroad was connected to other rail systems in New York State.*

The cultural resources report identified that "White Haven continued to have a focus on industry into the late nineteenth and early-twentieth centuries, although this influence was waning....The last remaining sawmill closed in 1892, and the gradual decline of coal began in the early twentieth century....Between 1939 and 1959, the canal basin was filled in and the dam across the river was removed. Some additional blocks towards the western and southern sides of town were developed for residential use during this time, and by 1969, minor changes were made to the industrial section of town along the waterfront.... Today, one rail line remains passing through town, and the historically significant industrial activity has been discontinued."

The aerial photograph from 1963 (**Figure 6**) shows the project area prior to the development of the I-80 corridor north of White Haven and East Side. The town of White Haven and the bridge crossing the Lehigh River (now SR 940) are visible. The area outside of the town is rural and appears generally forested, with some agricultural and residential development. The aerial photograph from 1969 (**Figure 7**) shows the area after the highway and bridges are constructed. Road configurations appear similar to current conditions. Residential development appears more defined north of town by 1992 (**Figure 8**) and 2018 (**Figure 9**).

**Figure 6 – 1963 Aerial Photograph of the Project Area**



Note: I-80 has not been constructed south of White Haven. The sections of SR-940 through the community (Church Street, Berwick Street, the bridge crossing the Lehigh River, River Street, and State Street) are visible.

Source: U.S. Geological Survey photo, Flight Year 1963, from Appendix D of the PennDOT Engineering District 5-0 Phase I Environmental Site Assessment: I-80 Section 08B White Haven Bridges Over RBMN RR, Lehigh Gorge Trail, Lehigh River, and SR 100. April 2021.

**Figure 7 – 1969 Aerial Photograph of the Project Area**



Note: I-80 corridor and bridges have been constructed. South of the highway in East Side, the Mountain Laurel Golf Club source can be seen (constructed in 1969).  
Source: U.S. Geological Survey photo, Flight Year 1969, from Appendix D of the PennDOT Engineering District 5-0 Phase I Environmental Site Assessment: I-80 Section 08B White Haven Bridges Over RBMN RR, Lehigh Gorge Trail, Lehigh River, and SR 100. April 2021.

Figure 8 – 1992 Aerial Photograph of the Project Area



Image U.S. Geological Survey

Source: U.S. Geological Survey photo, accessed using Google Earth Pro, Image date April 14, 1992.

Figure 9 – 2020 Aerial Photograph of the Project Area



Source: Google Earth Pro, Image dated 6/15/2018. Development Few changes observed from 1992 aerial photograph (see Figure 8).



### 4.9.3 Present

Current conditions of each resource are summarized below. Details are described in the corresponding affected environment sections in Chapter 4 of this EA and in technical reports cited in those sections.

**Streams, Rivers, and Watercourses:** The Lehigh River is a 109-mile-long tributary of the Delaware River, located in eastern Pennsylvania. The Lehigh River and a number of its tributaries are designated Pennsylvania Scenic Rivers, managed by the Pennsylvania Department of Conservation and National Resources (DCNR). It originates near the Poconos, and its upper course offers many recreational whitewater boating sections. The most popular is through Lehigh Gorge State Park (from the Francis E. Water Dam to Jim Thorpe), which includes the project area. The Lehigh River is a naturally reproducing trout stream, as well as a trout-stocked stream. There are other streams in the project vicinity; however, this project is not anticipated to impact other streams or watercourses. For more information, see Section 4.1 Aquatic Resources of the EA, and the Wetland and Watercourse Identification and Delineation Report.

**Wetlands:** Wetlands within the study area include just over 0.9 acre of palustrine forested and palustrine scrub shrub wetlands. The Lehigh River is not included, although U.S. Fish and Wildlife Service National Wetlands Inventory identifies it as a riverine upper perennial unconsolidated bottom permanently flooded (R3UBH) wetland. For more information, see Section 4.1 Aquatic Resources of the EA, and the Wetland and Watercourse Identification and Delineation Report.

**Navigable Waters:** The stretch of the Lehigh River within the project area is considered navigable by the USCG, a water trail by the PFBC, and a recreational water according to Keystone Canoeing. There is a boat access point at the bridge. While the project is exempt from needing a USCG permit, an Aids to Navigation (ATON) plan is required.

**Vegetation:** The project area is heavily forested beyond the roadside vegetation and there are lawns and landscaping through the developed parts of the project area.

**Park and Recreation Resources (including Section 4(f) and 6(f) resources):** The project bridges span Lehigh Gorge State Park (LGSP) and the Lehigh Gorge Trail (a section of the Delaware and Lehigh [D&L] National Heritage Corridor Trail). The Lehigh River within the project area is part of the PFBC-designated Lehigh River Water Trail. The 41-mile northern section of the water trail begins at the White Haven boat access underneath the existing bridges. More details about the water trail can be found in Sections 4.1 and 4.5 of the EA. A White Haven Borough baseball field and stadium, Lehigh Park, is located approximately 0.1 mile west of the project. All of these properties—LGSP (including the Lehigh Gorge Trail within the park), the Lehigh River Water Trail, and Lehigh Park—are afforded consideration and protection under Section 4(f) of the U.S. Department of Transportation Act of 1966. The LGSP has received funding from the Land and Water Conservation Fund (LWCF) and also is considered a Section 6(f) resource.

**Hazardous Materials:** There is an abandoned municipal landfill approximately 0.32 mile southwest of the project area and historic fill associated with the former railroad in what is now the Lehigh Gorge Trail. These two locations are considered Recognized Environmental Conditions (see Section 4.2 of the EA), and Phase II/III surveys will be conducted during final design to determine whether special provisions are needed during construction to avoid, manage, or properly dispose of excavated materials. Further evaluations may occur if the project changes to impact the railroad. The bridges will need to be tested for lead paint and asbestos-containing materials.

**Environmental Justice:** The regional study area comprises 18 counties (17 in Pennsylvania and 1 in New Jersey) with populations concentrated in villages and city centers that flank I-80. Nearly 4 million people live in this study area, with 9.5 percent living below the poverty level and a minority population of approximately 21 percent. Low-income and minority populations are located throughout the regional study area and concentrated in the town of Bloomsburg, City of Hazelton, Pocono Township, Paradise Township, Stroud Township, Hamilton Township, Stroudsburg Borough and East Stroudsburg Borough.

**Indirect Traffic Impacts:** With the introduction of a toll eastbound on the I-80 Bridge over the Lehigh River, there is potential for traffic to divert off the Interstate to avoid paying the toll. A primary diversion route consisting of SR 940 and SR 537 was identified. Traffic modeling indicated that approximately 5 percent of traffic is anticipated to divert from I-80.

#### 4.9.4 Future

This section identifies reasonably foreseeable future actions anticipated to occur over a 30-year period (the anticipated length that tolls will be in place).

**Growth Trends:** The project is not anticipated to result in substantial project-related growth, so no substantial indirect effects or induced growth are expected. Growth or land use changes in the project area are related primarily to redevelopment of previously developed properties and would not be caused by the project.

**Reasonably Foreseeable Future Actions (RFFAs):** Several transportation projects that are programmed to be completed within the area of the I-80 bridges over the Lehigh River project were evaluated for potential contributions to cumulative impacts and are listed below in **Table 4**.

**Table 4**  
**Reasonably Foreseeable Transportation Development Projects**

Locale	Name	Land Use	Description
I-80 various municipalities, Luzerne County	I-80 Surface over ASR 2015	Resurface	Concrete overlay on I-80 westbound and bridge preservation over SR 309 in Butler Township, over Tributary to Nescopeck Creek in Butler Township, and over Linesville Creek in White Haven Borough, Luzerne County.
District 4-0	Interstate Guide Rail Upgrade - 4-0	Guiderail Improvement	Project to upgrade existing guiderail and end treatments on Statewide Interstate System based on <i>Manual for Assessing Safety Hardware</i> criteria. These projects will address necessary upgrades that were not already addressed.
I-80, Luzerne County	I-80 Rehabilitation	Resurface	This project involves concrete preservation on Interstate 80 Eastbound from 1 mile west of Exit 292, SR 309 to Exit 292, SR 309 and from the Roadside Rest to the Luzerne/Carbon County Line in Butler, Dennison and Foster Townships, White Haven Borough.
PA 940, Carbon County	PA 940 Resurface	Resurface	Highway Resurface of PA 940 from Eastside Borough to SR 8004 in Kidder Township, Carbon County.

Source: Twelve Year Program (TYP) / Transportation Improvement Program (TIP)

The project team coordinated with borough, township, and county planning departments to identify anticipated land development projects in the project area vicinity that could contribute to cumulative impacts. As previously mentioned, the identified growth is not caused by the project, but could contribute to potential cumulative impacts. The developments listed in **Table 5** are planned in the project area vicinity.

**Table 5**  
**Reasonably Foreseeable Land Development Projects**

<b>Locale</b>	<b>Name</b>	<b>Land Use</b>	<b>Description</b>
White Haven, Luzerne County, Lehigh Gorge State Park	LGSP Capital Improvements	Parks and Recreation	Construction of a new LGSP entrance from SR-940 south of the I-80 bridges. Will include parking lots, flush facilities, and four boat launches. All park vehicular traffic, including commercial biking and boating outfitters are intended to use the new entrance.
White Haven, Luzerne County	Townhomes	Residential	Construction of approximately 60 townhomes near SR-940 and SR-437
Kidder Township, Carbon County	Exeter Property Group	Warehouse	921,000+ square foot class A warehouse (187 dock doors; almost 300 trailer parking spaces; approx. 300 jobs) is currently under construction on 284-acre parcel along SR-940 approximately 1 mile east of the PA Turnpike Pocono interchange. 80 of the 284 acres being developed; remainder including area where creek meanders through, to remain forested & undeveloped.

Source: Phone conversation with Linda Szoke, White Haven Borough, March 22, 2022. Phone conversation with Cindy Norato, Kidder Township Planning Administrator/Zoning Officer, April 18, 2022. In phone conversations with Eastside Borough, Luzerne County, and Carbon County, planners were unaware of development activities near the project.

The traffic modeling completed for the project includes future growth assumptions; therefore, anticipated traffic generated by developments listed in **Table 5** should be accounted for in the traffic forecast for the project area. The noise, air quality, and other traffic-related cumulative impacts are incorporated into the traffic model and are described in the respective sections of the EA if applicable.

#### 4.9.5 Cumulative Effects Summary

This section presents the cumulative effects analysis of the project on each evaluated resource when added to other past, present, and reasonably foreseeable future actions. The analysis identifies whether the cumulative impacts would be significant. **Table 6** at the end of this section, shows the effects of past actions combined with past, present, and reasonably foreseeable projects/actions in the cumulative effects study area. No significant cumulative effects resulting from this project are identified.

**Streams, Rivers, and Watercourses:** Permanent impacts associated with the project include removal of existing bridge piers and the installation of new bridge piers within the Lehigh River. In-stream construction timing restrictions (when work cannot be performed in the stream) occur from both February 15 to June 1 (stocked trout restriction) and October 1 through December 31 (wild trout restriction). Best management practices (BMPs) would be used for erosion and sediment control. The U.S. Army Corps of Engineers (USACE) would require mitigation, which would further reduce potential impacts. Once construction is complete, streams would function similar to existing conditions. The LGSP capital improvements would include adding/improving boat launches and adding other amenities in the same general area along the river bank as the existing I-80 bridges. BMPs for erosion and sediment control and adherence to appropriate permit requirements would keep effects on the river to a minimum. Other identified RFFA land development is not anticipated to affect the Lehigh River. Cumulative impacts are not anticipated to be significant.

**Navigable Waterways:** The construction and demolition of the project bridges would have temporary impacts to the navigation of the river. Temporary causeways will extend from both riverbanks but will not extend completely across the river. Boat traffic through the bridge construction site will be maintained for most of construction; however, temporary closures of boat traffic will be allowed for certain activities, such as bridge demolition, construction of piers near or within the river, and setting of beams. The boat launch directly under the bridge will be closed during construction. The project would impact the proposed boat launch as part of the LGSP capital improvements project but would restore all amenities, access, and landscaping as part of mitigation. The three boat launches downstream of the bridges would remain open. An ATON plan was approved by PFBC on August 16, 2021, which includes plans to place buoys to indicate safe passageways for river traffic and signage. None of the other RFFAs would affect the river's navigability. Cumulative impacts are not anticipated to be significant.

**Wetlands:** The project would permanently impact 0.075 palustrine forested and 0.035 palustrine scrub shrub wetland acres. Compensatory mitigation would be required for this project, which would offset the wetland impacts. RFFAs are not sufficiently developed to know their contributions to cumulative wetland impacts, but would require mitigation per federal and state law, thereby minimizing overall impacts to the health of wetlands in the watershed. Cumulative impacts are not anticipated to be significant.

**Vegetation:** Permanent impacts to forested, landscaped, and other vegetated areas would occur for installation of the proposed bridges, abutments, and piers, cut and fill, and stormwater controls. Temporary impacts would occur to provide constructor access. BMPs would be followed to avoid and minimize actions that would transplant roots or seeds of noted invasive, non-native plants during earth-moving operations. All disturbed areas would be stabilized and seeded with non-invasive vegetation following construction. Vegetated areas within LGSP that are disturbed would be restored in consultation with DCNR. As noted under parks and recreation resources, approximately 6 acres of forestland is being acquired to off-set the Section 6(f) property conversion; this would also off-set the vegetation effects of the project. The LGSP capital improvement project would disturb vegetation in the same basic area as the I-80 bridge replacement project. The Exeter warehouse involves development of approximately 80 acres of a 284-acre forested parcel. To off-set effects, 200 acres, including the area where a stream meanders through, are being preserved as undeveloped land. With BMPs and other mitigation/preservation, cumulative impacts are not anticipated to be significant.

**Parks and Recreation Resources (including Section 4(f) and Section 6(f) resources):** LGSP received an LWCF grant, which affords it protection under Section 6(f). It is also considered a Section 4(f) resource. Approximately 6 acres of LGSP property would be impacted during construction. Since the temporary construction easements would be needed for approximately 4 years, the project would result in a permanent Section 6(f) conversion, and replacement land would need to be acquired in accordance with LWCF requirements. Approximately 6 acres of forested land is being purchased and turned over to DCNR to address the Section 6(f) conversion requirements. With mitigation, the project would not contribute to cumulative impacts. The project was determined to have a de minimis impact on the LGSP/Lehigh Gorge Trail within the park as a Section 4(f) property and as such would not substantially contribute to Section 4(f) cumulative impacts.

The Lehigh River Water Trail is also a Section 4(f) resource. The Lehigh River would be temporarily impacted due to temporary causeways needed to construct the bridges. An ATON plan has been developed to maintain river traffic during construction. Short-term, temporary closures of boat traffic would occur for certain activities, including bridge demolition, construction of piers near or within the river, and setting of beams. With the

proposed mitigation, the project was determined to have a de minimis impact on the river trail as a Section 4(f) property.

There would be no permanent or temporary impacts to Lehigh Park. While there is an RFFA to construct parking facilities and boat launches within LGSP, the LGSP capital improvement project would enhance the recreational amenities and features of the park and would not result in a conversion of use to a transportation facility; and therefore, would not be a Section 4(f) or Section 6(f) impact. This project, in combination with the RFFAs, would not have significant cumulative impacts to parks and recreation resources or to Section 6(f) or Section 4(f) resources.

**Hazardous Materials:** Further inspections would be conducted during final design, including soils analysis for the abandoned municipal landfill and the historic fill in the state park, as well as testing of the existing bridges for asbestos-containing materials and lead paints to ensure that safe construction and demolition practices are established. Mitigation practices that emerge from these investigations would be sufficient so that the project would not contribute to cumulative impacts. The presence of hazardous materials anticipated to be encountered as part of the RFFAs is unknown. Each project would be evaluated and mitigated. Therefore, cumulative impacts are not anticipated to be significant.

**Environmental Justice:** Tolling the Lehigh bridges has the potential to have socioeconomic effects due to the cost of the toll and its potential to change mobility patterns and accessibility. Implementation of a toll would affect all bridge users regardless of income and minority status. Because tolls would comprise a larger percentage of a low-income bridge user's income, tolls would have a greater effect on these users, particularly if they depend on the bridge for daily travel to work or other destinations. However, PennDOT is proposing to offer toll-free bridge access to low-income persons. Due to the availability of the diversion route and the low-income program offered under the PennDOT tolling policy, the effects on low-income travel patterns would be minimal and would not constitute an adverse impact on low-income or minority populations. Additionally, the proposed tolling would result in the revenue needed to construct a new bridge, which would provide overall benefits that include improved safety conditions and reduced delay due to maintenance and incidents on the bridge. These benefits would accrue to all commuters and would offset inconveniences resulting from the introduction of a toll. For these reasons, no disproportionately high and adverse effects on low-income or minority populations have been identified for the Project since adverse effects to these populations are not anticipated as a result of project and/or tolling implementation. RFFAs are not anticipated to contribute adverse effects to low-income or minority populations. No significant cumulative impacts to environmental justice are anticipated.

**Indirect Traffic Impacts:** A primary diversion route consisting of SR 940 and SR 534, north of I-80, was identified. Traffic modeling indicated that with currently proposed eastbound-only tolling, approximately 1,360 vehicles eastbound would divert daily from I-80 (5% of total traffic). It is estimated that 940 of the diverting vehicles would use this primary diversion route. It was originally proposed to provide tolling in both directions of I-80; however, a subsequent decision was made to implement tolling only in the eastbound direction for this bridge. As a result, there are no diversions expected in the WB direction. Based on field investigation, stakeholder input, and traffic analysis, a number of community concerns and potential roadway issues were identified along the primary diversion route. To mitigate the roadway issues, PennDOT would improve the diversion route, including prohibiting large trucks and trailers on Bridge Street; replacing signage and striping; improving pedestrian crossings; repaving and regrading sections; and replacing the rock gabion wall on SR-940 just east of the bridge. For more detail, see Section 3.1 of the EA. The diversion route improvements would be beneficial to

indirect traffic flow and would not contribute to a cumulative impact. The RFFAs are not anticipated to induce indirect traffic impacts, other than temporary changes during construction detours. The proposed improvements would mitigate those instances. Trucks leaving warehousing east of the PA Turnpike Pocono interchange could travel west on I-80 without paying a toll. Trucks traveling I-80 eastbound headed for the warehouse would be restricted from traveling SR-940 through White Haven and East Side Boroughs and would remain on I-80. No significant cumulative impacts to indirect traffic are anticipated.

**Table 6  
Potential Cumulative Impacts**

<b>Topic</b>	<b>Past Actions/Impacts</b>	<b>Present Condition/Proposed Impacts</b>	<b>RFFA Impacts</b>	<b>Summary</b>
<b>Streams, Rivers, and Watercourses</b>	Lehigh River within the project area has been bridged by I-80 bridges since 1965. Previous alterations include a channel basin and a dam that have both been removed.	The creek is both a stocked trout stream and a wild trout stream. The project will have: - 79 linear feet permanent stream impact -578 linear feet temporary stream impact  Construction timing would minimize bridge replacement impacts.	An RFFA to install multiple boat launches, including one in the project footprint, is anticipated. Warehouse east of Pocono interchange is avoiding the stream and preserving 200 acres of forested area.	In-water work timing restrictions and the removal of piers within the main river channel should minimize impacts to the waterbody. RFFAs are not expected to contribute to cumulative impacts.
<b>Wetlands</b>	Past wetland impacts from the highway development likely occurred during original highway construction.	Just over 0.9 acre of wetlands were identified within the project area. All wetlands are considered of exceptional value. The project would impact just over 0.11 acre of palustrine wetlands. Mitigation would be required and would reduce the effects.	RFFAs are not sufficiently developed to know their contributions to cumulative wetland impacts, but would require mitigation per federal and state law, thereby minimizing overall impacts to the health of wetlands in the watershed.	Project wetland impacts are minimal. RFFAs are not expected to contribute to cumulative impacts.
<b>Navigable Waters</b>	The Francis E. Walter Dam upstream of the project area releases waters that influence river flows. Project bridge piers are within the river channel.	The proposed project would require temporary causeways for bridge construction. An ATON plan has been approved that will maintain a channel during construction. One of four LGSP boat launches proposed under the I-80 bridges would be closed during construction; remaining three would be accessible.	RFFAs would not affect the navigable waterway.	Project impacts to navigation would be temporary. RFFAs would not affect navigation.
<b>Vegetation</b>	Highway and bridge development has removed existing natural forest.	Includes roadside vegetation, residential lawns, forested areas, and landscaping. Approximately 6	Resurfacing and guiderail projects would not affect vegetation. LGSP capital	Project vegetation impacts are relatively minimal and off-set by the preservation of forestland associated

Topic	Past Actions/Impacts	Present Condition/Proposed Impacts	RFFA Impacts	Summary
		<p>acres of forestland is being acquired and transferred to DCNR to off-set the Section 6(f) property conversion which would off-set forestland impacts for the project as well. BMPs would be established to prevent the introduction and spread of invasive species.</p>	<p>improvements would affect vegetation in the same area as the bridge replacement. Warehouse involved clearing trees, but is preserving 200 of its 284 acres in forest.</p>	<p>with the Section 6(f) land replacement. With the preservation of 200 acres in forestland on the 284-acre warehouse site, RFFAs are not expected to contribute substantially to cumulative impacts.</p>
<p><b>Parks and Recreation Resources</b></p>	<p>The project bridges span LGSP and the Lehigh Gorge Trail (a section of the Delaware and Lehigh [D&amp;L] National Heritage Corridor Trail). The Lehigh River within the project area is part of the Lehigh River Water Trail. LGSP/Lehigh Gorge Trail and the Lehigh River Water Trail are Section 4(f) resources, and LGSP is a Section 6(f) resource.</p>	<p>Approximately 6 acres of LGSP property would be converted for transportation use under Section 6(f) and the Lehigh River Water Trail would be temporarily impacted. The project was determined to have a de minimis Section 4(f) use of LGSP/Lehigh Gorge Trail and the Lehigh River water trail. The project would result in a Section 6(f) conversion, and replacement land is being acquired in accordance with LWCF requirements for LGSP.</p>	<p>The LGSP capital improvement project being undertaken by DCNR would enhance park and recreational resources and features. Other RFFAs would not impact park or recreational resources.</p>	<p>Project impacts to LGSP/Lehigh Gorge Trail and the Lehigh River Water Trail would be mitigated and are not anticipated to be significant. The LGSP capital improvements would enhance the park and recreational resources. Other RFFAs would not affect parks and recreational resources.</p>
<p><b>Hazardous Materials</b></p>	<p>A municipal landfill and historic fill at the railroad are in the project area. Original bridge construction and maintenance may have included lead paint and/or asbestos-containing materials.</p>	<p>Investigations would continue in design to identify whether hazardous materials are present and require use and handling procedures. If so, special provisions would be used during construction and the current bridges' demolition to protect workers and environment.</p>	<p>RFFAs are not anticipated to contribute to hazardous material impacts.</p>	<p>With proposed mitigation, the project would not contribute to cumulative impacts. RFFAs are not anticipated to affect hazardous materials.</p>
<p><b>Environmental Justice</b></p>	<p>The I-80 bridge has been free since it was built. Tolls have not affected</p>	<p>Environmental justice populations are present in the travelshed of the bridge. Without the proposed low-</p>	<p>RFFA projects are not anticipated to negatively</p>	<p>With the proposed low-income toll program, the project is not anticipated to contribute to cumulative</p>



Topic	Past Actions/Impacts	Present Condition/Proposed Impacts	RFFA Impacts	Summary
	environmental justice populations.	income toll program they could experience economic impact from tolls; however the low-income toll program has been incorporated into the project to mitigate this impact .	affect environmental justice communities.	environmental justice impacts, and RFFAs would not negatively affect environmental justice communities.
<b>Indirect Traffic Impacts</b>	Diversion route roadways have largely been in place since before I-80 was built. Traffic has generally been local and low-volume on diversion routes in the past.	Traffic on potential diversion routes has been low. Diversion traffic could contribute to some congestion and safety issues but improvements are proposed as part of the project to minimize these effects.	RFFAs would not contribute to adverse indirect traffic impacts. Trucks to and from the warehouse would be restricted from using SR-940 through East Side and White Haven Boroughs.	With proposed improvements on the diversion route, the project is not anticipated to create significant cumulative traffic impacts. Trucks to and from the warehouse would be restricted from traveling SR-940 through East Side and White Haven Boroughs.

In summary, no significant cumulative effects resulting from this project together with past, present, and reasonably foreseeable future actions were identified.

## 4.10 Permits Checklist

No Permits Required

United States Army Corps of Engineers Section 404 and/or Section 10 Permit

Individual  Nationwide  PASPGP

DEP Waterway Encroachment (105) Permit

Standard  Small Project  General  Other

DEP 401 Water Quality Certification

Coast Guard Permit

NPDES Permit

General  Individual  Exempt

Other Permits

### Remarks

Although it is anticipated that the permits indicated above will be required for the project, a final determination of their applicability will be determined during Final Design. The P3 Development Entity will prepare and submit the permit applications and secure necessary permit authorizations prior to construction.

## 5.0 PUBLIC INVOLVEMENT

	#	Comments
<input checked="" type="checkbox"/> Plans Display	1	See Remarks
<input checked="" type="checkbox"/> Public Officials Meetings	1	See Remarks
<input checked="" type="checkbox"/> Public Meetings	1	See Remarks
<input checked="" type="checkbox"/> Public Hearing		A Hearing will be held.
<input checked="" type="checkbox"/> Special Purpose Meetings (specify)	3	Diversion Route Workshop 8/9/2021 Two meetings with White Haven and East Side Borough, See Remarks
<input type="checkbox"/> Section 106 Public Involvement / Consulting Parties		
<input type="checkbox"/> Section 106 Tribal Consultation		
<input checked="" type="checkbox"/> Environmental Justice Community Involvement		Knowledgeable Parties emails and flyers, see Remarks
<input type="checkbox"/> Other information dissemination activities		
<input checked="" type="checkbox"/> Commitment for Further Public Involvement		The contractor will continue to coordinate with local municipalities and the public.

### Remarks

Public outreach activities were conducted beginning in November 2020 for the PennDOT Pathways program under an Alternative Funding PEL Study. The project was identified as a candidate for bridge tolling through PennDOT Pathways Program: The Major Bridge P3 Initiative in February 2021. Additional public outreach effort was conducted for the I-80 Lehigh River Bridges.

- Project information was posted on a project-specific website in February 2021 at <https://www.penndot.pa.gov/RegionalOffices/district-5/ConstructionsProjectsAndRoadwork/Pages/I-80-White-Haven.aspx>
- A diversion route workshop was conducted on August 9, 2021 to gather additional information regarding potential issues along the diversion routes. The focus of the workshop, conducted at the Split Rock Resort in Lake Harmony, PA, was to collect input and identify concerns related to potential routes travelers may use to avoid paying a toll on the bridge.
- The diversion route workshop attendees were invited to attend a follow-up briefing on November 17, 2021 to review the proposed diversion route improvements included in the public meeting materials.

#### Supporting documentation for Chapter 5 includes:

- [I-80 Over Lehigh River Bridges Project Virtual Public Meeting Summary \(November 17 to December 17, 2021\)](#)
- [I-80 Over Lehigh River Bridges Public Meeting Summary \(January 2022\)](#)

- Additional coordination was held with White Haven and East Side Borough officials on December 14, 2021 and February 10, 2022 to discuss potential traffic improvements to enhance safety along the diversion route.
- A project-specific virtual public meeting was held from November 17, 2021, to December 17, 2021. The online meeting was comprised of text, graphics and videos that provided a project overview and explained the project purpose and need, project design, proposed funding, traffic studies and associated diversion route improvements, environmental studies, comment process and next steps. The online meeting website provided a comment form that allowed individuals to submit their comments directly within the virtual public meeting. The website also noted other ways in which comments could be submitted, including the comment form on the general project website, project phone number, project email and a physical mailing address.
- An in-person public open house was held on Wednesday December 8, 2021 at Split Rock Resort in Lake Harmony, PA. At the in-person public open house display boards were provided for project purpose and need, project design, proposed funding, traffic studies and associated diversion route improvements, environmental studies, and schedule. Comment forms were provided for individuals to submit their comment while in attendance or at their convenience. While the comment period for the public meeting has closed, the online meeting materials are available for reference via the project website. In-person meeting materials were printed versions of the online content. Public involvement documentation is located in the project's technical file.

Prior to and during the public comment period for the public meeting, the project team executed several outreach strategies to maximize public participation at the public meeting or online consultation of the Virtual Public Meeting on project website. The outreach activities are listed in **Table 7**.

**Table 7  
Public Outreach Activities for Public Meeting**

<b>Outreach Type</b>	<b>Number of Recipients</b>	<b>Type of Recipients</b>	<b>Date Sent</b>
Virtual Public Meeting Website	N/A	-General Public via <a href="https://www.penndot.pa.gov/RegionalOffices/district-5/ConstructionsProjectsAndRoadwork/Pages/I-80-Lehigh-VPM.aspx">https://www.penndot.pa.gov/RegionalOffices/district-5/ConstructionsProjectsAndRoadwork/Pages/I-80-Lehigh-VPM.aspx</a>	Launched 11/1/21
Postcard	5,567	-General Public -Mailed via Every Door Direct Mail Service -Sent to all postal routes within the direct project area and along the diversion route.	Mailed Week of 11/8/21
Legal Ad	Circulation approximately ~1,000	-General public Placed in <i>The Journal-Herald</i>	Ran 11/11 /21
Stakeholder & Public Mailing List Email	118	-Key stakeholders, legislators and those who requested to be put on the project's mailing list. -Email with information about the virtual public meeting and in-person open house.	11/17/21
Knowledgeable Parties Email & Flyer	13	-Knowledgeable parties identified in environmental justice analysis -Email with information about virtual and in-person meetings, along with a flyer to be distributed in the community and copies of social media art for sharing	11/17/21
News Release	N/A	-Sent to area media to distribute via news stories and calendars of events for the general public.	11/17/21
Public Officials Briefing	N/A	-Invited public officials to a pre-launch briefing to get a first look at the materials to launch in the virtual public meeting	11/17/21
Social Media Posts	30,733 people reached total from three posts	-Social media posts on PennDOT social media regarding how to participate in the public meeting and comment period -104 engagements across three posts	11/17/21, 12/8/21, 12/16/21

The I-80 Lehigh River Environmental Justice Analysis, March 2022, and the I-80 Lehigh River Bridges Diversion Route Traffic Evaluation are included in the project technical files and are incorporated by reference to this EA. Public involvement documentation is located in the project technical files.

## 6.0 ENVIRONMENTAL JUSTICE

Executive Order 12898, *Federal Actions to Address Environmental Justice in Minority and Low-Income Populations* (February 11, 1994), directs federal agencies to identify and address, as appropriate, disproportionately high and adverse human health or environmental effects of programs, policies, and activities on minority and low-income populations. To achieve effective and equitable decision-making, the U.S. Department of Transportation (USDOT) identifies three fundamental principles of environmental justice to consider in all USDOT programs, policies, and activities:

Supporting documentation for Chapter 6 includes:

- [I-80 Lehigh River Environmental Justice Analysis \(March 2022\)](#)

An Environmental Justice Analysis was completed for the project. The I-80 Lehigh River Environmental Justice Analysis, March 2022, is included in the project technical files and is incorporated by reference to this summary.

The Environmental Justice analysis for the project was performed by completing the following process:

- **Step 1: Define the Study Area.** Consistent with NEPA practices, identify the reasonable and logical boundaries by considering the potential for direct and indirect impacts related to the implementation of the toll and increased traffic on routes that may be used to avoid the toll.
- **Step 2: Identify Low-income and Minority Populations.** Collect recent data on race, color, national origin, income, tribal governments, and seasonal and migrant workers in the study area, and apply FHWA and PennDOT methodology to identify low-income and minority populations.
- **Step 3: Solicit Input from Low-income and Minority Populations.** Using PennDOT's *Public Involvement Handbook* and other environmental justice outreach guidance, identify appropriate outreach techniques. Through targeted outreach to potentially affected low-income and minority populations, identify transportation needs and concerns about the project to inform Steps 4, 5, and 6.
- **Step 4: Evaluate Adverse and Beneficial Effects.** Analyze whether the project would create impacts to communities or populations in the near, medium, or long term. Then, with input from the community, assess whether the impacts are adverse, beneficial, or both.
- **Step 5: Identify Disproportionately High and Adverse Effects.** Determine whether adverse effects are predominately borne by low-income persons and minorities, and if these effects are more or greater than those effects borne by the general population.
- **Step 6. Evaluate Mitigation Measures.** If adverse effects would be predominately borne by low-income and minority populations and are more or greater in magnitude than the adverse effect that would be suffered by the general population, consult with the community to identify measures to avoid, minimize, or mitigate the impacts. Determine whether the mitigation measures are practical. Practical mitigation measures are those that are: effective and do not create other adverse effects that are more severe; feasible in terms of implementation and operation; and cost effective, while maintaining the financial viability of the project.
- **Step 7: Re-evaluate Disproportionately High and Adverse Effects and Document Decision.** If practical mitigation measures have been identified, re-evaluate whether adverse effects borne by low-income and minority populations are appreciably more severe or greater than those effects borne by non-environmental justice populations.

As a result of the PEL Study, PennDOT developed a plan for implementation, which identified alternative bridge funding as an immediately needed priority and bridge tolling as the reasonable means for financing priority bridge improvements. PennDOT began engaging the community, stakeholders, and legislators in the Pathways

Program’s Major Bridge P3 Initiative, which included announcing nine interstate bridges as candidates for tolling in February 2021, including the I-80 Over Lehigh River Bridges Project. Early outreach activities for the I-80 Over Lehigh River Bridges Project included: a public engagement program conducted throughout the PEL process; targeted outreach to knowledgeable parties; a traffic diversion route workshop; targeted outreach to low-income and minority populations in the Lehigh River Regional Study Area; on-demand public meetings (virtual) over a 30-day period between November 17 and December 17, 2021; and an in-person meeting held on Wednesday December 8, 2021 at Split Rock Resort in Lake Harmony, PA.

Potential implementation of a toll would affect all bridge users regardless of income and minority status. Because tolls would comprise a larger percentage of a low-income bridge user's income, tolls would have a greater effect on these users, particularly if they depend on the bridge for daily travel to work or other destinations. As a result, and in keeping with other DHS financial assistance programs offered in Pennsylvania (Supplemental Nutrition Assistance Program (SNAP), Medicaid, Low Income Home Energy Assistance Program (LIHEAP)), PennDOT is proposing to offer toll-free bridge access to low-income persons qualifying for one or more of these DHS programs. The DHS financial assistance programs use a progressive income limit based on the number of people in a household (equivalent to approximately \$35,000 for a family of four, but it varies slightly by DHS program). Individuals who qualify for toll-free bridge access would be able to select one toll bridge from the Major Bridge P3 Initiative to apply these benefits. Because of their proximity, the Lehigh River bridges and Nescopeck Creek bridges could be selected as “one bridge.” PennDOT’s tolling policy would also include toll-free bridge access for emergency vehicles and emergency medical service volunteers when responding to emergencies.

As a result of this analysis and associated outreach effort, no disproportionately high and adverse effects on low-income or minority populations have been identified for the I-80 Over Lehigh River Bridges Project since adverse effects to these populations are not anticipated as a result of the project and/or tolling implementation. Considering the availability of acceptable options for toll avoidance, PennDOT’s toll policy that offers a toll-free ride for low-income drivers and others who meet the eligibility criteria, and a diversion route that has the capacity to accommodate projected traffic increases, with minor improvements proposed, adverse socioeconomic effects on low-income and minority populations would be minimized.

Additionally, PennDOT commits to:

- Reassess the low-income toll program 5 years after substantial completion of the project. The purpose of this reassessment will be to determine the effectiveness of the low-income toll program for reducing the burden of tolls on low-income households and, depending upon the outcome of the reassessment, identify alternative solutions for reducing the burden on low-income households.
- Perform a before and after study on the primary diversion route to identify if additional traffic and/or safety improvements are needed to mitigate the effects of the toll.

As a result, evaluation of additional mitigation measures to off-set adverse effects and the re-evaluation of disproportionately high and adverse effects on low-income and minority populations is not warranted.

## 7.0 ENVIRONMENTAL COMMITMENTS AND MITIGATION

The mitigation measures summarized in this section shall be incorporated into the project's design documents. In order to track and transfer mitigation commitments through the project development process, Environmental Commitments & Mitigation Tracking System (ECMTS) documentation shall be prepared and submitted through appropriate channels, as the project moves through Final Design and Construction. Impacts and mitigation commitments are based on Preliminary Design and may change as the project moves through Final Design and Construction. Final design information and final mitigation commitments will be included in the ECMTS documentation.

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### STREAMS

**Permanent Stream Impacts:** *79 linear feet*

**Mitigation Remarks:** Stream mitigation details will be determined in final design through consultation with permitting agencies, in accordance with current regulations and practices, and incorporated into the waterway permit application.

### WETLANDS

**Permanent Wetland Impacts:** *0.11 acres*

**Mitigation Remarks:** Replacement wetlands will be provided to compensate for the wetlands impacted by the project. Through consultation with permitting agencies, mitigation details will be determined in Final Design and incorporated into the waterway permit application. Protective fencing will be placed to prohibit contractor access in Wetland 2.

### COMMITMENTS FOR FURTHER PUBLIC INVOLVEMENT

The contractor will continue to coordinate with local municipalities and the public.

### STATE SCENIC RIVERS & STREAMS

Through coordination with PA DCNR, PennDOT agreed to incorporate the following mitigation measures into the project:

- Vegetative buffers around construction staging areas will be provided to the extent practicable.
- Piers will not be allowed in the middle portion of the river.
- Steel girders, if used on the new bridge, will be weathering steel or painted a “natural” color to blend with the setting.
- Where possible, riprap will be covered with soil and vegetation.
- Scenic river signs will be placed at the leading ends of each bridge. Signs will also be placed on the bridge and/or piers for water trail users

### NAVIGABLE WATERWAYS

A preliminary ATON Plan was prepared and submitted to PFBC for review. PFBC approved the ATON Plan on August 16, 2021. The ATON Plan and approval letter are in the project technical files. The ATON Plan shows the proposed causeways and staging, and sign locations in the vicinity of the bridges. Signs will also be placed at boat launches upstream of the project. Additional advanced notification to PFBC, DCNR and the public will be provided. The P3 Development Entity will be responsible for implementation of the ATON plan during construction.

### SOIL EROSION & SEDIMENTATION



- BMPs will be defined and implemented as a component of the erosion and sedimentation plan and waterway encroachment permit.
- The E&S Control Plan will be reviewed by the PADEP and coordination will be conducted to ensure the selected BMPs are adequate for the project.
- The approved E&S Control Plan will be implemented prior to any earth disturbance, during construction.
- Installed BMPs will be inspected and maintained throughout the duration of construction.
- All areas of earth disturbance will be stabilized immediately following completion of earthwork.
- PCSMs will be evaluated in final design and included in the NPDES permit application, if required.

## **VEGETATION**

In accordance with PennDOT's invasive species guidance (Publication 756, 2014), care will be taken not to transplant roots or seeds of noted invasive, non-native plants during earth moving operations. Re-vegetation of impacted areas will be implemented through the E&S plan. Prior to completion of construction, all remaining areas of earth disturbance will be restored by re-seeding with standard PennDOT seed formulas. These seed formulas may contain native plant species; but per Executive Order 13112, will avoid those plant species that are listed on the Noxious Weed Control List.

## **PARKS & RECREATIONAL FACILITIES**

Approximately 6 acres of land will be replaced for permanent and temporary impacts within LGSP to satisfy Section 6(f) and NPS's policies.

LGSP is also protected under Section 4(f) of USDOT Act of 1966. Additional mitigation measures are proposed as mitigation for Section 4(f) uses within LGSP (see Section 4(f) below).

## **HAZARDOUS OR RESIDUAL WASTE SITES**

Phase II and Phase III investigations will be completed for the identified potentially contaminated areas as required. Remediation and mitigation will be determined upon completion of these investigations.

If a design change requires disturbance of soil at 4700 SR 534, a SMP should be developed. The SMP will provide the contractor with instruction and methods to identify and manage unsuitable fill material.

## **SECTION 4(F) RESOURCES**

Mitigation measures for Lehigh Gorge State Park will include:

- Access to and use of the Lehigh Gorge Trail will be maintained to the extent possible during construction:
  - April through October: Overnight closure of the trail (during hours that the state park is closed) will be allowed. Short-term closure of the trail (up to 15 minutes) will be allowed during daylight hours Monday through Friday using flaggers and advance signage.
  - November through March: Long-term closure of the trail will be allowed for certain construction activities including bridge superstructure demolition, pier demolition, pier construction, and setting of beams and deck forms, with consideration of working room and safety.
  - Advance notice of trail closures will be provided to DCNR, D&L National Heritage Corridor, the Borough of White Haven, and the public.

- The contractor may temporarily realign the trail through the project site if approved by DCNR. A 10' wide trail will be maintained except as needed for closures noted above.
- If necessary due to pier locations, reconfiguration of the driveway, parking, or boat launches will be coordinated with DCNR.
- An Aids to Navigation Plan will be implemented and will include advance signing. Signs will be placed at boat launches upstream of the project. Additional advanced notification to PFBC, DCNR, and the public will be provided.
- The contractor will restore disturbed areas, and full access to the park amenities will be provided upon completion. As part of restoring the disturbed areas, a planting plan will be prepared in coordination with DCNR during Final Design.
- PennDOT will coordinate with DCNR and White Haven Borough to provide wayfinding signs.

LGSP is also protected under Section 6(f) of the Land and Water Conservation Fund Act. PennDOT will provide replacement land for Section 6(f) conversion within LGSP totaling approximately 6 acres. This replacement land will also serve as mitigation of the Section 4(f) use of LGSP. The replacement land being provided is forested land; therefore, the replacement land offsets the removal of trees in the construction staging area, and in the area where the existing gabion wall along SR 940 will be replaced.

Mitigation measures for the Lehigh River Water Trail / PA Scenic River will include:

- An Aids to Navigation Plan will be implemented and will include advance signing. Signs will be placed at boat launches upstream of the project. Additional advanced notification to PFBC, DCNR and the public will be provided.
- Temporary closures of boat traffic will be allowed for certain activities, including bridge demolition, construction of piers near or within the river, and setting of beams.
- If necessary due to pier locations, reconfiguration of the driveway, parking, or boat launches will be coordinated with DCNR.
- The contractor will restore disturbed areas, and full access to the park amenities will be provided upon completion. As part of restoring the disturbed areas, a planting plan will be prepared in coordination with DCNR during Final Design.

In addition, to mitigate for the PA Scenic Rivers designation, PennDOT has agreed to incorporate the following actions into the project:

- Vegetative buffers around construction staging areas will be provided to the extent practicable.
- Piers will not be allowed in the middle portion of the river.
- Steel girders, if used on the new bridge, will be weathering steel or painted a "natural" color to blend with the setting.
- Where possible, riprap will be covered with soil and vegetation.
- Scenic river signs will be placed at the leading ends of each bridge. Signs will also be placed on the bridge and/or piers for water trail users. PA Scenic Rivers coordination letters are provided in Appendix E.

#### **NON-RESOURCE SPECIFIC MITIGATION COMMITMENTS**

- Property acquisitions will be conducted in accordance with the Uniform Relocation Assistance and Real Property Acquisitions Policies Act of 1970, as amended; Title VI of the Civil Rights Act of 1964; and the Pennsylvania Eminent Domain Code of 1964.

- While no residential relocations are anticipated, any individual or family displaced by the project would be offered the full extent of benefits and payments.
- Provisions would be made to ensure that any person with a disability who is displaced is offered replacement housing that meets any special needs. Based on current design plans, no displacements are anticipated.
- If the Development Entity/Contractor requires area outside of the Project PSA delineated in this EA, including the Original PSA and Expanded PSA which includes the areas delineated for the proposed traffic improvements, the contractor is required to coordinate with PennDOT to determine necessary NEPA Reevaluation studies and documentation.

This NEPA Reevaluation may include but not be limited to:

- Delineation of aquatic resources in accordance with USACE protocol;
  - Phase I ESA or Environmental Due Diligence (EDD) statement;
  - PNDI review and coordination with resource protection agencies;
  - Section 106 Consultation; and
  - Public outreach.
  - PennDOT will conduct a before/after traffic study to evaluate actual toll diversion volumes and roadway performance to evaluate actual volumes, including truck traffic, compare it to the projections in the I-80 Lehigh River Diversion Route Traffic Evaluation, March 2022, and, if appropriate, identify and evaluate additional mitigation measures.
  - PennDOT will implement a program offering toll-free bridge access to low-income persons qualifying for one or more DHS financial assistance programs. PennDOT will reassess the low-income toll program 5 years after substantial completion of the project. The purpose of this reassessment will be to determine the effectiveness of the low-income toll program for reducing the burden of tolls on low-income households and, depending upon the outcome of the reassessment, identify alternative solutions for reducing the burden on low-income households.
-

**Appendix A**  
**Engineering Information**

## **Project Identification**

**Originating Office:** 05, 04

**Date:** 03/30/21

**Federal Project Number:** TBD

**Township/Municipality:** White Haven Borough, East Side Borough, Kidder Township

**Local Name:** I-80 White Haven Bridges

**Limits of Work (Segment/Offset)**

<b>Start:</b>	<b>End:</b>
2720/1097	2743/1236
2721/2330	2731/2253

**Construction Stations**

<b>Start:</b>	<b>End:</b>
2720/0997	2743/1136
2721/2430	2731/2153

**Total Length:** 8143 EB, 7993 WB ft

**Date of First Federal Authorization for Preliminary Engineering:**

Not Applicable

**Date of Federal Authorization Time Extension(s) for Preliminary Engineering(if applicable):**

Not Applicable

# Design Criteria

Roadway Description: SR 0080-08B

Functional Classification: Freeways/Interstates

Urban  Rural

Current ADT: WB/EB 15805/16797

Design Year No-Build ADT: WB/EB 26050/27686

Current LOS: N/A

Design Year Build ADT: WB/EB 26050/27686

Design Year Build LOS: N/A

DHV: WB/EB 1824/1938

Truck %: 34

D (Directional Distribution) %: 52

Design Speed: 70 mi/h

Posted Speed: 65 mi/h

## Required Minimum Widths

Lane Width: 12 ft

Shoulder Width: 12 Right 8 Left ft

Bridge Curb-to-Curb: 44 ft

Design Exception Required?

Yes  No

The existing stopping sight distance of I-80 Westbound through the SR 940 overpass at the western end of the project is substandard. The proposed I-80 Westbound alignment will approximately match existing, and the stopping sight distance will remain substandard.

Typology: Limited Access Freeway – Rural Interstate

Topography:  Level  Rolling  Mountainous

Proposed Design Criteria: New and Reconstruction

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## Traffic Control Measures

The following traffic control measures will be implemented:

- Temporary Bridge(s)
- Temporary Roadway
- Detour
- Ramp Closure
- Other (specify)
- None

Other Description: Temporary ramp realignment due to traffic staging.

Provisions for access by local traffic will be made and so posted.

True  False

Through-traffic dependent business will not be adversely affected.

True  False

There will be no interference with any local special event or festival.

True  False

There will be no substantial environmental consequences associated with the traffic control measure(s).

True  False

There is no substantial controversy associated with the traffic control measure(s).

True  False

There are no substantial impacts to bicycle or pedestrian routes.

True  False

An alternate, ADA compliant bicycle/pedestrian access route is available.

True  False

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Approximate length of planned detour: 5.3 miles

Detour Map

**Make the selection that best describes the planned detour:**

- Detour will use local roads with no improvements.
- Detour will involve improvements to local roads with no resulting impacts on safety or the environment.
- Detour will involve improvements to local roads and will impact safety and/or the environment.
- Detour will use only state owned roads.

**Describe impacts**

SR 1005 (River Road) will be closed and detoured during construction. The detour route is attached. The P3 Development Entity will coordinate with Weatherly School District officials and maintain school bus access, including turnaround area(s) if needed.

PA Bike Route L travels on SR 1005 (River Road) through the project area. Bike Route L will be detoured during construction. The detour will use SR 940 east to SR 534 south.

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**Estimated Costs**

**Engineering:** \$ 4,900,000

**Right-of-Way:** \$ 1,600,000

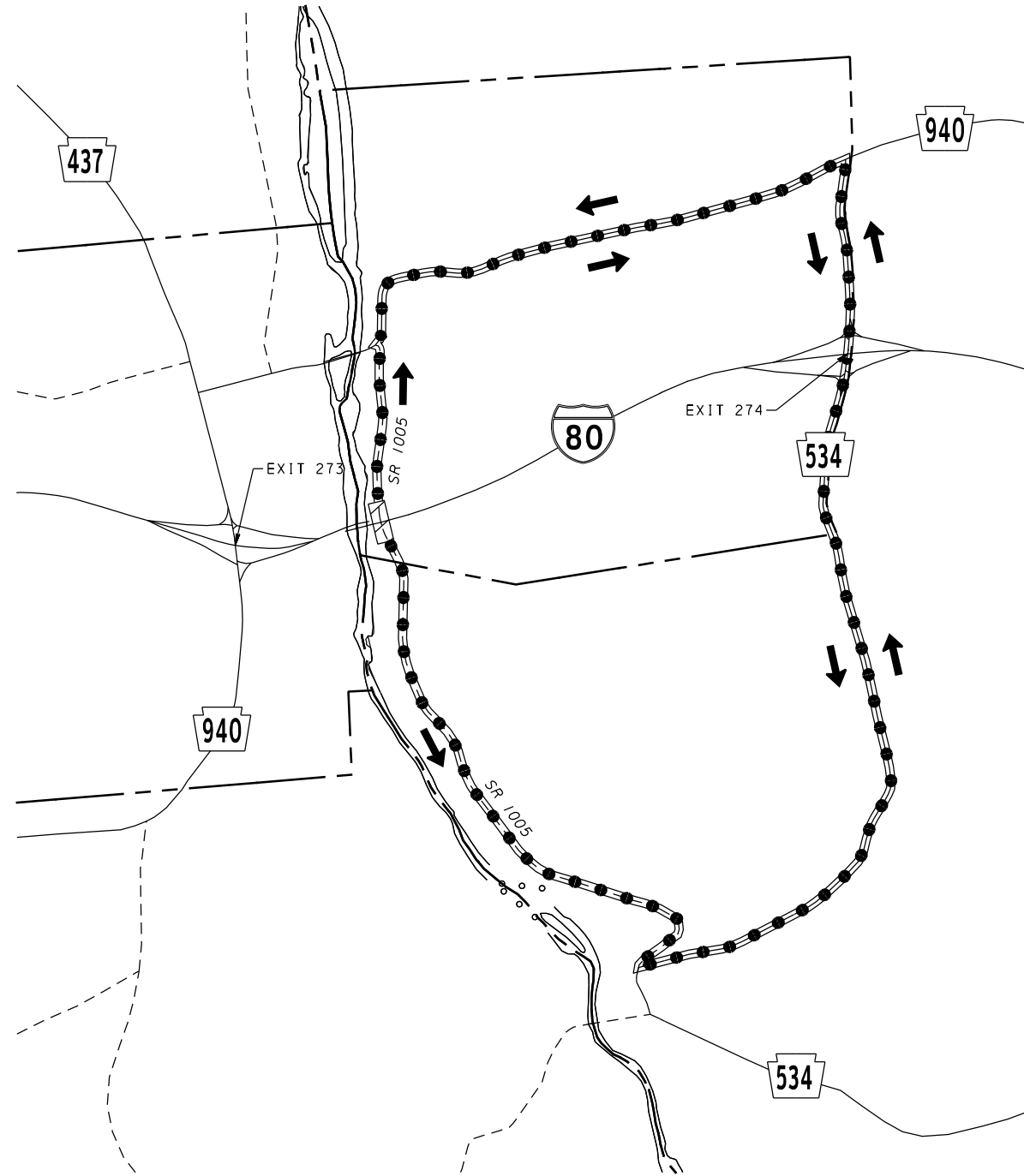
**Construction:** \$ 67,867,000

**Utilities:** \$ 200,000

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\*  
 LUZERNE COUNTY  
 CARBON COUNTY

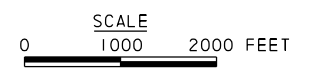
DISTRICT	COUNTY	ROUTE	SECTION	SHEET
5-0	*	0080	08B	59 OF 59
WHITE HAVEN & EAST SIDE BOROUGHES AND KIDDER TOWNSHIP				
REVISION NUMBER	REVISIONS	DATE	BY	



- LEGEND**
- - DETOUR ROUTE
  - ← - DIRECTION OF DETOUR
  - ▨ - CONSTRUCTION
  - - HIGHWAY
  - - - - STATE ROUTE
  - - - - MUNICIPAL BOUNDARY

**NOTE:**  
 THIS DETOUR TO BE IN EFFECT FOR THE ENTIRE DURATION OF CONSTRUCTION ACTIVITIES.

TRAFFIC CONTROL PLAN  
 SR 1005 & BICYCLE ROUTE L CLOSED



USER: AHOLIZAP PLOT DRIVER: PgmDOT\_PDF\_Mono.plt.ctb PLOT DATE: 12-01-2021 12:29:16 PM  
 PATH: c:\pwworking\esri\apps\mapserver\bin\gdal2835\ FILE: 008008B-TC-DTROT.dgn MODEL: traffic plan references



## Roadway

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### Roadway Description

SR 0080-08B

	Existing	Proposed
<b>Number of Lanes:</b>	4	4
<b>Lane Width:</b>	12 ft	12 ft
<b>Shoulder Width:</b>	4 ft	Right/Left 12/8 ft
<b>Median Width:</b>	Varies ft	Varies ft
<b>Sidewalk Width:</b>	N/A ft	N/A ft
<b>Bicycle Lane Width:</b>	N/A ft	N/A ft
<b>Clear Zone Width:</b>	30' fill / 22' cut ft	30' fill / 22' cut ft

---

### Roadway Description

SR 8006 (Ramp B) I-80 WB Exit Ramp to SR 940

	Existing	Proposed
<b>Number of Lanes:</b>	1	1
<b>Lane Width:</b>	14 ft	14 ft
<b>Shoulder Width:</b>	10 outside / 4 inside ft	10 outside / 4 inside ft
<b>Median Width:</b>	N/A ft	N/A ft
<b>Sidewalk Width:</b>	N/A ft	N/A ft
<b>Bicycle Lane Width:</b>	N/A ft	N/A ft
<b>Clear Zone Width:</b>	30' fill / 22' cut ft	30' fill / 22' cut ft

---

### Roadway Description

SR 8006 (Ramp C) - I-80 EB On Ramp from SR 940

	Existing	Proposed
<b>Number of Lanes:</b>	1	1
<b>Lane Width:</b>	14 ft	14 ft
<b>Shoulder Width:</b>	10 outside / 4 inside ft	10 outside / 4 inside ft
<b>Median Width:</b>	N/A ft	N/A ft
<b>Sidewalk Width:</b>	N/A ft	N/A ft
<b>Bicycle Lane Width:</b>	N/A ft	N/A ft
<b>Clear Zone Width:</b>	30' fill / 22' cut ft	30' fill / 22' cut ft

---

### Roadway Description

SR 940 at I-80

	Existing	Proposed
<b>Number of Lanes:</b>	Varies, see Remarks	Varies, see Remarks
<b>Lane Width:</b>	12 ft	12 ft
<b>Shoulder Width:</b>	10 ft	10 ft
<b>Median Width:</b>	14 ft	14 ft
<b>Sidewalk Width:</b>	N/A ft	N/A ft
<b>Bicycle Lane Width:</b>	N/A ft	N/A ft
<b>Clear Zone Width:</b>	14 ft	14 ft

### Remarks

Number of lanes is 4 (2 in each direction) on bridge over I-80 and to the south. Number of lanes north of I-80 into White Haven is 2 lanes (1 in each direction). Outer shoulder on bridge over I-80 is 5'-6".

## Structure

BMS Number: 13-0080-2728-0000

BRKEY: 8972

Description: I-80 Eastbound over RBMN RR, Lehigh Gorge Trail, Lehigh River, and SR 1005 (River Road)

	Existing	Proposed
Structure Type:	Girder-floorbeam-stringer	Multi Steel Plate Girder
Weight Restrictions:	None ton	None ton
Height Restrictions:	None ft	None ft
Curb to Curb Width:	33 & varies ft	56 ft
Lane Width:	12 ft	12 ft
Shoulder Width:	4.5 ft	12 outside / 8 inside ft
Sidewalk Width:	None ft	None ft
Total Bridge Width*:	36.5 and varies ft	59.375 ft

\*Total Bridge Width is measured from outside of barrier to outside of barrier, which should include sidewalks, when present.

Under Clearance:	26.9 ft	29.1 ft
Lateral Clearance:	2 ft	53.1 ft
Sufficiency Rating:	62	
Structure Length:	836 ft	855 ft

### Remarks

Auxiliary lane for on-ramp is carried across the bridge.

BMS Number: 13-0080-2729-0000

BRKEY: 8974

Description: I-80 Eastbound over RBMN RR, Lehigh Gorge Trail, Lehigh River, and SR 1005 (River Road)

	Existing	Proposed
Structure Type:	Girder-floorbeam-stringer	TBD
Weight Restrictions:	None ton	None ton
Height Restrictions:	None ft	None ft
Curb to Curb Width:	33 and varies ft	44 ft
Lane Width:	12 ft	12 ft
Shoulder Width:	4.5 ft	12 outside / 8 inside ft
Sidewalk Width:	None ft	None ft
Total Bridge Width*:	36.5 and varies ft	47.375 ft

\*Total Bridge Width is measured from outside of barrier to outside of barrier, which should include sidewalks, when present.

Under Clearance:	24.5 ft	24.7 ft
Lateral Clearance:	2 ft	36.25 ft
Sufficiency Rating:	63	
Structure Length:	757 ft	770 ft

## Structure

---

**BMS Number:** 13-0080-2730-0300

**BRKEY:** 8973

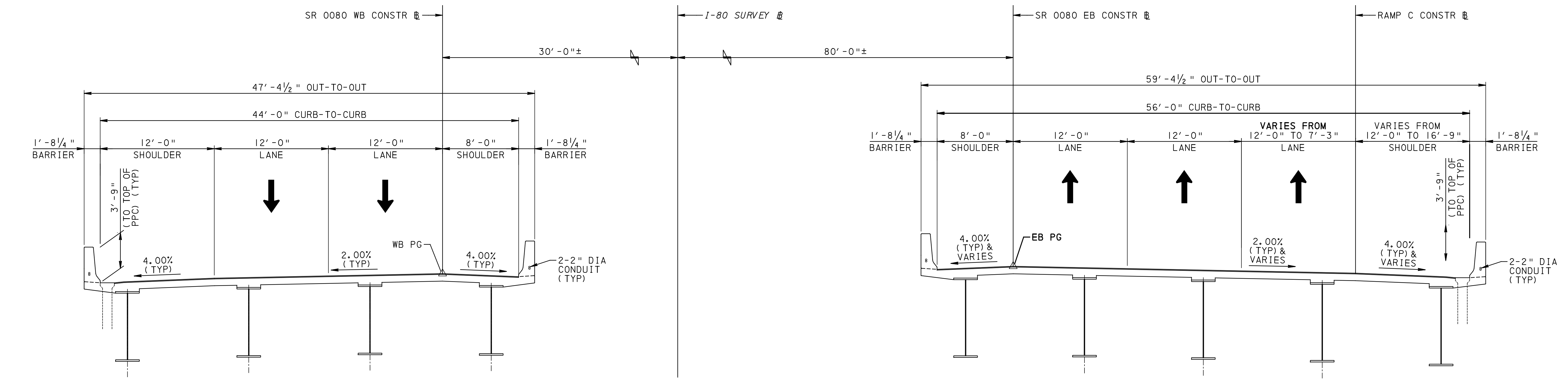
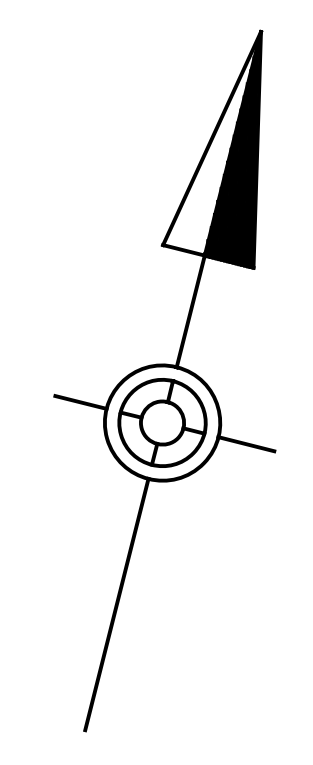
**Description:** I-80 over Tributary to Lehigh River

	<b>Existing</b>	<b>Proposed</b>
<b>Structure Type:</b>	Concrete Box Culvert	Concrete Box Culvert
<b>Weight Restrictions:</b>	None ton	None ton
<b>Height Restrictions:</b>	None ft	None ft
<b>Curb to Curb Width:</b>	N/A under fill ft	N/A under fill ft
<b>Lane Width:</b>	12 ft	12 ft
<b>Shoulder Width:</b>	12 outside / 8 inside ft	12 outside / 8 inside ft
<b>Sidewalk Width:</b>	N/A ft	N/A ft
<b>Total Bridge Width*:</b>	243 ft	274.5 ft
<b>*Total Bridge Width is measured from outside of barrier to outside of barrier, which should include sidewalks, when present.</b>		
<b>Under Clearance:</b>	N/A ft	N/A ft
<b>Lateral Clearance:</b>	N/A ft	N/A ft
<b>Sufficiency Rating:</b>	70	
<b>Structure Length:</b>	16 ft	16 ft

---

**Appendix B**  
**Project Design Exhibit**

# I-80 Over Lehigh River Bridges Eastbound and Westbound Replacement



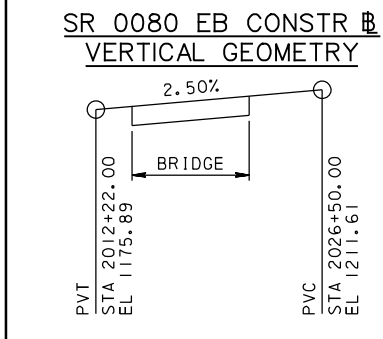
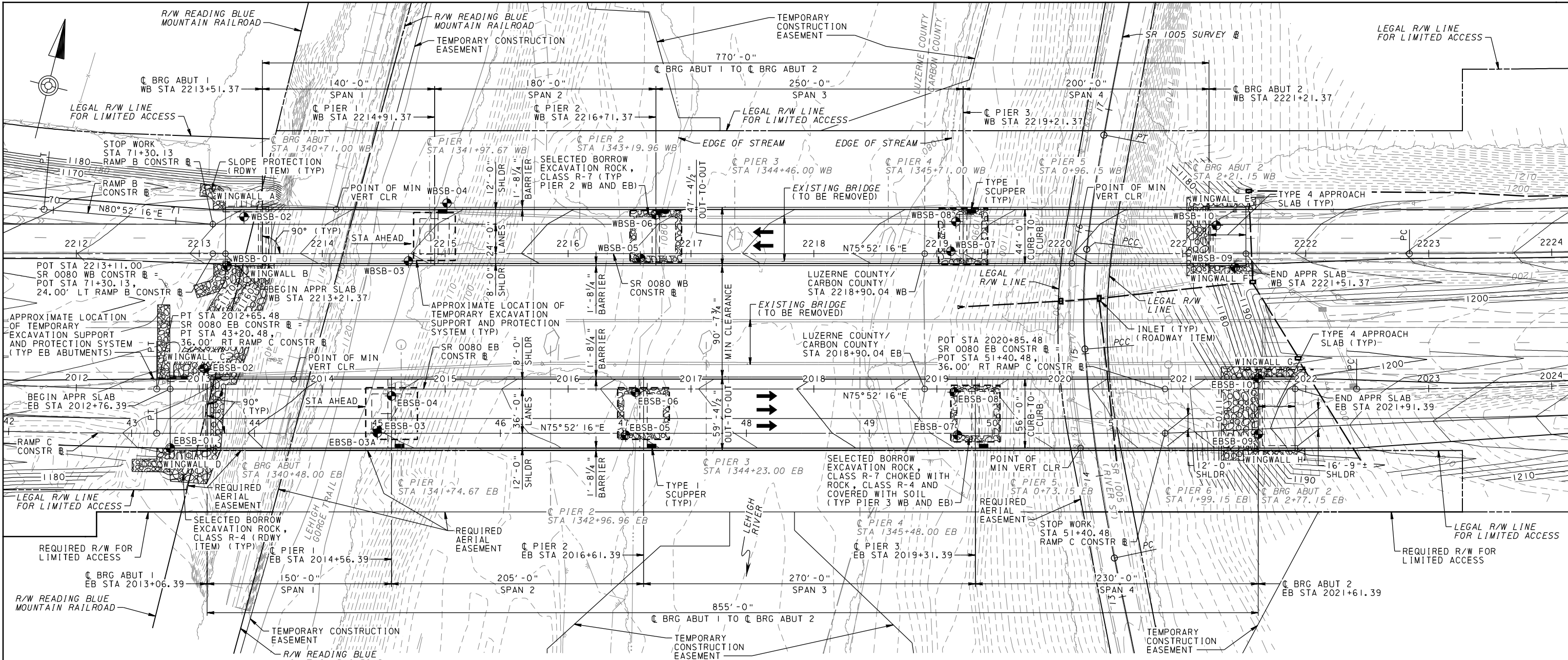
SR 0080 TYPICAL BRIDGE SECTION

## Legend

- Proposed Pavement Edge —
- Proposed Shoulder Edge —
- Property Line —
- Cut Line ---
- Fill Line ---
- Wetland Boundary - - -
- Proposed Structure
- Proposed Pavement
- Number/Direction Lanes ①



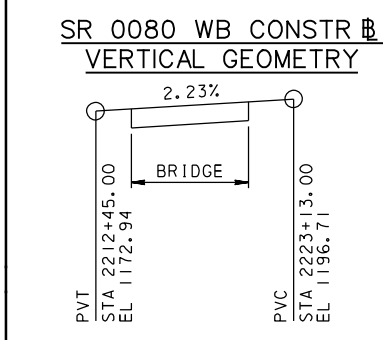
**Appendix C**  
**Preliminary Design Plans**



**HYDRAULIC DATA**

DRAINAGE AREA= 311 SQ MI

EXISTING SR 0080 WB STRUCTURE	PROPOSED SR 0080 WB STRUCTURE
DESIGN FLOOD (50 YEAR): Q = 13,502 CFS VELOCITY= 6.7 FPS WS EL= 1086.64	DESIGN FLOOD (50 YEAR): Q = 13,502 CFS VELOCITY= 7.2 FPS WS EL= 1086.14
100 YEAR FLOOD: Q = 15,373 CFS VELOCITY= 6.9 FPS WS EL= 1087.30	100 YEAR FLOOD: Q = 15,373 CFS VELOCITY= 7.4 FPS WS EL= 1086.79

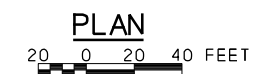


**AS-DRILLED BORING LOCATIONS (WB)**

BORING	STATION	OFFSET (FT)	SURFACE EL (FT)
WBSB-01	2213+22.7	9.7 RT	1174.1
WBSB-02	2213+36.4	30.0 LT	1174.4
WBSB-03	2214+70.2	6.0 RT	1120.0
WBSB-04	2215+01.5	41.1 LT	1119.1
WBSB-05	2216+58.8	3.5 RT	1083.1
WBSB-06	2216+71.4	31.6 LT	1081.8
WBSB-07	2219+11.3	2.0 LT	1084.8
WBSB-08	2219+15.5	26.1 LT	1084.4
WBSB-09	2221+42.4	11.1 RT	1190.8
WBSB-10	2221+27.3	23.0 LT	1192.0

**AS-DRILLED BORING LOCATIONS (EB)**

BORING	STATION	OFFSET (FT)	SURFACE EL (FT)
EBSB-01	2012+76.4	48.0 RT	1166.3
EBSB-02	2013+04.1	16.6 LT	1175.7
EBSB-03	2014+44.8	32.7 RT	1118.4
EBSB-03A	2014+44.8	35.7 RT	1118.4
EBSB-04	2014+56.4	5.6 RT	1118.9
EBSB-05	2016+46.8	37.5 RT	1080.4
EBSB-06	2016+54.8	2.5 RT	1080.8
EBSB-07	2019+16.8	37.5 RT	1089.9
EBSB-08	2019+14.0	2.5 RT	1088.7
EBSB-09	2021+61.4	36.8 RT	1178.3
EBSB-10	2021+61.4	8.8 LT	1185.0



- LEGEND:**
- AS-DRILLED TEST BORINGS
  - EXISTING CONTOUR
  - PROPOSED CONTOUR
  - EDGE OF STREAM
  - WETLAND LINES
  - DIRECTION OF TRAVEL
  - APPROXIMATE LOCATION OF TEMPORARY EXCAVATION SUPPORT AND PROTECTION SYSTEM

- NOTES:**
- FOR GENERAL NOTES AND LIST OF SUPPLEMENTAL DRAWINGS, SEE SHEET 3.
  - FOR TYPICAL SECTION, SEE SHEET 4.
  - FOUNDATION TYPES AND LAYOUTS TO BE DETERMINED FOLLOWING SUBSURFACE INVESTIGATION.
  - THE STATIONING SHOWN FOR EXISTING BRIDGE IS MEASURED ALONG THE EXISTING BR.
  - FOR SR 1005 AND RAMP B AND C VERTICAL GEOMETRY, SEE SHEET 4.
  - FOR SR 0080 HORIZONTAL GEOMETRY, SEE SHEET 4.

Mark	Description	By	Chk'd.	Recm'd.	Date
REVISIONS					

S.R. 0080 PREVIOUSLY KNOWN AS L.R. 1009 MPMS/ECMS: 99552  
BMS STR ID: 13-0080-2728-0000, 13-0080-2729-0000 BRKEY: 8972, 8974

**COMMONWEALTH OF PENNSYLVANIA**  
**DEPARTMENT OF TRANSPORTATION**

**LUZERNE AND CARBON COUNTIES**  
**S.R. 0080 SECTION 08B**

SEG. 2728 OFF. 0000 EB & SEG. 2729 OFF. 0000 WB  
S.R. 0080-08B STA. 2017+33.89 EB & STA. 2217+36.37 WB  
OVER READING BLUE MOUNTAIN & NORTHERN RAILROAD,  
LEHIGH GORGE TRAIL, LEHIGH RIVER, AND S.R. 1005  
DUAL 4-SPAN CONTINUOUS STEEL GIRDER BRIDGES  
**CONCEPTUAL TYPE, SIZE, & LOCATION - PLAN**

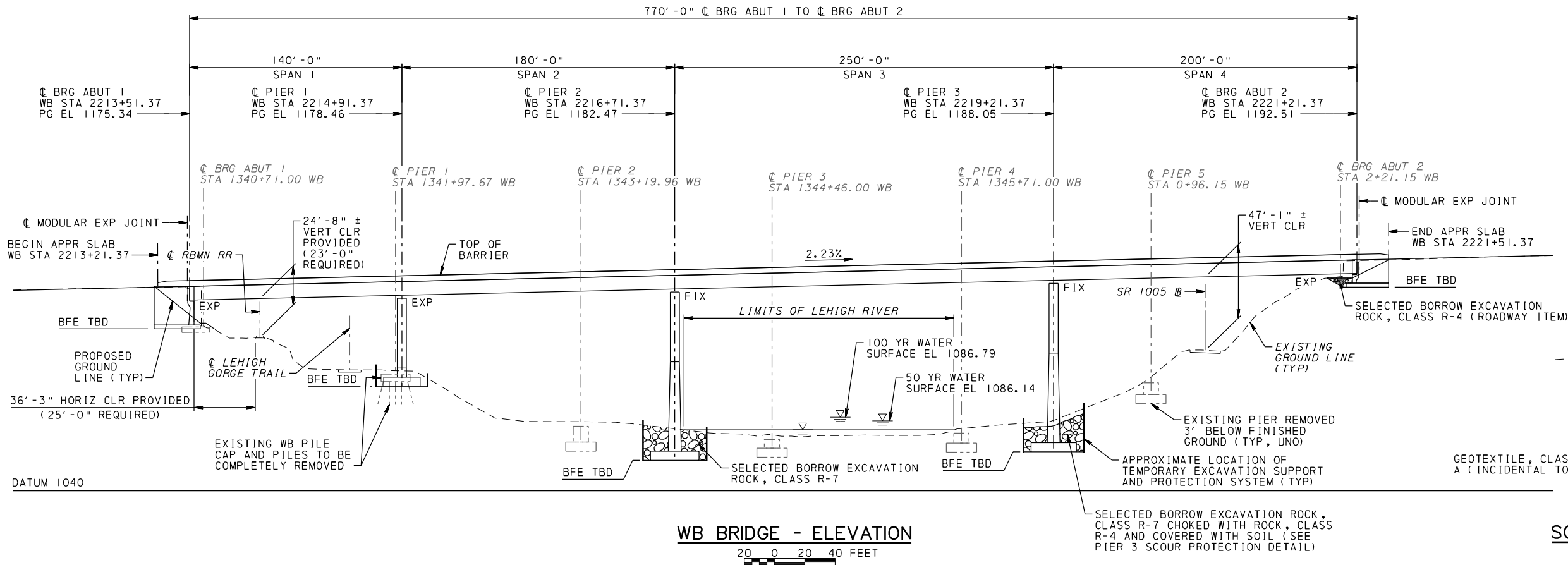
RECOMMENDED \_\_\_\_\_  
DISTRICT BRIDGE ENGINEER

SHEET 1 OF 7  
+SUPPLEMENTAL DRAWINGS  
S-XXXX

PREPARED BY:  
**HDR**  
HDR ENGINEERING, INC.  
3025 CHEMICAL RD  
SUITE 110  
PLYMOUTH MEETING, PA 19462

SIGNATURE & DATE \_\_\_\_\_

USER: AGRLES PLOT: DRIVER: PLOT: 08-24-2021 8:22:47 AM  
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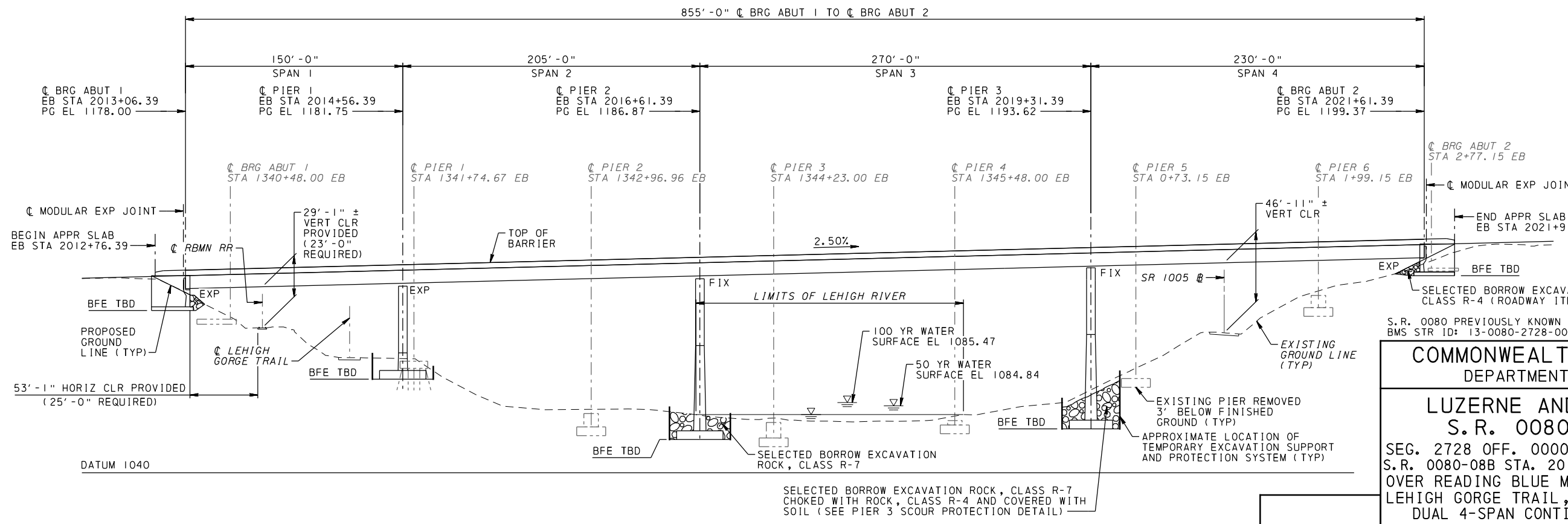


**WB BRIDGE - ELEVATION**  
20 0 20 40 FEET



**PIER 3 SCOUR PROTECTION DETAIL**  
NOT TO SCALE

- NOTES:**
- FOR GENERAL NOTES, SEE SHEET 3.
  - FOR TYPICAL SECTION, SEE SHEET 4.
  - FOUNDATION TYPES AND LAYOUTS TO BE DETERMINED FOLLOWING SUBSURFACE INVESTIGATION.
  - THE STATIONING SHOWN FOR EXISTING BRIDGE IS MEASURED ALONG THE EXISTING  $\mathcal{C}$ .
  - BEARING FIXITY IS CONCEPTUAL. BEARING TYPE, FIXITY AND JOINT MOVEMENT TO BE DETERMINED DURING FINAL DESIGN.



**EB BRIDGE - ELEVATION**  
20 0 20 40 FEET

S.R. 0080 PREVIOUSLY KNOWN AS L.R. 1009 MPMS/ECMS: 99552  
BMS STR ID: 13-0080-2728-0000, 13-0080-2729-0000 BRKEY: 8972, 8974

**COMMONWEALTH OF PENNSYLVANIA**  
DEPARTMENT OF TRANSPORTATION

**LUZERNE AND CARBON COUNTIES**  
**S.R. 0080 SECTION 08B**

SEG. 2728 OFF. 0000 EB & SEG. 2729 OFF. 0000 WB  
S.R. 0080-08B STA. 2017+33.89 EB & STA. 2217+36.37 WB  
OVER READING BLUE MOUNTAIN & NORTHERN RAILROAD,  
LEHIGH GORGE TRAIL, LEHIGH RIVER, AND S.R. 1005  
DUAL 4-SPAN CONTINUOUS STEEL GIRDER BRIDGES  
**CONCEPTUAL TYPE, SIZE, & LOCATION - ELEVATIONS**

USER: AGRLES PLOT DRIVER: P:\p\dot\pdf\memo.plt\c\g MODEL: dde\coul\1  
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 8:06:57 AM



**GENERAL NOTES:**

**DESIGN SPECIFICATIONS**

AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS, 8TH EDITION, 2017, AND AS SUPPLEMENTED BY DESIGN MANUAL, PART 4, DECEMBER 2019.

LIVE LOAD DISTRIBUTION TO BEAMS IS BASED UPON DESIGN MANUAL, PART 4 DISTRIBUTION FACTOR METHOD.

DESIGN IS IN ACCORDANCE WITH THE LRFD METHOD.

**DESIGN LIVE LOADS**

PHL-93, P-82, AND P2016-13

FATIGUE DESIGN IS BASED ON THE FOLLOWING:

STEEL STRUCTURES: ADTT 9,413 (2046) 1-80 EB (ONE-DIRECTIONAL)  
ADTT 8,857 (2046) 1-80 WB (ONE-DIRECTIONAL)

**DEAD LOADS**

INCLUDES A SURFACE AREA DENSITY OF 15 PSF FOR THE POLYESTER POLYMER CONCRETE (PPC) OVERLAY ON THE DECK SLAB.

INCLUDES A SURFACE AREA DENSITY OF 30 PSF FOR FUTURE WEARING SURFACE ON THE DECK SLAB, WITH THE POLYESTER POLYMER CONCRETE (PPC) OVERLAY REMOVED.

INCLUDES A SURFACE AREA DENSITY OF 0.015 KSF FOR PERMANENT METAL DECK FORMS, WHICH TAKES INTO ACCOUNT THE WEIGHT OF THE FORM PLUS THE WEIGHT OF THE CONCRETE IN THE VALLEYS OF THE FORMS.

**GENERAL**

PROVIDE MATERIALS AND PERFORM WORK IN ACCORDANCE WITH SPECIFICATIONS, PUBLICATION 408/2020, AASHTO/AWS D1.5M/D1.5:2008 BRIDGE WELDING CODE, AND THE CONTRACT SPECIAL PROVISIONS. (USE AASHTO/AWS D1.1/D1.1M:2008 FOR WELDING NOT COVERED IN AASHTO/AWS D1.5M/D1.5:2008).

PROVIDE STRUCTURAL STEEL CONFORMING TO AASHTO M 270/M 270M, GRADE 50W (ASTM A709/A 709M, GRADE 50W) DESIGNATION, EXCEPT WHEN NOTED OTHERWISE.

PROVIDE 2" CONCRETE COVER ON REINFORCEMENT BARS, EXCEPT AS NOTED.

USE CLASS AAAP CEMENT CONCRETE IN DECK SLAB.

USE CLASS AA CEMENT CONCRETE IN BARRIERS, TYPE 4 APPROACH SLABS, DRAIN TROUGHS, ABUTMENT BACKWALLS, CHEEKWALLS, AND SLEEPER SLABS.

USE CLASS A CEMENT CONCRETE IN PILE CAPS, ABUTMENTS BELOW BRIDGE SEAT, PEDESTALS, WINGWALLS, FOOTINGS AND PIERS.

USE CLASS C CEMENT CONCRETE BELOW THE BOTTOMS OF FOOTINGS WHEN SPECIFIED.

A HIGHER CLASS CONCRETE MAY BE SUBSTITUTED FOR A LOWER CLASS CONCRETE AT NO ADDITIONAL COST TO THE DEPARTMENT, IF APPROVED BY THE DISTRICT BRIDGE ENGINEER.

PROVIDE GRADE 60 REINFORCING STEEL BARS THAT MEET THE REQUIREMENTS OF ASTM A615/A 615M, A996/A996 M, OR A706/A 706 M. DO NOT WELD GRADE 60 REINFORCING STEEL BARS UNLESS SPECIFIED. GRADE 40 REINFORCING STEEL BARS MAY BE SUBSTITUTED WITH A PROPORTIONAL INCREASE IN CROSS-SECTIONAL AREA, IF APPROVED BY THE CHIEF BRIDGE ENGINEER. DO NOT USE RAIL STEEL A996 REINFORCEMENT BARS IN BRIDGE PIERS, ABUTMENTS, FOOTINGS, BARRIERS OR WHERE BENDING OR WELDING OF THE REINFORCEMENT BARS IS INDICATED.

USE EPOXY-COATED REINFORCEMENT BARS IN THE DECK SLAB, BARRIERS, ABUTMENT BACKWALLS, U-WINGS ABOVE THE CONSTRUCTION JOINT, AND ABUTMENT SEAT BARS. EPOXY-COAT J-BAR REINFORCEMENT IN SUBSTRUCTURE UNITS AND EPOXY-COAT OTHER SUBSTRUCTURE REINFORCEMENT BARS AS INDICATED.

GALVANIZED REINFORCING STEEL BARS MAY BE SUBSTITUTED FOR EPOXY-COATED REINFORCING STEEL BARS AT NO ADDITIONAL COST TO THE DEPARTMENT.

RAKE-FINISH ALL HORIZONTAL CONSTRUCTION JOINTS, EXCEPT AS INDICATED.

SITE CLASS IS NOT CLASS E.

VERIFY ALL DIMENSIONS AND GEOMETRY OF THE EXISTING STRUCTURE IN THE FIELD AS NECESSARY FOR PROPER FIT OF THE PROPOSED CONSTRUCTION.

CONSTRUCT DECK SLAB TRANSVERSE CONSTRUCTION JOINTS PARALLEL TO BRIDGE CENTERLINE OF BEARINGS.

CHAMFER EXPOSED CONCRETE EDGES 3/4 IN BY 3/4 IN, EXCEPT AS NOTED.

ALL DIMENSIONS SHOWN ARE HORIZONTAL, EXCEPT AS NOTED.

USE EITHER PERMANENT METAL FORMS OR REMOVABLE FORMS TO CONSTRUCT THE DECK SLAB.

DECK SLAB THICKNESS INCLUDES A 1/2" INTEGRAL WEARING SURFACE.

SUBSTRUCTURE DIMENSIONS SHOWN ARE FOR A NORMAL TEMPERATURE OF 68° F.

PROVIDE MINIMUM EMBEDMENT AND SPLICE LENGTHS IN ACCORDANCE WITH STANDARD DRAWING BC-736M, UNLESS OTHERWISE INDICATED.

PREPARE BEARING AREAS AS SPECIFIED IN PUBLICATION 408, SECTION 1001.3(K)9.

PROTECTIVE COATING FOR REINFORCED CONCRETE SURFACES (PENETRATING SEALERS, BRIDGE SUPERSTRUCTURE): APPLY PENETRATING SEALER TO THE TOP OF THE BRIDGE DECK, AND TOPS AND INSIDE FACES OF BARRIERS.

WELDING OF REINFORCEMENT BARS DURING FABRICATION OR CONSTRUCTION IS NOT PERMITTED UNLESS SPECIFIED.

**GENERAL (CONTINUED)**

NOTIFY THE REGIONAL HEADQUARTERS OF THE FISH COMMISSION PRIOR TO CONSTRUCTION AND COOPERATE WITH FISH COMMISSION DURING CONSTRUCTION.

WATERWAY CONSERVATION OFFICER  
5566 MAIN ROAD  
SWEET VALLEY, PA 18656  
(570) 477-5717

IN-STREAM RESTRICTIONS FROM OCTOBER 1 THROUGH DECEMBER 31 (WILD TROUT) AND MARCH 1 THROUGH JUNE 15 (STOCKED TROUT) WILL APPLY TO THIS PROJECT.

**UTILITY NOTES**

COORDINATE, LOCATE, AND CONDUCT ALL WORK RELATED TO PUBLIC AND PRIVATE UTILITIES IN ACCORDANCE WITH PUBLICATION 408 SECTIONS 105.06 AND 107.12.

**STEEL GIRDERS**

IF GIRDERS CANNOT BE SHIPPED IN THE LENGTHS SHOWN ON THE PLANS, FIELD SPLICE(S) WILL BE PERMITTED AT THE REQUEST OF THE CONTRACTOR, BUT NO COMPENSATION WILL BE ALLOWED FOR THE SPLICES.

IF GIRDERS CAN BE FABRICATED IN LENGTHS LONGER THAN THE SECTIONS SHOWN ON THE PLANS BY ELIMINATING FIELD SPLICES, FIELD SPLICE(S) MAY BE OMITTED AT THE REQUEST OF THE CONTRACTOR. THE CONTRACTOR ASSUMES FULL RESPONSIBILITY FOR SECURING A HAULING PERMIT. APPROVAL FOR ELIMINATION OF A FIELD SPLICE AT THE SHOP DRAWING STAGE DOES NOT OBLIGATE THE DEPARTMENT TO ISSUE A HAULING PERMIT.

DO NOT USE FORM SUPPORT SYSTEMS THAT WILL CAUSE UNACCEPTABLE OVERSTRESS OR DEFORMATION TO PERMANENT BRIDGE MEMBERS.

ALL FASTENERS ARE ASTM F3125 GRADE A325, TYPE 3 HIGH STRENGTH BOLTS, UNO.

REAM SUBDRILLED OR SUBPUNCHED HOLES FOR FIELD SPLICES IN THE FABRICATION SHOP.

INSTALL SHEAR CONNECTORS IN THE FIELD AFTER INSTALLING THE DECK FORMS.

STABILITY OF PARTIAL GIRDERS AND COMPLETE GIRDERS IS TO BE MAINTAINED BY THE CONTRACTOR DURING ERECTION, UNTIL ALL GIRDERS AND DIAPHRAGMS ARE IN-PLACE AND ALL BOLTS ARE PROPERLY INSTALLED. ERECTION LOADS INCLUDING SELF WEIGHT OF THE STEEL MEMBERS, WIND LOADING AND CONSTRUCTION LIVE LOAD EFFECTS ARE TO BE EVALUATED BY THE CONTRACTOR FOR STABILITY, STRESSES, AND DEFLECTIONS ON THE STEEL MEMBERS DURING ANY STAGE OF ERECTION.

DO NOT MAKE WELDS BY MANUAL SHIELDED METAL ARC PROCESS FOR PRIMARY GIRDER WELDS, SUCH AS FLANGE-TO-WEB WELDS OR FOR SHOP SPLICES OF WEBS AND FLANGES.

DO NOT WELD PERMANENT METAL DECK FORMS OR OTHER ATTACHMENTS TO GIRDER TOP FLANGES IN TENSION AREAS. THREADED STUDS FOR THE SUPPORT OF THE OVERHANG DECK FORMING BRACKET IS PERMITTED PROVIDED THE THREADED STUD IS ATTACHED WITH THE SAME WELDING PROCESSING AS THE SHEAR STUDS.

PROVIDE WELDED STUD SHEAR CONNECTORS MANUFACTURED FROM STEEL CONFORMING TO ASTM A108.

THE STEEL SUPERSTRUCTURE SHALL BE DETAILED AND FABRICATED FOR TOTAL DEAD LOAD FIT (TDLF). GIRDER WEBS SHALL BE PLUMB UNDER THE FULL DEAD LOAD EXISTING AT THE END OF CONSTRUCTION.

SET ANCHOR BOLTS TO TEMPLATE OR IN PREFORMED HOLES. DO NOT DRILL UNLESS SPECIFICALLY INDICATED ON PLANS. FILL THE PREFORMED HOLES WITH NON-SHRINK GROUT. FILL THE CLEARANCE BETWEEN ANCHOR BOLTS AND HOLES IN MASONRY PLATES WITH APPROVED NON-HARDENING CAULKING COMPOUND CONFORMING TO PUBLICATION 408, SECTION 705.8.

DO NOT FIELD-WELD ON ANY PART OF THE EXISTING BRIDGE, EXCEPT WHERE SHOWN ON THE DRAWINGS, WITHOUT PRIOR APPROVAL OF THE REPRESENTATIVE.

BRACE STEEL FRAMEWORK IN LONGITUDINAL AND LATERAL DIRECTIONS UNTIL MEMBERS ARE IN STABLE (FINAL BRACED) CONDITION.

PROVIDE ADDITIONAL TEMPORARY BRACING BETWEEN THE FASCIA AND THE FIRST INTERIOR BEAM IF THE FASCIA BEAM COULD ROTATE, IN THE OPINION OF THE REPRESENTATIVE, WHEN PLACING DECK CONCRETE.

PERFORM CHARPY V-NOTCH TESTS AS SPECIFIED AS PER PUBLICATION 408, SECTION 1105.02(A)5.

PAINT ALL STEEL WITHIN 12 FEET OF THE CENTER LINE OF BEARING AT EACH ABUTMENT IN ACCORDANCE WITH PUBLICATION 408, SECTION 1060.

**NOTES FOR EXISTING PLANS**

DO NOT CONSIDER ANY OF THE DATA ON THE EXISTING STRUCTURE SUPPLIED IN THE ORIGINAL DESIGN DRAWINGS OR MADE AVAILABLE BY THE DEPARTMENT OR ITS AUTHORIZED AGENTS AS POSITIVE REPRESENTATIONS OF ANY OF THE CONDITIONS THAT WILL BE ENCOUNTERED IN THE FIELD.

THE INFORMATION SHOWN ON THE PLANS FOR THE EXISTING BRIDGES IS NOT PART OF THE PLANS, PROPOSAL, OR CONTRACT AND IS NOT TO BE CONSIDERED AS A BASIS FOR COMPUTATION OF THE UNIT PRICES USED FOR BIDDING PURPOSES. THERE IS NO EXPRESSED OR IMPLIED AGREEMENT THAT INFORMATION IS CORRECTLY SHOWN. THE BIDDER IS NOT TO RELY ON THIS INFORMATION, BUT IS TO ASSUME THE POSSIBILITY THAT CONDITIONS AFFECTING THE COST AND/OR QUANTITIES OF WORK TO BE PERFORMED MAY DIFFER FROM THOSE INDICATED. (ORIGINAL PLANS S-5849 - 1964 AND REHABILITATION PLANS S-14076 - 1991).

THE EXISTING BRIDGE STRUCTURAL MEMBERS ARE ASSUMED TO CONTAIN LEAD PAINT AND OTHER TOXIC MATERIALS.

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 PATH: c:\pwworking\info\11\11\1944488\ FILE: 008008B-BR-NOTI.dgn MODEL: 08.fou1.t

CLASSIFICATION OF EARTHWORK AND STRUCTURES	RC-11M	6/1/2010
BACKFILL AT STRUCTURES	RC-12M	2/8/2019
SUBSURFACE DRAINS	RC-30M	12/17/2019
ENDWALLS	RC-31M	6/1/2010
ELECTRICAL DETAILS	BC-721M	2/19/2021
PERMANENT METAL DECK FORMS	BC-732M	1/31/2019
ANCHOR SYSTEMS	BC-734M	2/19/2021
WALL CONSTRUCTION AND EXPANSION JOINT DETAILS	BC-735M	9/30/2016
REINFORCEMENT BAR FABRICATION DETAILS	BC-736M	1/31/2019
BRIDGE DRAINAGE	BC-751M	1/31/2019
CONCRETE DECK SLAB DETAILS	BC-752M	2/19/2021
STEEL GIRDER DETAILS	BC-753M	1/31/2019
STEEL DIAPHRAGMS FOR STEEL BEAM/GIRDER STRUCTURES (STRAIGHT GIRDERS ONLY)	BC-754M	1/31/2019
TYPICAL WATERPROOFING AND EXPANSION DETAILS	BC-788M	1/31/2019
DESCRIPTION	DWG. NO.	APP. DATE
SUPPLEMENTAL DRAWINGS		

Mark	Description	By	Chk'd.	Recm'd.	Date
REVISIONS					

S.R. 0080 PREVIOUSLY KNOWN AS L.R. 1009 MPMS/ECMS: 99552  
BMS STR ID: 13-0080-2728-0000, 13-0080-2729-0000 BRKEY: 8972, 8974

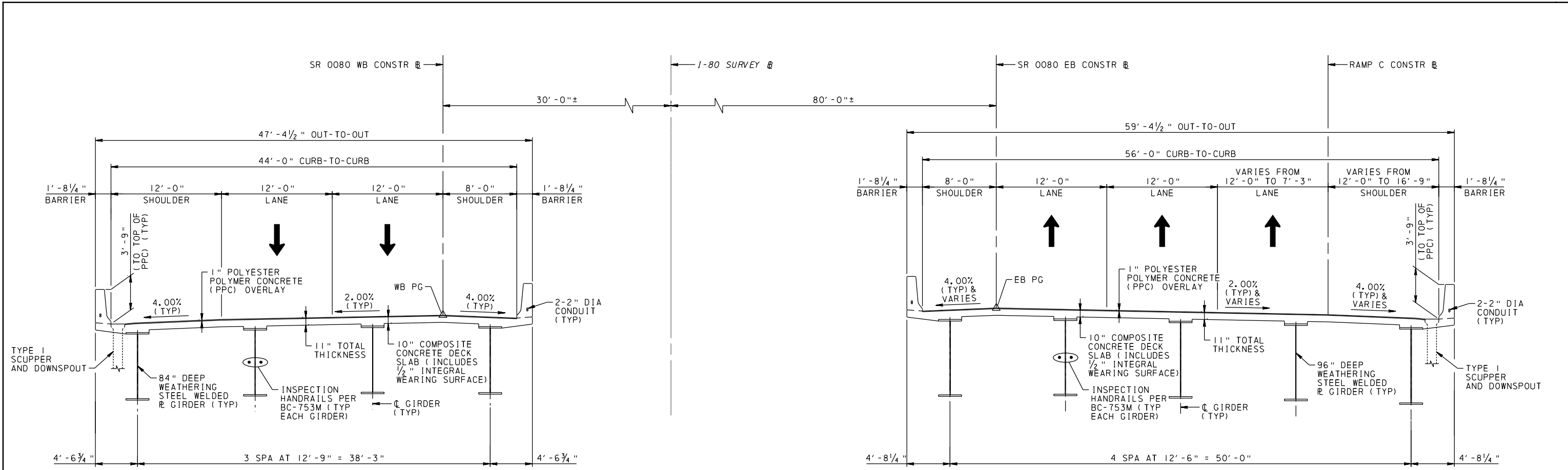
**COMMONWEALTH OF PENNSYLVANIA**  
DEPARTMENT OF TRANSPORTATION

**LUZERNE AND CARBON COUNTIES**  
**S.R. 0080 SECTION 08B**

SEG. 2728 OFF. 0000 EB & SEG. 2729 OFF. 0000 WB  
S.R. 0080-08B STA. 2017+33.89 EB & STA. 2217+36.37 WB  
OVER READING BLUE MOUNTAIN & NORTHERN RAILROAD,  
LEHIGH GORGE TRAIL, LEHIGH RIVER, AND S.R. 1005  
DUAL 4-SPAN CONTINUOUS STEEL GIRDER BRIDGES

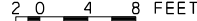
**GENERAL NOTES**

RECOMMENDED _____	SHEET <u>3</u> OF <u>7</u>
S-XXXXX	



**PROPOSED TYPICAL SECTION**

(LOOKING STATIONS AHEAD)  
2 0 4 8 FEET



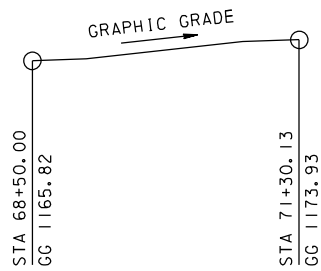
**SR 0080 WB  
HORIZONTAL GEOMETRY  
WEST APPROACH**

PI STA 2207+12.22  
 $\Delta = 23^\circ 19' 06''$  LT  
 T = 397.22'  
 L = 783.44'  
 R = 1925.00'  
 E = 40.56'  
 PCC STA 2203+15.00  
 PT STA 2210+98.44  
 SUPERELEVATE 8.0%  
 DESIGN SPEED 70 MPH

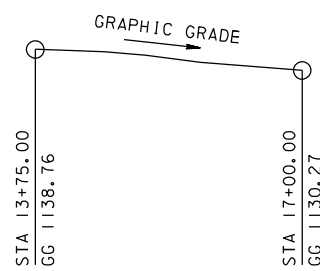
**SR 0080 EB  
HORIZONTAL GEOMETRY  
WEST APPROACH**

PI STA 2010+35.80  
 $\Delta = 12^\circ 38' 39''$  LT  
 T = 231.55'  
 L = 461.23'  
 R = 2090.00'  
 E = 12.79'  
 PCC STA 2008+04.25  
 PT STA 2012+65.48  
 SUPERELEVATE 7.8%  
 DESIGN SPEED 70 MPH

**SR 0080 RAMP B  
VERTICAL GEOMETRY**



**SR 1005  
VERTICAL GEOMETRY**



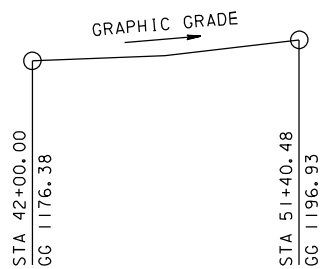
**SR 0080 WB  
HORIZONTAL GEOMETRY  
EAST APPROACH**

PI STA 2224+58.96  
 $\Delta = 3^\circ 05' 48''$  LT  
 T = 174.61'  
 L = 349.13'  
 R = 6460.00'  
 E = 2.36'  
 PC STA 2222+84.35  
 PCC STA 2226+33.48  
 SUPERELEVATE 3.2%  
 DESIGN SPEED 70 MPH

**SR 0080 EB  
HORIZONTAL GEOMETRY  
EAST APPROACH**

PI STA 2025+76.65  
 $\Delta = 7^\circ 35' 47''$  LT  
 T = 335.26'  
 L = 669.53'  
 R = 5050.00'  
 E = 11.12'  
 PC STA 2022+41.39  
 PT STA 2029+10.92  
 SUPERELEVATE 4.0%  
 DESIGN SPEED 70 MPH

**SR 0080 RAMP C  
VERTICAL GEOMETRY**



**NOTE:**

1. FOR GENERAL NOTES, SEE SHEET 3.

Mark	Description	By	Chk'd.	Recm'd.	Date
REVISIONS					

S.R. 0080 PREVIOUSLY KNOWN AS L.R. 1009 MPMS/ECMS: 99552  
 BMS STR ID: 13-0080-2728-0000, 13-0080-2729-0000 BRKEY: 8972, 8974

**COMMONWEALTH OF PENNSYLVANIA  
DEPARTMENT OF TRANSPORTATION**

**LUZERNE AND CARBON COUNTIES  
S.R. 0080 SECTION 08B**

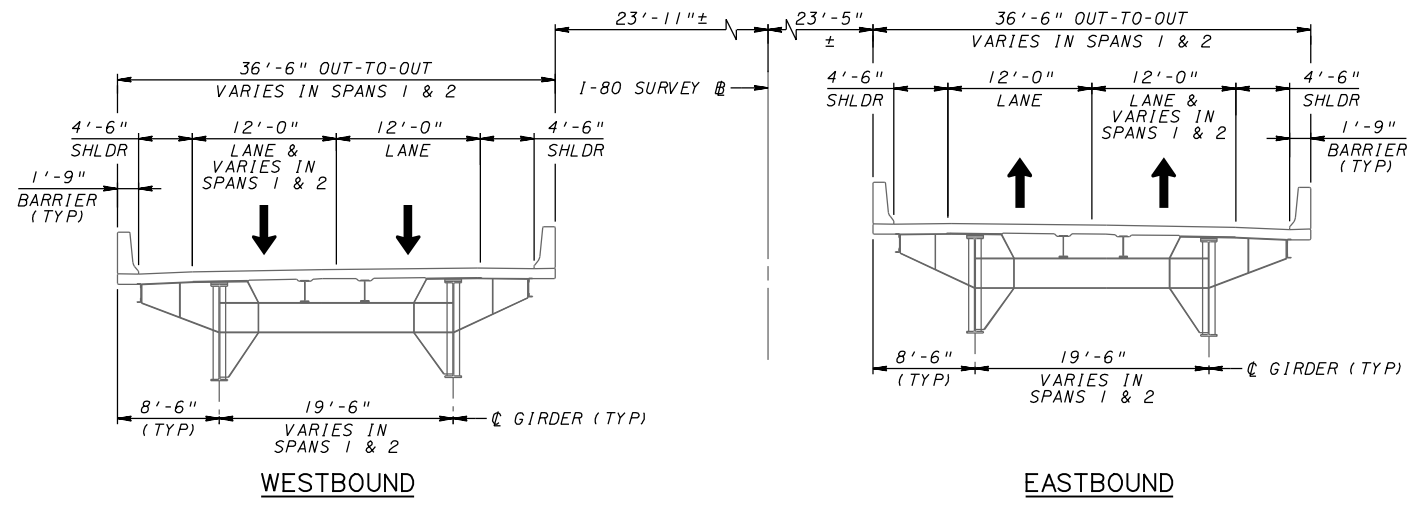
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 S.R. 0080-08B STA. 2017+33.89 EB & STA. 2217+36.37 WB  
 OVER READING BLUE MOUNTAIN & NORTHERN RAILROAD,  
 LEHIGH GORGE TRAIL, LEHIGH RIVER, AND S.R. 1005  
 DUAL 4-SPAN CONTINUOUS STEEL GIRDER BRIDGES

**TYPICAL SECTION**

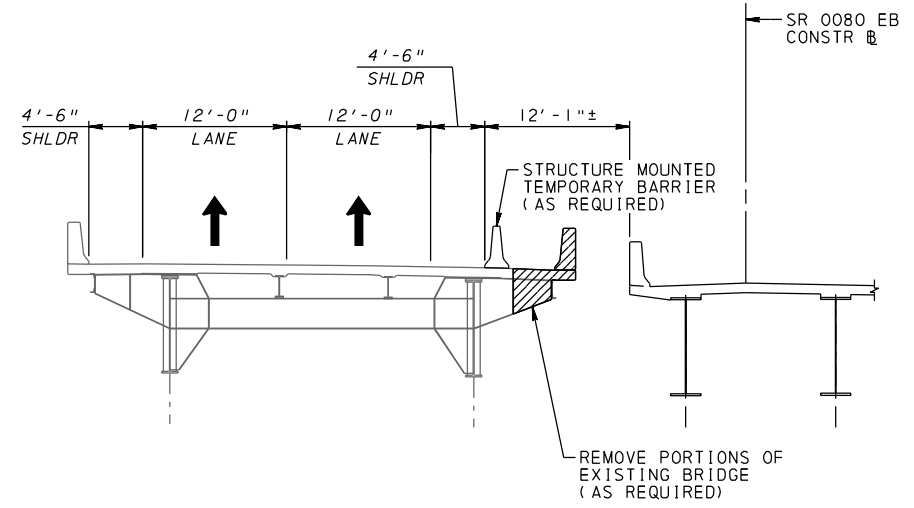
RECOMMENDED \_\_\_\_\_

SHEET 4 OF 7

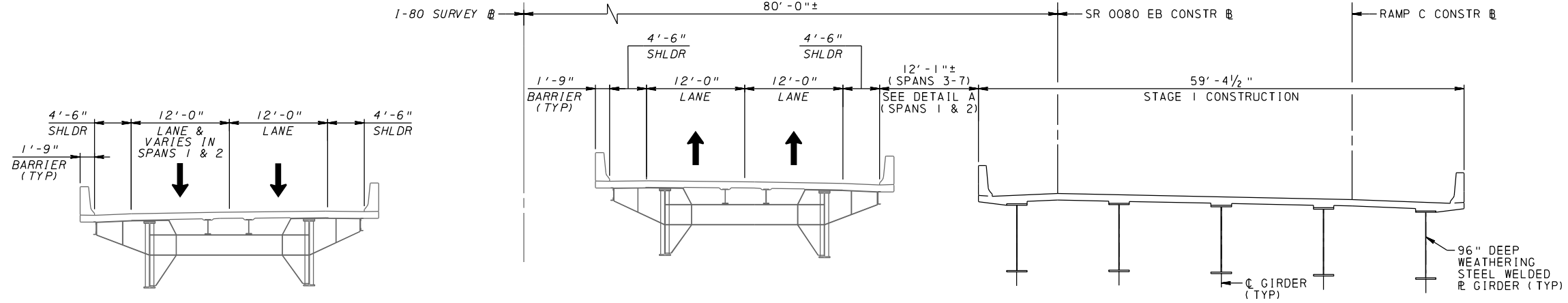
S-XXXXX



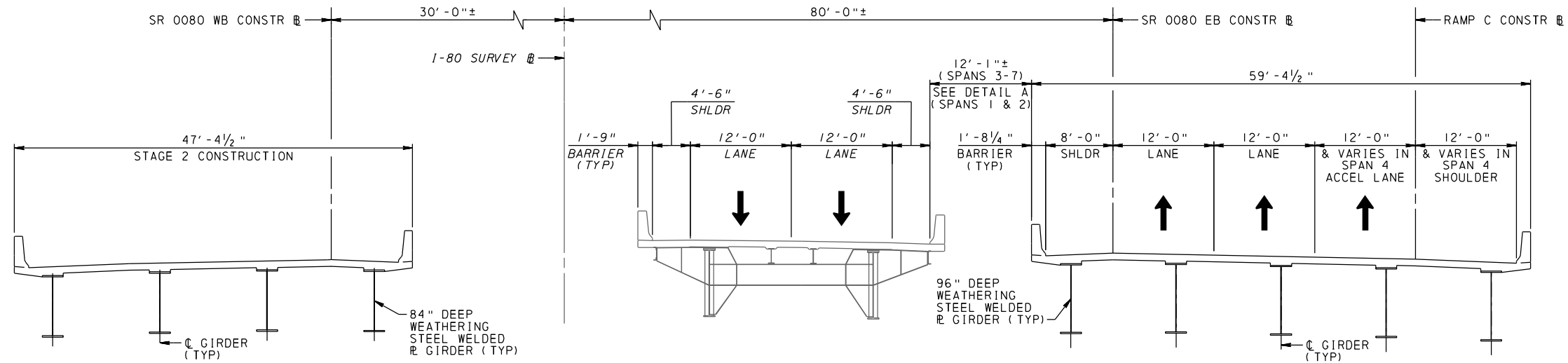
**EXISTING**  
(LOOKING STATIONS AHEAD)  
2 0 4 8 FEET



**DETAIL A**  
(SPANS 1 & 2 ONLY)  
2 0 4 8 FEET



**STAGE 1 CONSTRUCTION**  
(LOOKING STATIONS AHEAD)  
2 0 4 8 FEET



**STAGE 2 CONSTRUCTION**  
(LOOKING STATIONS AHEAD)  
2 0 4 8 FEET

**NOTE:**  
1. FOR GENERAL NOTES, SEE SHEET 3.

Mark	Description	By	Chk'd.	Recm'd.	Date
REVISIONS					

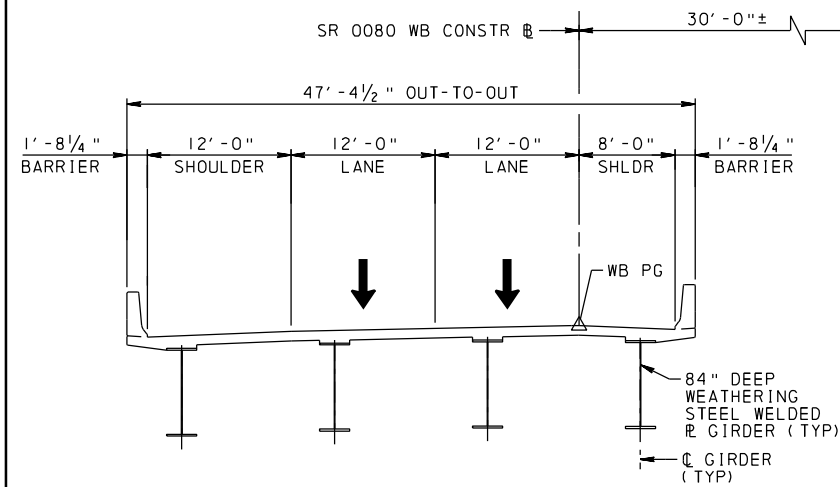
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BMS STR ID: 13-0080-2728-0000, 13-0080-2729-0000 BRKEY: 8972, 8974

**COMMONWEALTH OF PENNSYLVANIA**  
DEPARTMENT OF TRANSPORTATION

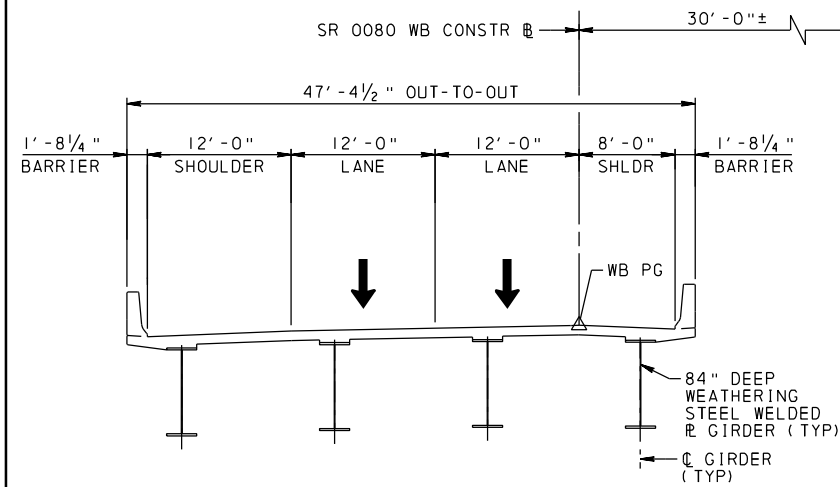
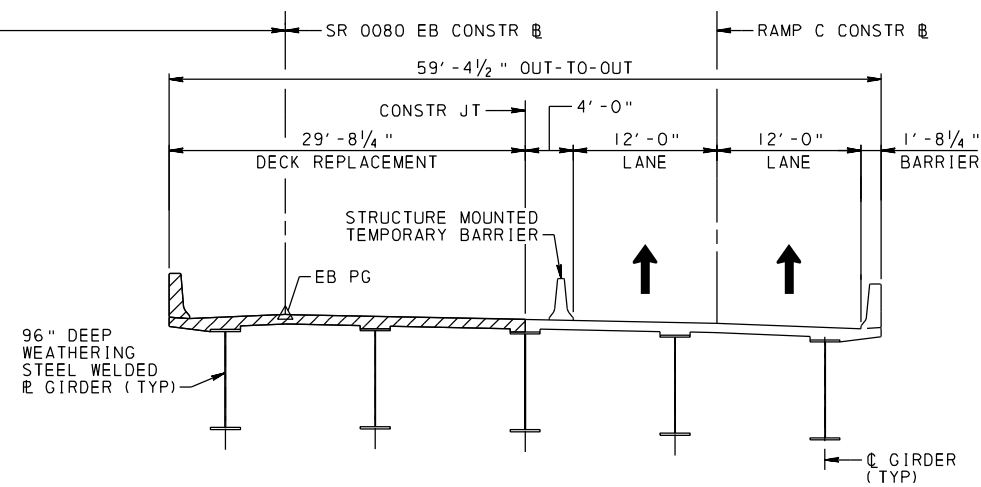
**LUZERNE AND CARBON COUNTIES**  
**S.R. 0080 SECTION 08B**

SEG. 2728 OFF. 0000 EB & SEG. 2729 OFF. 0000 WB  
S.R. 0080-08B STA. 2017+33.89 EB & STA. 2217+36.37 WB  
OVER READING BLUE MOUNTAIN & NORTHERN RAILROAD,  
LEHIGH GORGE TRAIL, LEHIGH RIVER, AND S.R. 1005  
DUAL 4-SPAN CONTINUOUS STEEL GIRDER BRIDGES  
**CONSTRUCTION STAGING**

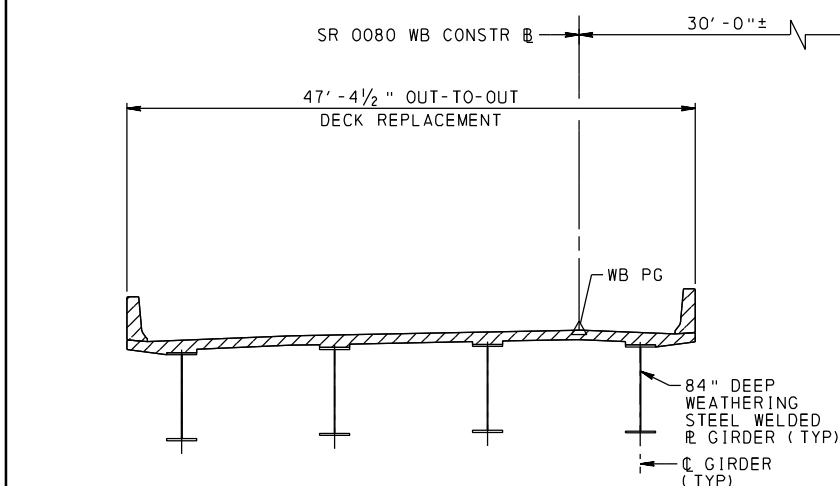
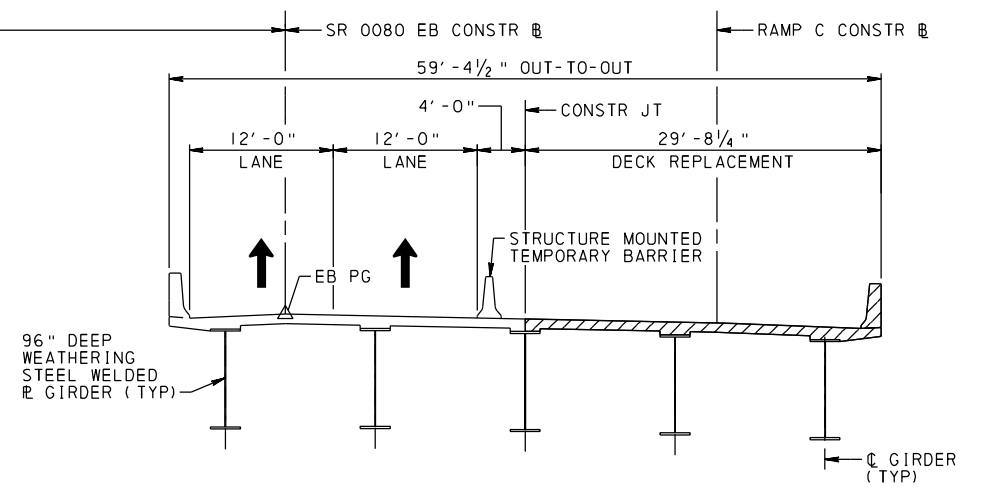
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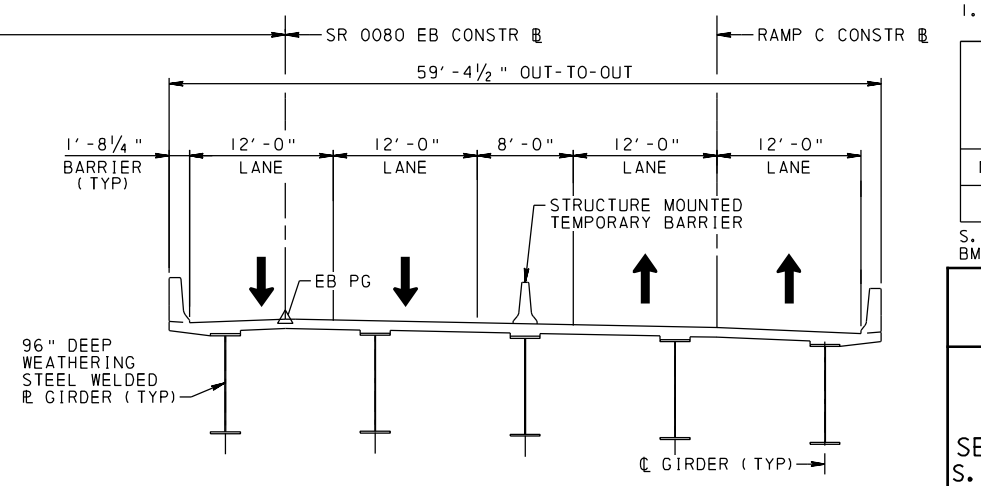
PHASE 1



PHASE 2



PHASE 3



**FUTURE DECK REPLACEMENT**

(LOOKING STATIONS AHEAD)  
2 0 4 8 FEET

**NOTE:**

1. FOR GENERAL NOTES, SEE SHEET 3.

Mark	Description	By	Chk'd.	Recm'd.	Date
REVISIONS					

S.R. 0080 PREVIOUSLY KNOWN AS L.R. 1009 MPMS/ECMS: 99552  
BMS STR ID: 13-0080-2728-0000, 13-0080-2729-0000 BRKEY: 8972, 8974

**COMMONWEALTH OF PENNSYLVANIA  
DEPARTMENT OF TRANSPORTATION**

**LUZERNE AND CARBON COUNTIES  
S.R. 0080 SECTION 08B**

SEG. 2728 OFF. 0000 EB & SEG. 2729 OFF. 0000 WB  
S.R. 0080-08B STA. 2017+33.89 EB & STA. 2217+36.37 WB  
OVER READING BLUE MOUNTAIN & NORTHERN RAILROAD,  
LEHIGH GORGE TRAIL, LEHIGH RIVER, AND S.R. 1005  
DUAL 4-SPAN CONTINUOUS STEEL GIRDER BRIDGES

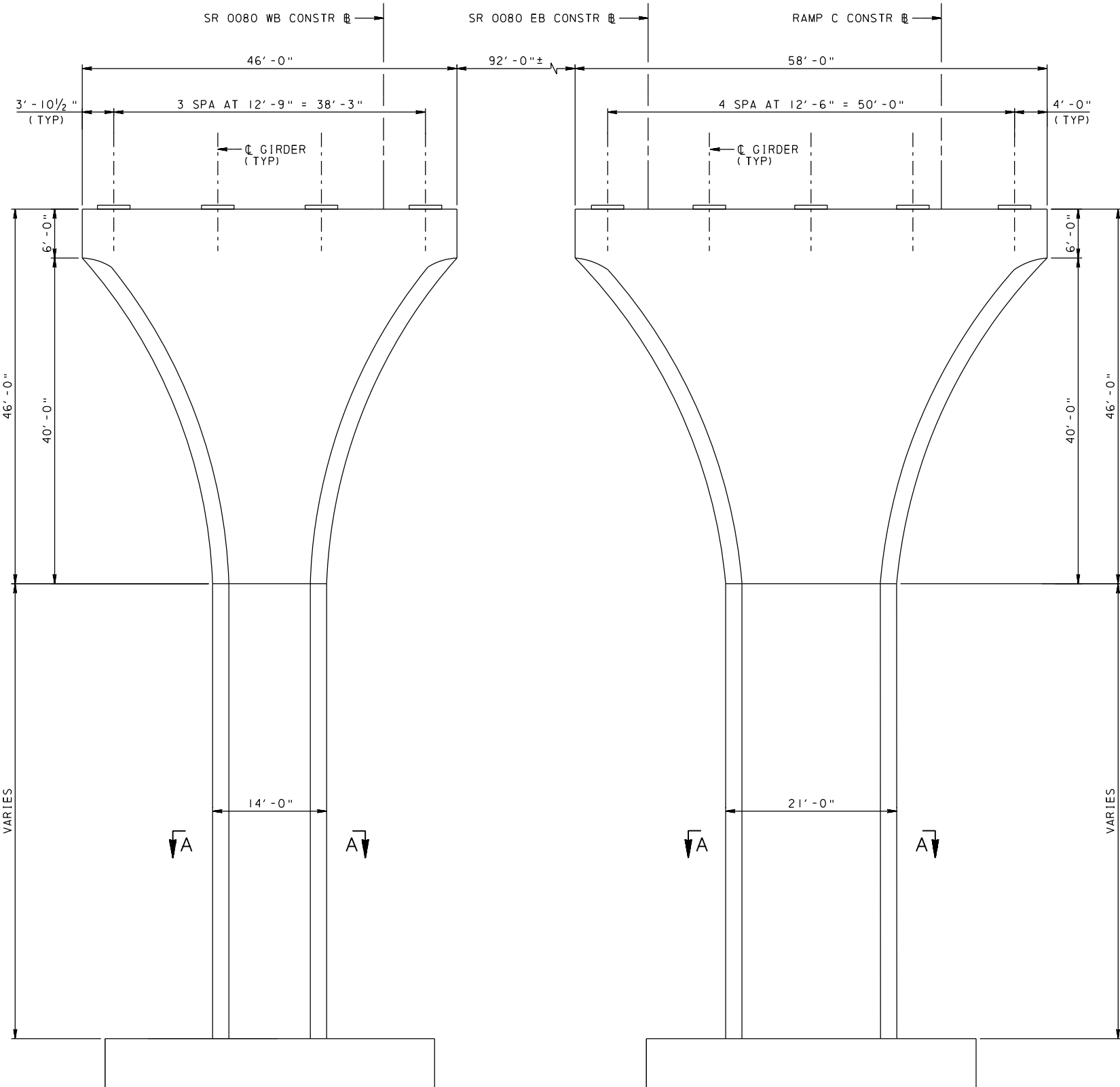
**FUTURE DECK REPLACEMENT**

RECOMMENDED \_\_\_\_\_

SHEET 6 OF 7

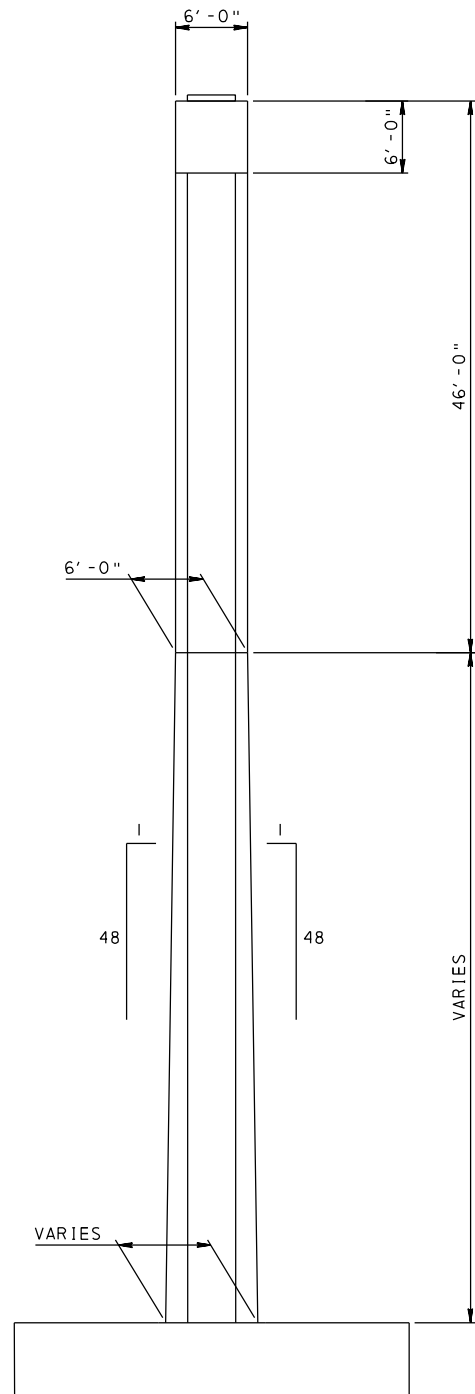
S-XXXX

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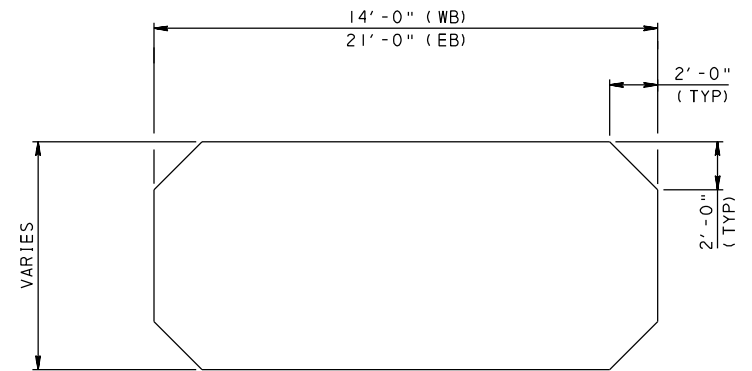


**WB PIER - ELEVATION**  
 2 0 4 8 FEET

**EB PIER - ELEVATION**  
 2 0 4 8 FEET



**SIDE VIEW**  
 2 0 4 8 FEET



**SECTION A-A**  
 2 0 2 4 FEET

**NOTE:**

1. FOR GENERAL NOTES, SEE SHEET 3.

Mark	Description	By	Chk'd.	Recm'd.	Date
REVISIONS					

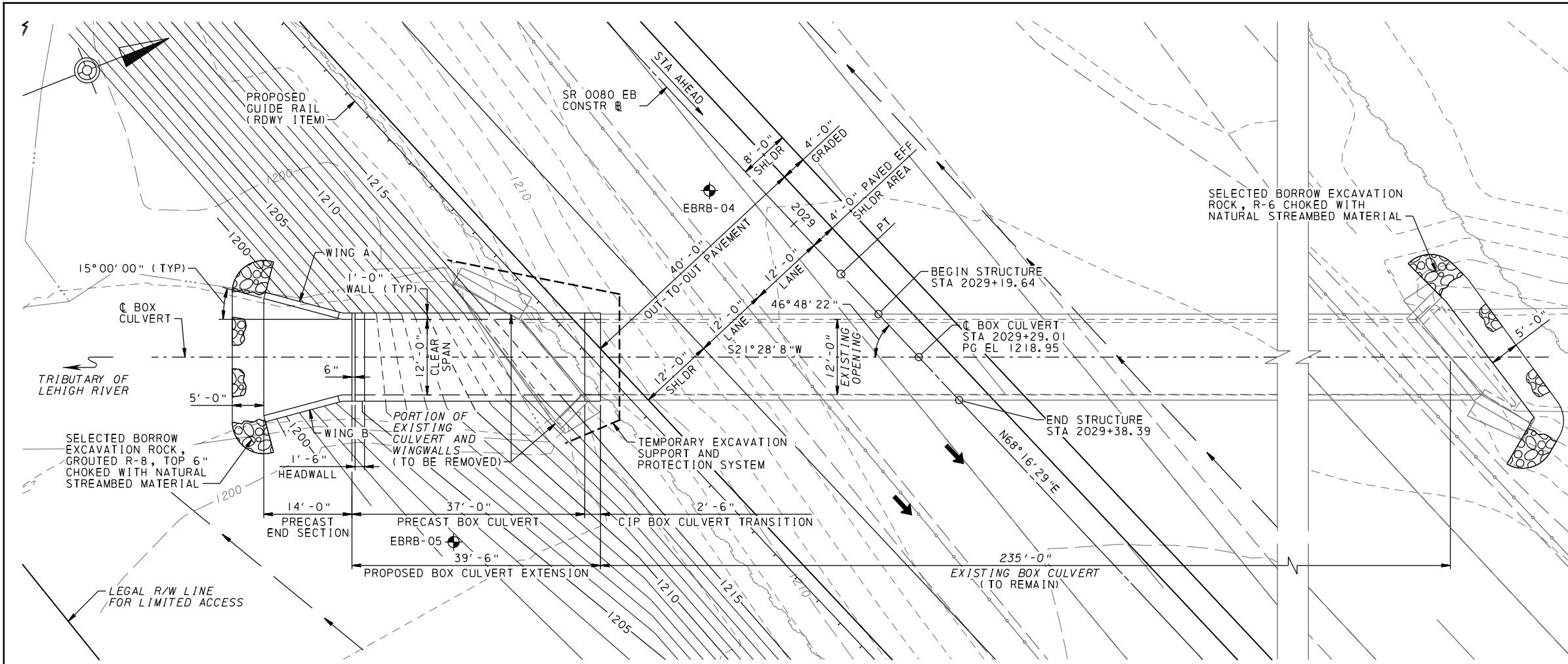
S.R. 0080 PREVIOUSLY KNOWN AS L.R. 1009 MPMS/ECMS: 99552  
 BMS STR ID: 13-0080-2728-0000, 13-0080-2729-0000 BRKEY: 8972, 8974

**COMMONWEALTH OF PENNSYLVANIA**  
 DEPARTMENT OF TRANSPORTATION

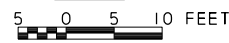
**LUZERNE AND CARBON COUNTIES**  
**S.R. 0080 SECTION 08B**

SEG. 2728 OFF. 0000 EB & SEG. 2729 OFF. 0000 WB  
 S.R. 0080-08B STA. 2017+33.89 EB & STA. 2217+36.37 WB  
 OVER READING BLUE MOUNTAIN & NORTHERN RAILROAD,  
 LEHIGH GORGE TRAIL, LEHIGH RIVER, AND S.R. 1005  
 DUAL 4-SPAN CONTINUOUS STEEL GIRDER BRIDGES  
**PIER DETAILS**

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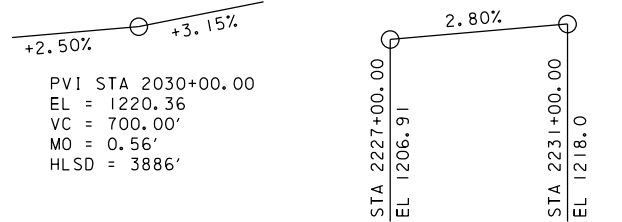


PLAN



AS-DRILLED BORING LOCATION			
BORING	STATION	OFFSET (FT)	SURFACE EL (FT)
EBRB-04	2028+87.00	6.22 RT	1218
EBRB-05	2029+00.00	74.00 RT	1202

**VERTICAL GEOMETRY SR 0080 EB CONSTR B**      **VERTICAL GEOMETRY SR 0080 WB CONSTR B**



**HORIZONTAL GEOMETRY SR 0080 EB CONSTR B**      **HORIZONTAL GEOMETRY SR 0080 WB CONSTR B**

SR 0080 EB CONSTR B		SR 0080 WB CONSTR B	
PI STA 2025+76.65	Δ = 7°35'47" LT	PI STA 2230+17.69	Δ = 7°34'41" LT
T = 335.26'	L = 669.53'	T = 384.21'	L = 767.30'
R = 5050.00'	E = 11.12'	R = 5801.43'	E = 12.71'
PC STA 2022+41.39	PT STA 2029+10.92	PCC STA 2226+33.48	PT STA 2234+00.79
SUPERELEVATE 4.0%		SUPERELEVATE N/A	
DESIGN SPEED 70 MPH		DESIGN SPEED 70 MPH	

**HYDRAULIC DATA**

DRAINAGE AREA= 1.7 SQ MI  
 DESIGN FLOOD (50 YEAR):  
 Q = 511 CFS  
 VELOCITY= 4.7 FPS  
 WS EL= 1211.42  
 100 YEAR FLOOD:  
 Q = 635 CFS  
 VELOCITY= 4.8 FPS  
 WS EL= 1212.43  
 FLOOD OF RECORD: N/A

- LEGEND:**
- ◆ PROPOSED TEST BORINGS
  - 1200- EXISTING CONTOUR
  - 1200- PROPOSED CONTOUR
  - EDGE OF NORMAL HIGH WATER
  - ➔ DIRECTION OF TRAFFIC

**PROPOSED REPAIRS TO EXISTING CULVERT**

- THE EXISTING CULVERT PRESERVATION CONSISTS OF, BUT IS NOT LIMITED TO, THE FOLLOWING ITEMS OF WORK:
- PERFORM TYPE I CONCRETE REPAIRS TO DETERIORATED AREAS OF EXISTING CULVERT (APPROX 425 SF).
  - PERFORM TYPE II CONCRETE REPAIRS TO DETERIORATED AREAS OF EXISTING CULVERT (APPROX 6 SF).
  - EPOXY INJECTION SEAL CRACKS IN THE EXISTING PORTION OF THE CULVERT (APPROX 275 LF).
  - PLACE SELECTED BORROW EXCAVATION ROCK, R-6 CHOKED WITH NATURAL STREAMBED MATERIAL AT INLET (APPROX 19 CY).

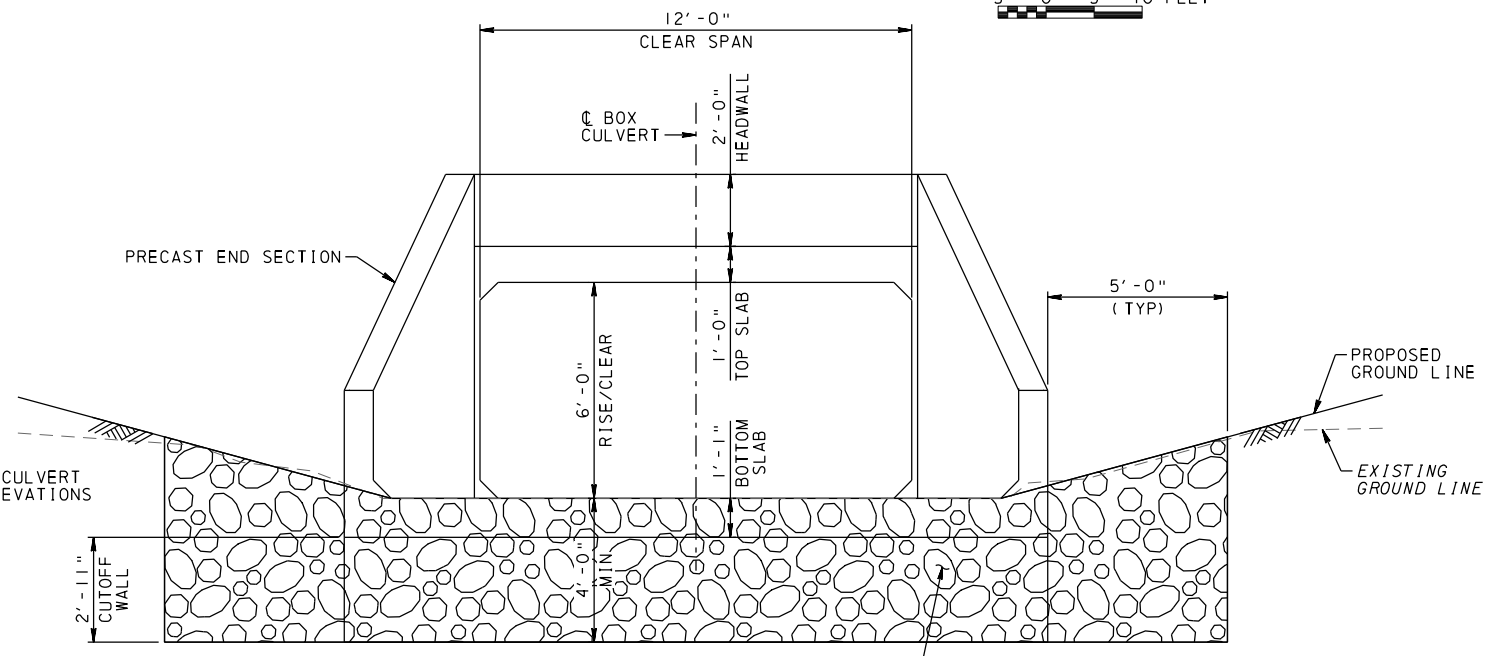
- NOTES:**
- FOR GENERAL NOTES, SEE SHEET 2.
  - FOR TYPICAL SECTIONS, SEE SHEET 3.

Mark	Description	By	Chk'd.	Recm'd.	Date
REVISIONS					

S.R. 0080 PREVIOUSLY KNOWN AS L.R. 1009  
 BMS STR ID: 13-0080-2730-0300 MPMS/ECMS PROJ: 99552 BRKEY: 8973

**COMMONWEALTH OF PENNSYLVANIA**  
 DEPARTMENT OF TRANSPORTATION

**CARBON COUNTY**  
 S.R. 0080 SECTION 08B  
 SEG. 2730 OFF. 0300  
 S.R. 0080 EASTBOUND STA. 2029+29.01  
 OVER TRIBUTARY OF LEHIGH RIVER  
 12'-0" x 6'-0" BOX CULVERT EXTENSION  
**CONCEPTUAL TYPE, SIZE, & LOCATION**



OUTLET ELEVATION



NOTE: SEE SECTION ALONG CULVERT FOR WATER SURFACE ELEVATIONS AT INLET.

PREPARED BY:  
**HDR**  
 HDR ENGINEERING, INC.  
 3025 CHEMICAL RD  
 SUITE 110  
 PLYMOUTH MEETING, PA 19462

SIGNATURE & DATE \_\_\_\_\_

RECOMMENDED \_\_\_\_\_  
 DISTRICT 5-0 BRIDGE ENGINEER

SHEET 1 OF 3  
 +SUPPLEMENTAL DRAWINGS  
 S-XXXX

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GENERAL NOTES:

CULVERT EXTENSION DESIGN SPECIFICATIONS

AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS, 8TH EDITION, 2017 AND AS SUPPLEMENTED BY DESIGN MANUAL PART 4 DECEMBER 2019.

DESIGN IS IN ACCORDANCE WITH THE LRFD METHOD.

DESIGN LIVE LOADS

PHL-93, P-82, AND P2016-13

LIVE LOAD TO THE CULVERT IS BASED UPON DM-4 SECTION 4.6.2.10.

DEAD LOADS

A UNIT WEIGHT OF 0.140 kcf WAS USED FOR EARTH LOADS FOR CULVERT EXTENSION DESIGN.

GENERAL

PROVIDE MATERIALS AND PERFORM WORK IN ACCORDANCE WITH SPECIFICATIONS, PUBLICATION 408, 2020, AASHTO/AWS BRIDGE WELDING CODE D1.5-2008 (USE AASHTO/AWS D1.1-2008 FOR WELDING NOT COVERED IN AASHTO/AWS D1.5-008), AND CONTRACT SPECIAL PROVISIONS.

VERIFY ALL DIMENSIONS AND GEOMETRY OF THE EXISTING STRUCTURE IN THE FIELD AS NECESSARY FOR PROPER FIT OF THE PROPOSED CONSTRUCTION.

USE CARE WHEN REMOVING PORTIONS OF THE EXISTING STRUCTURE SO AS TO NOT DAMAGE REMAINING PARTS OF THE STRUCTURE, OR REMAINING REINFORCEMENT BARS. REPLACE ALL PARTS WHICH ARE DESIGNATED TO REMAIN AND ARE DAMAGED DURING THE REMOVAL OPERATIONS AT NO COST TO THE DEPARTMENT.

DO NOT CONSIDER ANY OF THE DATA ON THE EXISTING STRUCTURE SUPPLIED IN THE ORIGINAL DESIGN DRAWINGS OR MADE AVAILABLE BY THE DEPARTMENT OR ITS AUTHORIZED AGENTS AS POSITIVE REPRESENTATIONS OF ANY OF THE CONDITIONS THAT YOU WILL ENCOUNTER IN THE FIELD.

THE INFORMATION SHOWN ON THE PLANS FOR THE EXISTING STRUCTURE IS NOT PART OF THE PLANS, PROPOSAL OR CONTRACT AND IS NOT TO BE CONSIDERED A BASIS FOR COMPUTATION OF THE UNIT PRICES USED FOR BIDDING PURPOSES. THERE IS NO EXPRESSED OR IMPLIED AGREEMENT THAT INFORMATION IS CORRECTLY SHOWN. THE BIDDER IS NOT TO RELY ON THIS INFORMATION, BUT IS TO ASSUME THE POSSIBILITY THAT CONDITIONS AFFECTING THE COST AND/OR QUANTITIES OF WORK TO BE PERFORMED MAY DIFFER FROM THOSE INDICATED (ORIGINAL PLANS, S-5856 - OCT, 1962).

CONCRETE NOTES

PROVIDE 2 INCH CONCRETE COVER ON REINFORCEMENT BARS EXCEPT AS NOTED.

USE CEMENT CONCRETE WITH A 28 DAY COMPRESSIVE STRENGTH OF 5,000 PSI WHEN CONSTRUCTING PRECAST BOX CULVERTS SEGMENTS AND PRECAST END SECTIONS.

USE CLASS AA CEMENT CONCRETE IN ALL CONCRETE REPAIRS, HEADWALL, AND CUTOFF WALL.

USE CLASS A CEMENT CONCRETE IN CAST-IN-PLACE TRANSITION.

A HIGHER CLASS CONCRETE MAY BE SUBSTITUTED FOR A LOWER CLASS CONCRETE AT NO ADDITIONAL COST TO THE DEPARTMENT, IF APPROVED BY THE DISTRICT BRIDGE ENGINEER.

PROVIDE GRADE 60 REINFORCING BARS THAT MEET THE REQUIREMENTS OF ASTM A615/A615M, A996/A996M, OR A706/A706M. DO NOT WELD GRADE 60 REINFORCING STEEL BARS UNLESS SPECIFIED. GRADE 40 REINFORCING STEEL BARS MAY BE SUBSTITUTED WITH A PROPORTIONAL INCREASE IN CROSS-SECTIONAL AREA, IF APPROVED BY THE CHIEF BRIDGE ENGINEER. DO NOT USE RAIL STEEL A996/A996M REINFORCEMENT BARS.

USE EPOXY-COATED REINFORCEMENT BARS.

PROVIDE MINIMUM EMBEDMENT AND SPLICE LENGTHS IN ACCORDANCE WITH STANDARD DRAWING BC-736M, UNLESS OTHERWISE INDICATED.

GALVANIZED REINFORCING STEEL BARS MAY BE SUBSTITUTED FOR EPOXY-COATED REINFORCING STEEL BARS AT NO ADDITIONAL COST TO THE DEPARTMENT.

RAKE-FINISH ALL HORIZONTAL CONSTRUCTION JOINTS, EXCEPT AS INDICATED.

CHAMFER EXPOSED CONCRETE EDGES 1"x1" AND PRECAST CONCRETE EDGES 3/4"x3/4".

ALL DIMENSIONS SHOWN ARE HORIZONTAL, EXCEPT AS NOTED.

WELDING OF REINFORCEMENT BARS DURING FABRICATION OR CONSTRUCTION IS NOT PERMITTED UNLESS SPECIFIED.

BOX CULVERT NOTES

DO NOT EXCEED A 2'-0" DIFFERENCE IN FILL ELEVATION ON THE SIDES OF THE BOX CULVERT DURING PLACEMENT OF BACKFILL. DO NOT ALLOW THE WHEELS OR ROLLERS TO COME CLOSER THAN 1'-0" TO THE FACE OF THE STRUCTURE DURING COMPACTION OF THE BACKFILL.

CAST ALL (EPOXY COATED OR GALVANIZED) THREADED INSERTS INTO THE PRECAST BOX SEGMENTS. ALL INSERTS ARE INCIDENTAL TO THE BOX CULVERT SEGMENTS.

PREPARE EXISTING CONCRETE SURFACES WHICH WILL BE AGAINST NEWLY PLACED CONCRETE IN ACCORDANCE WITH PUBLICATION 408/2020, SECTION 1001.3(m). APPLY AN EPOXY BONDING COMPOUND BETWEEN EXISTING AND NEW CAST-IN-PLACE CONCRETE. MATERIAL, SURFACE PREPARATION, AND INSTALLATION IS INCIDENTAL TO CLASS A CEMENT CONCRETE.

USE EPOXY BONDING COMPOUND WHEREVER CAST-IN-PLACE CEMENT CONCRETE COMES IN CONTACT WITH PRECAST CEMENT CONCRETE. THE EPOXY COMPOUND IS TYPE 2, GRADE 2, AS DESCRIBED IN ASTM-C881-90 AND IN ACCORDANCE WITH PUBLICATION 408/2020, SECTION 1001.2(k). CLEAN SURFACES OF ALL MATERIAL OR CONTAMINATE DETRIMENTAL TO PROPER BONDING. MATERIAL, SURFACE PREPARATION, AND INSTALLATION IS INCIDENTAL TO THE CLASS OF CONCRETE FOR WHICH IT IS APPLIED.

REMOVE THE EXISTING CULVERT ONLY AS CALLED FOR BY PLAN OR AS PERMITTED BY THE DEPARTMENT'S REPRESENTATIVE.

BOX CULVERT NOTES (CONTINUED)

PROVIDE 2'-0" MINIMUM WIDTH OF WATERPROOFING MEMBRANE AS PER PUBLICATION 408/2020, SECTION 680.2(a) OR 680.2(b) ALONG THE TOP AND SIDE OF JOINTS. FOR ADDITIONAL DETAILS, REFER TO BC-788M.

PROVIDE 4'-0" MINIMUM SEGMENT LENGTHS.

SITE CLASS IS NOT CLASS E.

SUBMIT ERECTION DRAWINGS FOR REVIEW AND APPROVAL PRIOR TO SETTING BOX SECTIONS AND PRECAST END SECTIONS.

DETAIL ON SHOP DRAWINGS THE PRECAST BOX SECTION AND PRECAST END SECTION LENGTHS THAT MEET ALL THE HAULING GUIDELINES AND RESTRICTIONS, HAULING PERMITS ARE THE RESPONSIBILITY OF THE CONTRACTOR. SHOW METHOD OF BRACING AND MOVING SECTIONS TO AVOID CRACKING DURING TRANSPORTATION AND ERECTION.

SHIP PRECAST BOX SECTIONS AND PRECAST END SECTIONS FROM FABRICATOR AFTER ATTAINING REQUIRED 28 DAY COMPRESSIVE STRENGTHS.

USE GALVANIZED MECHANICAL STRAP CONNECTIONS WITH PRECAST END SECTION CONSTRUCTION.

PROPOSED CULVERT IS NOT WEIGHT RESTRICTED. SEE PUBLICATION 408 SECTION 105.17 FOR CONSTRUCTION LOADING LIMITS.

UTILITY NOTES

COORDINATE, LOCATE AND CONDUCT ALL WORK RELATED TO PUBLIC AND PRIVATE UTILITIES IN ACCORDANCE WITH PUBLICATION 408, 2020, SECTIONS 105.06 AND 107.12.

COORDINATE THE REQUIREMENTS FOR PROTECTION AND/OR RELOCATION OF UTILITIES WITH THE UTILITY OWNER PRIOR TO STARTING WORK.

VERIFY AND LOCATE ALL EXISTING UTILITIES PRIOR TO STARTING WORK. CONDUCT OPERATIONS IN A MANNER WHICH ENSURES THAT THE UTILITIES WILL NOT BE DISTURBED OR ENDANGERED AND ASSUME FULL RESPONSIBILITY FOR ANY DAMAGE TO UTILITIES DURING CONSTRUCTION. PENNDOT DOES NOT ASSUME RESPONSIBILITY FOR REIMBURSEMENT, PARTICIPATION IN DESIGN AND/OR REVISIONS OR LIABILITY FOR ACCURACY OF TYPE, SIZE AND LOCATION OF ANY UTILITY.

GALVANIZING NOTES

GALVANIZE ALL FABRICATED STRUCTURAL STEEL AFTER FABRICATION.

HOT DIP GALVANIZE THE FOLLOWING:  
BOX CULVERT MECHANICAL STRAP CONNECTION

MECHANICALLY GALVANIZE THE FOLLOWING:  
ASTM 325 BOLTS, ASTM A563 NUTS, AND ASTM F436 WASHERS

REPAIR ALL DAMAGED GALVANIZED SURFACES IN ACCORDANCE WITH PUBLICATION 408/2020, SECTION 1105.2(s).

POST TENSIONING NOTES

EXTEND BOTTOM SLAB POST-TENSIONING STRANDS THROUGH THE BOTTOM SLAB OF THE PRECAST CONCRETE END SECTIONS.

POST-TENSION THE PRECAST BOX SEGMENTS AND END SECTIONS PER BC-798M AND PUBLICATION 408 SECTION 1108.

SUBMIT THE POST-TENSION CONNECTION DESIGN COMPUTATIONS FOR THE BOX CULVERT AND END SECTIONS WITH THE SHOP DRAWINGS IN ACCORDANCE WITH BC-798M. INCLUDE STRAND PATTERN, STRAND LOADING, AND STRESSING DETAILS ON THE SHOP DRAWINGS.

POST-TENSIONING DUCTS MAY BE PLACED AT EITHER CORNER OF HAUNCH TO SATISFY DESIGN AND CONSTRUCTION REQUIREMENTS. PROVIDE A 3" CONCRETE CLEARANCE. HAUNCH DUCT MUST BE TIED TO INSIDE REINFORCEMENT. LOCATION MUST MEET FABRICATION REQUIREMENTS.

IF TIGHT JOINTS ARE NOT ATTAINED IN ANY LOCATION AFTER POST-TENSIONING, RELEASE STRANDS AND REPEAT UNTIL JOINTS ARE TIGHT AND APPROVED BY THE DEPARTMENT REPRESENTATIVE.

BOX SEGMENTS AND END SECTIONS ARE POST-TENSIONED IN STAGES. THE CONTRACTOR IS REQUIRED TO SUBMIT A PLAN FOR POST-TENSIONING SEQUENCE TO THE DEPARTMENT FOR APPROVAL PRIOR TO SETTING ANY SEGMENTS.

POST-TENSION BOX SEGMENTS FIRST, THEN PROVIDE:  
• MECHANICAL SPLICERS ON BOTTOM STRANDS TO CONNECT WITH THE INLET/OUTLET END SECTIONS AND POST-TENSION BOTTOM STRANDS THROUGH THE END SECTIONS.  
• STRANDS ON SIDES OF END SECTION AS SHOWN ON BC-798M.

STAGING, SPACING, AND POST-TENSIONING FORCE TO BE SHOWN ON FABRICATOR'S SHOP DRAWINGS.

CLASSIFICATION OF EARTHWORK FOR STRUCTURES	RC-11M	6/01/2010
BACKFILL AT STRUCTURES	RC-12M	2/08/2019
ANCHOR SYSTEMS	BC-734M	2/19/2021
WALL CONSTRUCTION AND EXPANSION JOINT DETAILS	BC-735M	9/30/2016
REINFORCEMENT BAR FABRICATION DETAILS	BC-736M	1/31/2019
BRIDGE DRAINAGE	BC-751M	1/31/2019
REINFORCED CONCRETE REPAIRS	BC-783M	1/31/2019
TYPICAL WATERPROOFING AND EXPANSION DETAILS	BC-788M	1/31/2019
MECHANICAL CONNECTION DETAILS	BC-798M	1/31/2019
DESCRIPTION	DWG. NO.	APP. DATE
SUPPLEMENTAL DRAWINGS		

Mark	Description	By	Chk'd.	Recm'd.	Date
REVISIONS					

S. R. 0080 PREVIOUSLY KNOWN AS L.R. 1009  
BMS STR ID: 13-0080-2730-0300 MPMS/ECMS PROJ: 99552 BRKEY: 8973

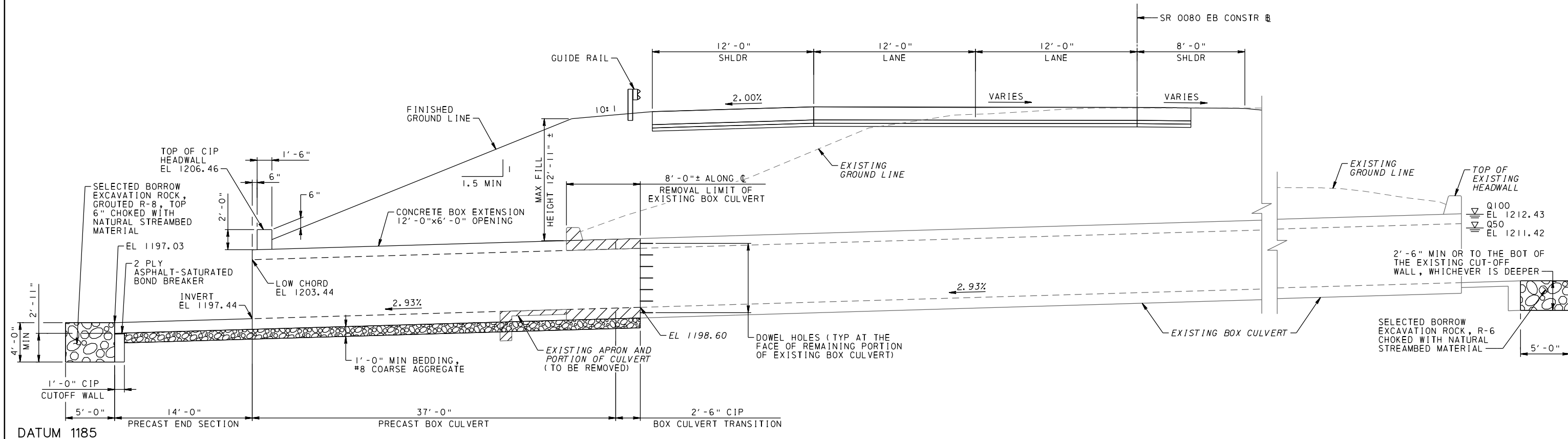
**COMMONWEALTH OF PENNSYLVANIA**  
DEPARTMENT OF TRANSPORTATION

**CARBON COUNTY**  
S. R. 0080 SECTION 08B  
SEG. 2730 OFF. 0300  
S. R. 0080 EASTBOUND STA. 2029+29.01  
OVER TRIBUTARY OF LEHIGH RIVER  
12' - 0" x 6' - 0" BOX CULVERT EXTENSION  
**GENERAL NOTES**

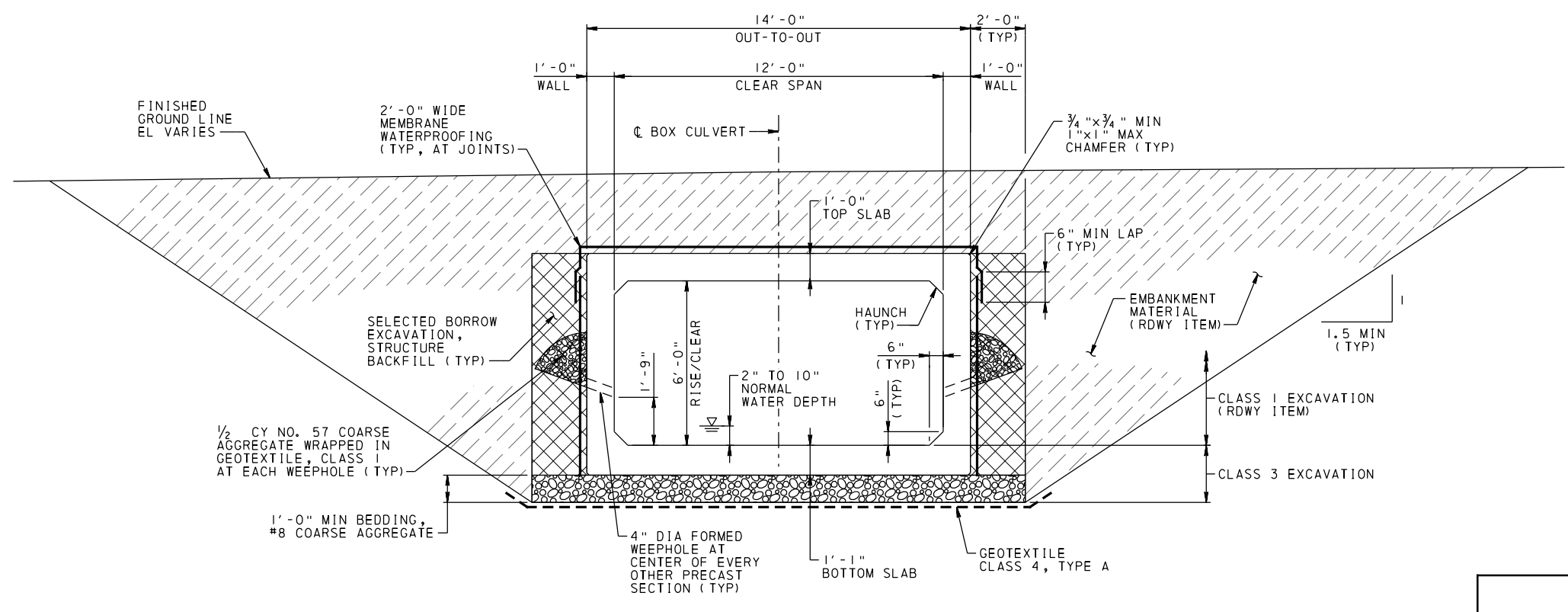
RECOMMENDED \_\_\_\_\_ SHEET 2 OF 3

S-XXXXX

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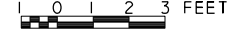


SECTION ALONG Q BOX CULVERT



BOX CULVERT EXTENSION TYPICAL SECTION

PERPENDICULAR TO Q BOX CULVERT



NOTES:

1. FOR GENERAL NOTES, SEE SHEET 2.

Mark	Description	By	Chk'd.	Recm'd.	Date
REVISIONS					

S.R. 0080 PREVIOUSLY KNOWN AS L.R. 1009  
BMS STR ID: 13-0080-2730-0300 MPMS/ECMS PROJ: 99552 BRKEY: 8973

**COMMONWEALTH OF PENNSYLVANIA**  
DEPARTMENT OF TRANSPORTATION

**CARBON COUNTY**  
S.R. 0080 SECTION 08B  
SEG. 2730 OFF. 0300  
S.R. 0080 EASTBOUND STA. 2029+29.01  
OVER TRIBUTARY OF LEHIGH RIVER  
12'-0" x 6'-0" BOX CULVERT EXTENSION  
TYPICAL SECTIONS

RECOMMENDED \_\_\_\_\_ SHEET 3 OF 3

S-XXXX

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DISTRICT	COUNTY	TOWNSHIP	BOROUGH	ROUTE	SECTION	TOTAL SHEETS
5-0	LUZERNE		WHITE HAVEN	0080	08B	26
	CARBON		EAST SIDE			
	CARBON	KIDDER				

MPMS/ECMS NO. 99552

# COMMONWEALTH OF PENNSYLVANIA



## DEPARTMENT OF TRANSPORTATION

### DESIGN DRAWINGS

FOR

### CONSTRUCTION

OF

STATE ROUTE 0080 SECTION 08B

IN LUZERNE COUNTY

FROM STA. 1984+55.00 EB TO STA. 2018+90.04 EB LENGTH 3385.04 FT. 0.64 MI.

FROM SEG. 2720 OFFSET 1097 TO SEG. 2728 OFFSET 0558

FROM STA. 2197+00.00 WB TO STA. 2218+90.04 WB LENGTH 2090.04 FT. 0.40 MI.

FROM SEG. 2721 OFFSET 2330 TO SEG. 2729 OFFSET 0533

AND

IN CARBON COUNTY

FROM STA. 2018+90.04 EB TO STA. 2049+06.00 EB LENGTH 2915.96 FT. 0.55 MI.

FROM SEG. 2728 OFFSET 0558 TO SEG. 2730 OFFSET 2204

FROM STA. 2218+90.04 WB TO STA. 2249+70.00 WB LENGTH 2979.96 FT. 0.56 MI.

FROM SEG. 2729 OFFSET 0533 TO SEG. 2731 OFFSET 2253

ALSO

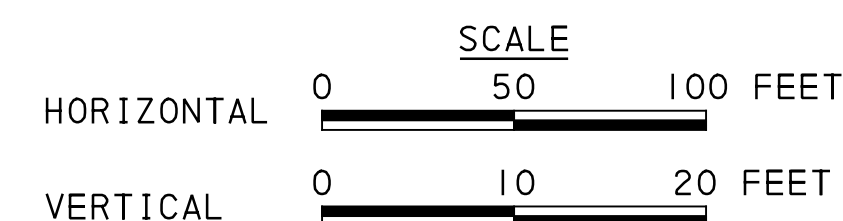
STATE ROUTE 8006 SECTION 08B

IN LUZERNE COUNTY

ALSO INCLUDED:

TRAFFIC CONTROL PLAN	59 SHEETS
SIGNING AND PAVEMENT MARKING PLAN	8 SHEETS
EROSION AND SEDIMENT POLLUTION CONTROL PLAN	6 SHEETS
POST CONSTRUCTION STORMWATER MANAGEMENT PLAN	11 SHEETS
STRUCTURE PLAN	
S-	7 SHEETS
S-	3 SHEETS
CROSS SECTIONS	101 SHEETS
OPEN ROAD CASHLESS TOLLING SITE DEVELOPMENT PLAN	6 SHEETS
OPEN ROAD CASHLESS TOLLING SIGNING AND PAVEMENT MARKING PLAN	27 SHEETS
OPEN ROAD CASHLESS TOLLING ELECTRICAL AND COMMUNICATIONS SERVICE PLAN	5 SHEETS
OPEN ROAD CASHLESS TOLLING STRUCTURE PLAN	3 SHEETS
OPEN ROAD CASHLESS TOLLING UTILITY BUILDING PLAN	9 SHEETS

ESTABLISHED AS A LIMITED ACCESS HIGHWAY FROM STATION 1989+00 EB TO STATION 2039+75 EB AND STATION 2197+00 WB TO STATION 2249+70 WB LEGISLATIVE ROUTE 1009 SECTION 95 R/W APPROVED OCTOBER 8, 1963.



#### DESIGN DESIGNATION

HIGHWAY CLASSIFICATION - INTERSTATE/FREEWAY  
 ROADWAY TYPOLOGY - LIMITED ACCESS FREEWAY,  
 RURAL INTERSTATE  
 DESIGN SPEED - 70 MPH (65 MPH POSTED)  
 PAVEMENT WIDTH - 24' (2-12' LANES)  
 SHOULDER WIDTH - 20' (12' OUTSIDE, 8' INSIDE)  
 MEDIAN WIDTH, MAXIMUM - 110'  
 MINIMUM - 60'

#### TRAFFIC DATA

CURRENT A. D. T. - 32,602 (2026)  
 DESIGN YEAR A. D. T. - 53,736 (2046)  
 D. H. V. - 3,762  
 D - 52%  
 T - 34%

DESIGN FIELD VIEW SUBMISSION  
 HDR ENGINEERING INC.  
 DECEMBER 2021

PREPARED BY:  
  
 HDR ENGINEERING, INC.  
 3025 CHEMICAL ROAD  
 SUITE 110  
 PLYMOUTH MEETING, PA 19462

REGISTERED PROFESSIONAL ENGINEER

DATE: \_\_\_\_\_

RECOMMENDED DATE: \_\_\_\_\_  
 DISTRICT EXECUTIVE

RECOMMENDED DATE: \_\_\_\_\_  
 DEPUTY SECRETARY

APPROVED DATE: \_\_\_\_\_  
 SECRETARY OF TRANSPORTATION  
 (ON BEHALF OF THE GOVERNOR  
 AS WELL AS THE SECRETARY)

USER: ZZZNN PLOT: DRIVER PenndOT\_PDF\_Memo.pltcfvg PLOT DATE: 12-01-2021 9:54:02 AM  
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SHEET INDEX BLOCK

DESCRIPTION	SHEET
TITLE SHEET	1
INDEX MAP AND PROPERTY OWNERS	2
LOCATION MAP AND GENERAL NOTES	3
PROJECT COORDINATES	4
GEOMETRIC DATA	5 TO 6
SUPERELEVATION TABLES	7
TYPICAL SECTIONS	8 TO 12
PLAN SHEETS	13 TO 17
PROFILE SHEETS	18 TO 26

\* LUZERNE COUNTY  
CARBON COUNTY

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
5-0	*	0080	08B	2 OF 26
WHITE HAVEN & EAST SIDE BOROUGHS AND KIDDER TOWNSHIP				
REVISION NUMBER	REVISIONS		DATE	BY

LIST OF PROPERTY OWNERS

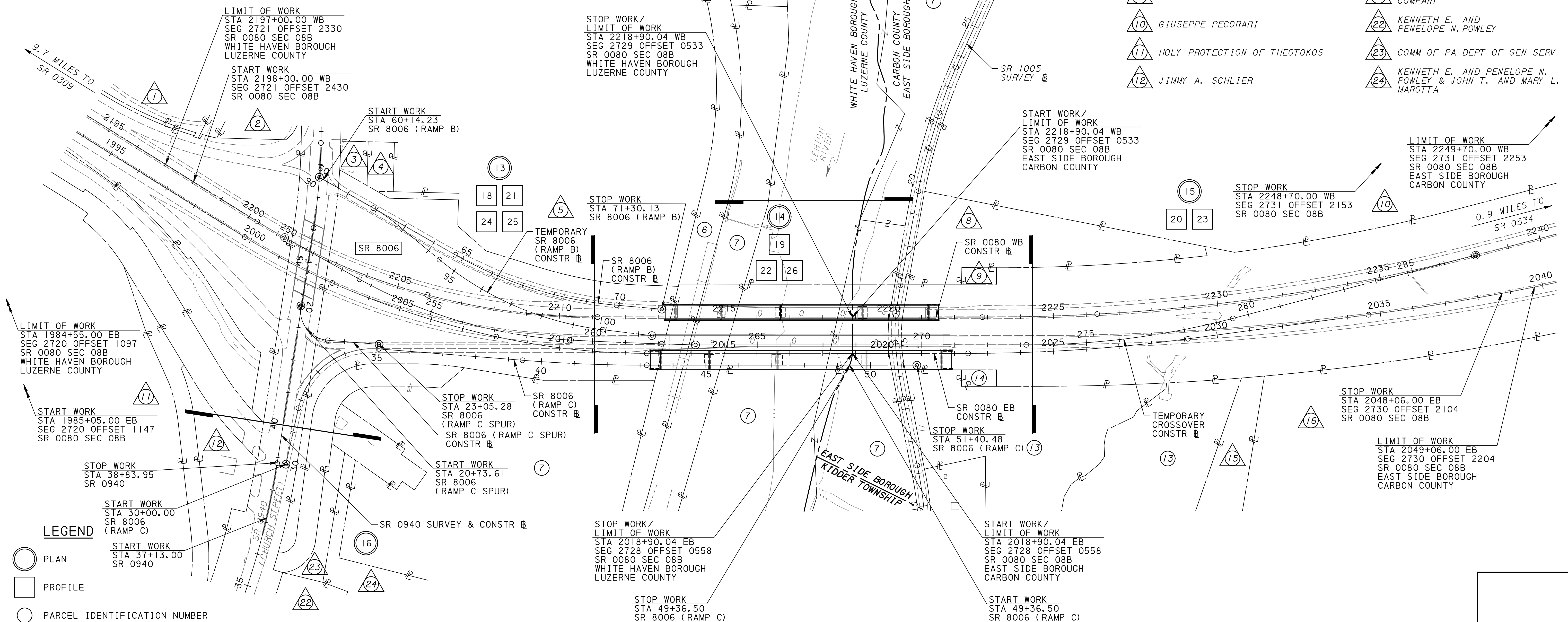
- |   |  |
|---|--|
| 1 LAUREL CEMETERY ASSOCIATES                | 13 JOSEPH & LAURA ALFIERI  |
| 2 FROM MAP ENTITLED: SCHAFFER'S GARAGE MAP  | 14 UNKNOWN OWNER   |
| 3 FRANK M. LUSHEFSKI                        | 15 PENN HAVEN MTN. ESTATE LP                                       |
| 4 MICHAEL J. AND CHRISTINA R. GRECO         | 16 WILLIAM KUNKLE JR   |
| 5 WHITE HAVEN BORO                          | 17 SHADOWSCAPE, LTD.   |
| 6 READING & NORTHERN R.R. (WIDTH VARIES)    | 18 LITTLE WASHINGTON WASTEWATER COMPANY                            |
| 7 COMMONWEALTH OF PENNSYLVANIA (STATE PARK) | 19 RIVERSIDE HOLDINGS, LLC   |
| 8 LAURIE ANNE LOUCKS                        | 20 RITE AID OF PA  |
| 9 UNKNOWN OWNER                             | 21 LITTLE WASHINGTON WASTEWATER COMPANY                            |
| 10 GIUSEPPE PECORARI                        | 22 KENNETH E. AND PENELOPE N. POWLEY                               |
| 11 HOLY PROTECTION OF THEOTOKOS             | 23 COMM OF PA DEPT OF GEN SERV                                     |
| 12 JIMMY A. SCHLIER                         | 24 KENNETH E. AND PENELOPE N. POWLEY & JOHN T. AND MARY L. MAROTTA |

TABULATION OF SEGMENT EQUALITIES

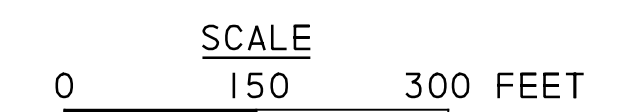
SR 0080 EB SEG 2720/0000 = STA 1973+58.44	SR 0080 WB SEG 2721/0000 = STA 2173+70.00
SR 0080 EB SEG 2724/0000 = STA 2001+68.44	SR 0080 WB SEG 2725/0000 = STA 2202+21.00
SR 0080 EB SEG 2728/0000 = STA 2013+32.44	SR 0080 WB SEG 2729/0000 = STA 2213+57.00
SR 0080 EB SEG 2730/0000 = STA 2027+02.05	SR 0080 WB SEG 2731/0000 = STA 2227+17.00

TABULATION OF SEGMENT LENGTH

SR 0080 EB SEG 2720 = 2810 FT	SR 0080 WB SEG 2721 = 2851 FT
SR 0080 EB SEG 2724 = 1164 FT	SR 0080 WB SEG 2725 = 1136 FT
SR 0080 EB SEG 2728 = 1370 FT	SR 0080 WB SEG 2729 = 1360 FT
SR 0080 EB SEG 2730 = 2650 FT	SR 0080 WB SEG 2731 = 2659 FT



INDEX MAP



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LEGEND

- PLAN
- PROFILE
- PARCEL IDENTIFICATION NUMBER
- PARCEL IDENTIFICATION NUMBER - NO TAKE

**TABULATION OF OVERALL LENGTH**

SR 0080 EB STA 1984+55.00 TO STA 2049+06.00 = 6451.00 FT = 1.22 MILES  
 SR 0080 WB STA 2197+00.00 TO STA 2249+70.00 = 5270.00 FT = 0.99 MILES  
 TOTAL = 11,721.00 FT = 2.22 MILES

**TABULATION OF CONSTRUCTION LENGTH**

SR 0080 EB STA 1985+05.00 TO STA 2048+06.00 = 6301.00 FT = 1.19 MILES  
 SR 0080 WB STA 2198+00.00 TO STA 2248+70.00 = 5070.00 FT = 0.96 MILES  
 TOTAL = 11,371.00 FT = 2.15 MILES

**LIST OF EQUALITIES**

SR 0940 STA 38+82.93 BK = STA 38+82.89 AHD  
 SR 0940 STA 48+65.03 BK = STA 48+65.00 AHD

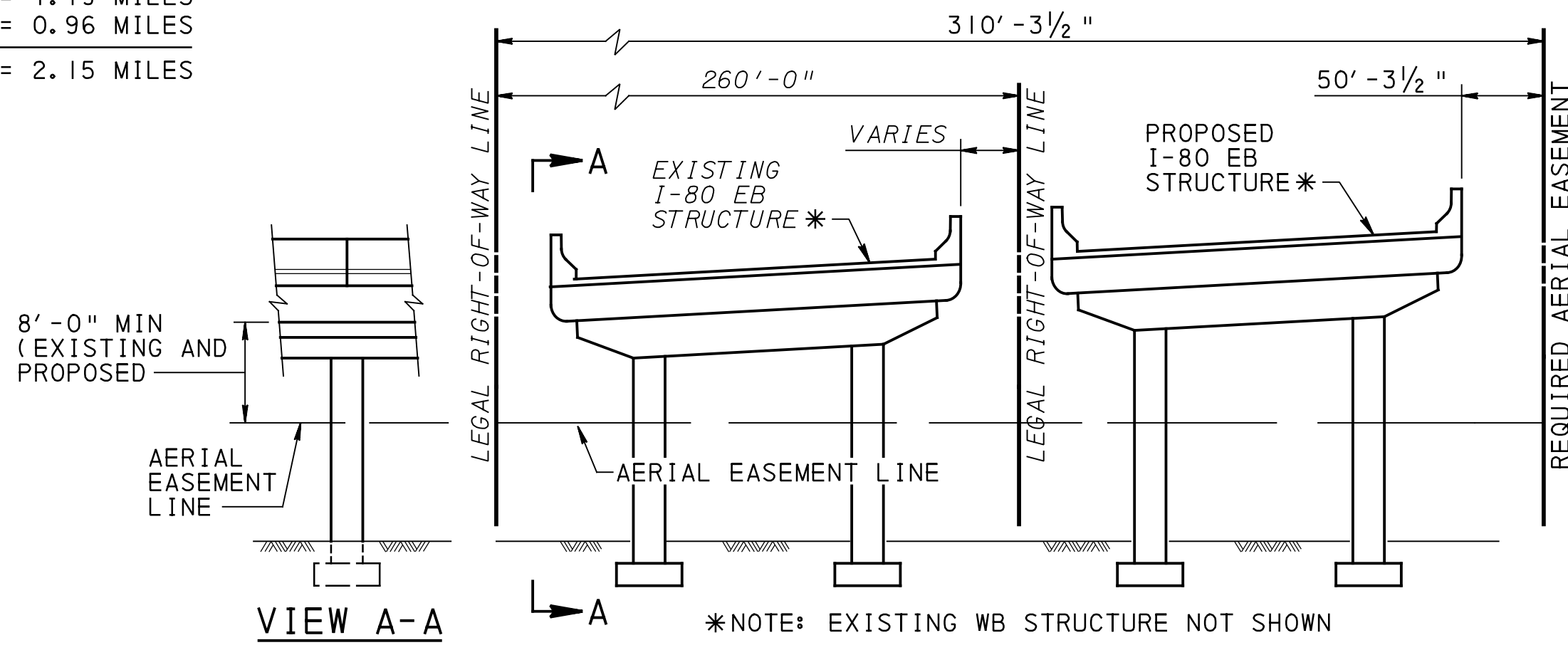
\* LUZERNE COUNTY  
 CARBON COUNTY

**GENERAL NOTES (CONT)**

DETAILS, OTHER THAN THOSE INDICATED, ARE ON THE FOLLOWING STANDARD DRAWINGS:

RC-10M (1) JUNE 1, 2010	TC-8600 (13) JUNE 13, 2013
RC-11M (2) JUNE 1, 2010	TC-8602 (4) JUNE 13, 2013
RC-12M (2) FEB. 8, 2019	TC-8604 (4) JUNE 13, 2013
RC-13M (1) JUNE 1, 2010	TC-8700C (12) JUNE 13, 2013
RC-20M (13) DEC. 17, 2019	TC-8701A (13) JUNE 13, 2013
RC-22M (6) FEB. 8, 2019	TC-8701D (13) JUNE 13, 2013
RC-24M (3) FEB. 19, 2021	TC-8702A (8) JUNE 13, 2013
RC-25M (4) FEB. 8, 2019	TC-8702B (9) JUNE 13, 2013
RC-27M (2) JUNE 1, 2010	TC-8702C (2) JUNE 13, 2013
RC-28M (2) FEB. 8, 2019	TC-8702E (5) JUNE 13, 2013
RC-30M (5) DEC. 17, 2019	TC-8710 (1) JUNE 13, 2013
RC-31M (2) JUNE 1, 2010	TC-8716 (1) JUNE 13, 2013
RC-32M (1) JUN. 1, 2010	TC-8717 (1) JUNE 13, 2013
RC-33M (2) JUNE 1, 2010	
RC-39M (30) FEB. 19, 2021	BC-721M (2) FEB. 19, 2021
RC-40M (1) FEB. 8, 2019	BC-732M (3) JAN. 31, 2019
RC-45M (24) FEB. 19, 2021	BC-734M (2) FEB. 19, 2021
RC-46M (34) FEB. 19, 2021	BC-735M (1) SEPT. 30, 2016
RC-50M (12) FEB. 19, 2021	BC-736M (3) JAN. 31, 2019
RC-51M (15) FEB. 19, 2021	BC-751M (7) JAN. 31, 2019
RC-54M (12) DEC. 17, 2019	BC-752M (2) FEB. 19, 2021
RC-58M (4) AUG. 4, 2017	BC-753M (3) JAN. 31, 2019
RC-60M (3) JUNE 1, 2010	BC-754M (2) JAN. 31, 2019
RC-65M (1) DEC. 17, 2019	BC-783M (4) JAN. 31, 2019
RC-70M (3) FEB. 8, 2019	BC-788M (12) JAN. 31, 2019
RC-71M (4) AUG. 4, 2017	BC-798M (3) JAN. 31, 2019
RC-72M (7) FEB. 8, 2019	
RC-73M (4) FEB. 8, 2019	
RC-75M (1) JUNE 1, 2010	
RC-77M (1) DEC. 17, 2019	

**AERIAL EASEMENT SKETCH**



SKETCH SHOWING ESTATE TO BE ACQUIRED FOR A LIMITED AERIAL EASEMENT FROM STA 2012+85.49 TO STA 2020+18.88 (SR 0080 EB CONSTR B)

USE OF AIRSPACE BENEATH THE ESTABLISHED GRADELINE OF THE HIGHWAY SHALL PROVIDE SUFFICIENT VERTICAL AND HORIZONTAL CLEARANCES FOR THE CONSTRUCTION, OPERATION, MAINTENANCE, VENTILATION AND SAFETY OF THE HIGHWAY FACILITY. THE ESTATE ACQUIRED ABOVE THE AERIAL EASEMENT LINE MAY BE ENTERED ON BY MOVING VEHICLES SUCH AS TRUCKS OR RAILROAD ROLLING STOCK.

**LIST OF PUBLIC UTILITIES**

PUBLIC UTILITIES					
LEGEND	UTILITY COMPANY	TELEPHONE	MAILING ADDRESS	COMPANY REPRESENTATIVE	E-MAIL
—S—	AQUA PENNSYLVANIA WASTEWATER, INC (SANITARY SEWER)	(570) 443 7099 X55502	1 AQUA WAY WHITE HAVEN, PA 18661	MR. JOSHUA SHOFF	JPSHOFF@AQUAAMERICA.COM
—W—	AQUA PENNSYLVANIA WASTEWATER, INC (WATER)	(570) 443 7099 X55502	1 AQUA WAY WHITE HAVEN, PA 18661	MR. JOSHUA SHOFF	JPSHOFF@AQUAAMERICA.COM
—CTV— —CTVU—	ATLANTIC BROADBAND	(570) 802 5642 X1174	911 N MARKET ST BERWICK, PA 18603	MR. DAVID RICHARDS	DRICHARDS@ATLANTICBB.COM
—E— —EU—	PPL ELECTRIC UTILITIES CORPORATION	(610) 774 6287	2 N 9TH ST ALLENTOWN, PA 18101-1179	MS. CHARLOTTE KRUPA	CAKRUPA@PPLWEB.COM
—G—	UGI UTILITIES INC.	(610) 807 3157 (570) 450 2612	2121 CITY LINE RD BETHLEHEM, PA 18017	MR. BRANDON HAYDT	BHAYDT@UGI.COM
—T— —TU—	VERIZON PENNSYLVANIA LLC	(570) 424 0902	20 S 7TH ST STROUDSBURG, PA 18360	MR. JOE SNYDER	J.ANTHONY.SNYDER@VERIZON.COM

PENNSYLVANIA ONE CALL TOLL FREE TELEPHONE NO.: 1-800-242-1776 DESIGNER SERIAL NO.: 20183300986, 1013, 1016, 1017

**EARTHWORK SUMMARY ENTIRE PROJECT**

THE INFORMATION ON ESTIMATED AMOUNTS OF EARTHWORK HAS BEEN USED IN THE PRELIMINARY ESTIMATE. DO NOT USE AS A WAIVER OF ANY PROVISIONS OF THE SPECIFICATIONS AND CONTRACTS.

CU. YDS. OF EXCAVATION						CUBIC YDS. OF COMPLETED EMBANKMENT *	CUBIC YDS. OF BORROW EXCAVATION	CUBIC YARDS OF WASTE
CLASS 1	CLASS 1A	CLASS 1B	CLASS 2	CLASS 3	CLASS 4			

\* INCLUDES ALL BORROW ITEMS.

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
5-0	*	0080	08B	3 OF 26
WHITE HAVEN & EAST SIDE BOROUGHS AND KIDDER TOWNSHIP				
REVISION NUMBER	REVISIONS	DATE	BY	

**GENERAL NOTES**

THE LEGAL RIGHT-OF-WAY ON SR 0080, FORMERLY LR 1009, FROM STATION 1984+55.00 EB TO STATION 2018+90.04 EB AND FROM STATION 2197+00.00 WB TO STATION 2218+90.04 WB VARIES BASED ON THE PLAN OF LR 1009, SECTION 95 R/W, SIGNED ON OCTOBER 8, 1963 AND RECORDED IN THE LUZERNE COUNTY RECORDER'S OFFICE IN MAP BOOK 23, PAGE 5.

THE LEGAL RIGHT-OF-WAY ON SR 0080, FORMERLY LR 1009, FROM STATION 2018+90.04 EB TO STATION 2049+06.00 EB AND FROM STATION 2218+90.04 WB TO STATION 2249+70.00 WB VARIES BASED ON THE PLAN OF LR 1009, SECTION 96 R/W, SIGNED ON OCTOBER 24, 1963 AND RECORDED IN THE CARBON COUNTY RECORDER'S OFFICE IN MAP BOOK 1.

THE LEGAL RIGHT-OF-WAY ON SR 0940, FORMERLY LR 40118, FROM STATION 27+11.75 TO STATION 50+00.00 VARIES BASED ON THE PLAN OF LR 1009, SECTION 95 R/W, SIGNED ON OCTOBER 8, 1963 AND RECORDED IN THE LUZERNE COUNTY RECORDER'S OFFICE IN MAP BOOK 23, PAGE 5.

THE LEGAL RIGHT-OF-WAY ON SR 1005, FORMERLY LR 13018, FROM STATION 5+00.00 TO STATION 26+47.73 IS THIRTY-TWO (32') FEET AS NOTED IN THE PLAN OF LR 1009, SECTION 96 R/W, SIGNED ON OCTOBER 24, 1963 AND RECORDED IN THE CARBON COUNTY RECORDER'S OFFICE IN MAP BOOK 1.

THIS IS A FEDERAL-AID PROJECT AND AS SUCH IS SUBJECT TO INSPECTION BY REPRESENTATIVES OF THE FEDERAL HIGHWAY ADMINISTRATION AND THE PENNSYLVANIA DEPARTMENT OF TRANSPORTATION.

THE PROJECT SURVEY IS BASED UPON THE NATIONAL GEODETIC REFERENCE SYSTEM (FORMERLY USC&GS).

THE HORIZONTAL SURVEY INFORMATION IS BASED UPON THE STATE PLANE COORDINATE SYSTEM NORTH ZONE (NAD83).

THE VERTICAL CONTROL IS BASED UPON THE NORTH AMERICAN VERTICAL DATUM (NADV88).

COMBINED SCALE FACTOR = 1.000072908

ALL CURVE DATA IS BASED ON THE ARC DEFINITION UNLESS OTHERWISE INDICATED.

THREE WORKING DAYS PRIOR TO EXCAVATION, THE CONTRACTOR MUST CONTACT THE PA ONE CALL SYSTEM, INC., PHONE 1-800-242-1776, SERIAL NO. \_\_\_\_\_ FOR WHITE HAVEN BOROUGH SERIAL NO. \_\_\_\_\_ FOR EAST SIDE BOROUGH

TEMPORARY CONSTRUCTION EASEMENT. AN EASEMENT TO USE THE LAND AS NECESSARY DURING CONSTRUCTION OF THE PROJECT. THE EASEMENT IS REQUIRED ONLY UNTIL THE CONSTRUCTION OR WORK INDICATED BY THE PLAN IS COMPLETED, UNLESS SOONER RELINQUISHED IN WRITING BY THE DEPARTMENT.

CHANNEL EASEMENT. AN EASEMENT FOR THE CONSTRUCTION, INSPECTION, MAINTENANCE, REPAIR, RECONSTRUCTION AND ALTERATION OF THE COURSE OF THE CHANNEL. THE EASEMENT SHALL NOT PREVENT THE PROPERTY OWNER FROM MAKING ANY LEGAL USE OF THE AREA WHICH IS NOT DETRIMENTAL TO THE NECESSARY FLOW OF WATER.

WHERE AN AERIAL EASEMENT IS ACQUIRED, IT SHALL INCLUDE AN EASEMENT IN THE AIR FOR THE ACCOMMODATION OF THE ELEVATED HIGHWAY STRUCTURE UNLIMITED IN VERTICAL DIMENSION ABOVE THE STRUCTURE, A SURFACE EASEMENT UNLIMITED IN VERTICAL DIMENSION FOR THE ACCOMMODATION OF PIERS AND OTHER APPURTENANCES AND A TEMPORARY EASEMENT FOR CONSTRUCTION PURPOSES INCLUDING THE STORAGE OF MATERIALS DURING CONSTRUCTION FOR THE ENTIRE AREA. THE FOLLOWING LIMITATIONS SHALL BE IMPOSED ON THE PROPERTY BENEATH THE AREA AFFECTED BY THE AERIAL EASEMENT.

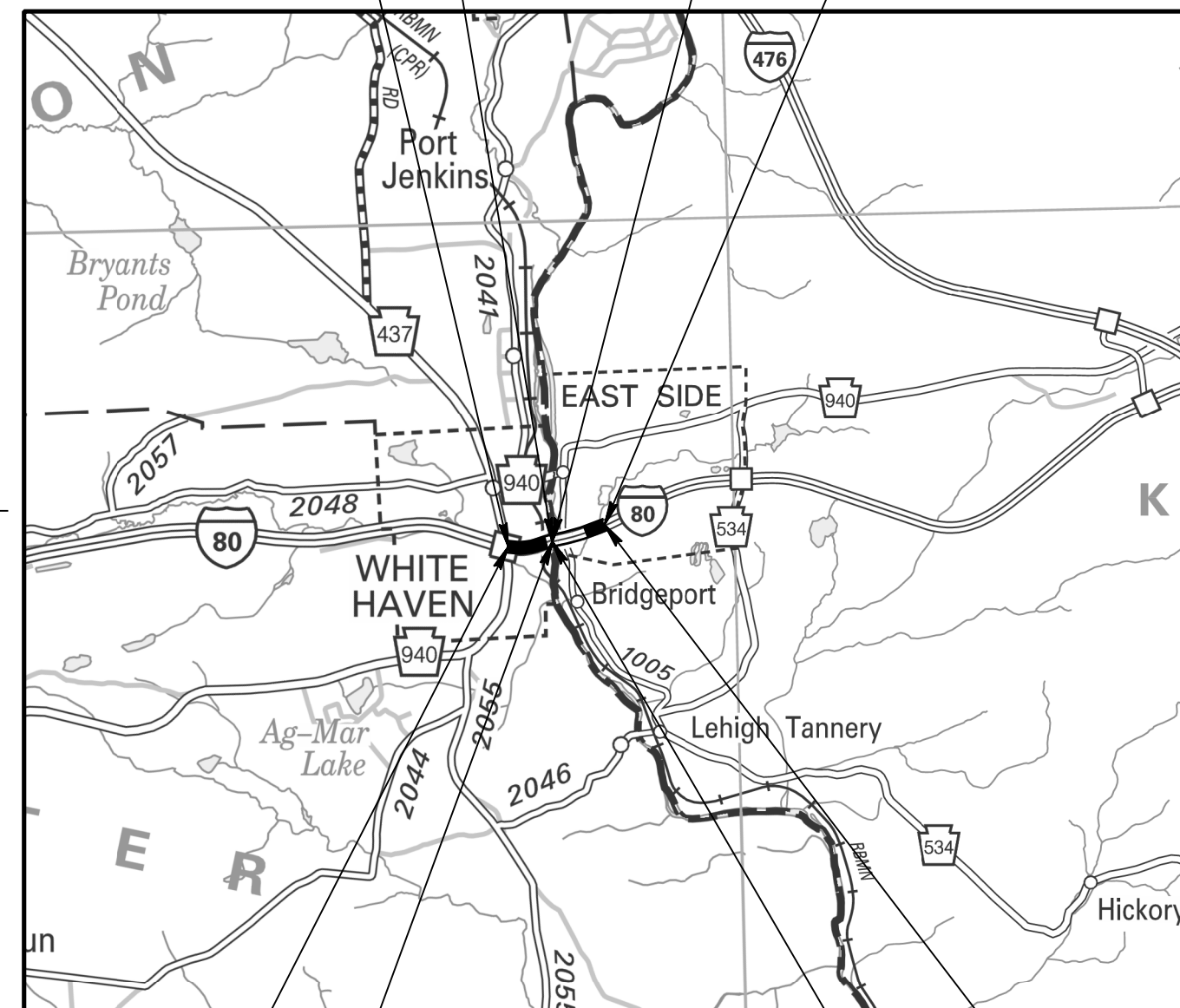
SLOPE EASEMENT. AN EASEMENT FOR THE SUPPORT AND PROTECTION OF THE HIGHWAY, INCLUDING THE RIGHT TO CONSTRUCT, INSPECT, MAINTAIN, REPAIR, RECONSTRUCT AND ALTER DRAINAGE FACILITIES AND THE CONTOUR OF THE LAND. THE EASEMENT SHALL NOT PREVENT THE PROPERTY OWNER FROM MAKING ANY LEGAL USE OF THE AREA WHICH IS NOT DETRIMENTAL TO THE NECESSARY SUPPORT AND PROTECTION OF THE HIGHWAY RIGHT-OF-WAY AND THE SAFETY OF THE TRAVELING PUBLIC.

**LIMIT OF WORK**  
 STA 2218+90.04 WB  
 SEG 2729 OFFSET 0533  
 SR 0080 SEC 08B  
 WHITE HAVEN BOROUGH  
 LUZERNE COUNTY

**LIMIT OF WORK**  
 STA 2218+90.04 WB  
 SEG 2729 OFFSET 0533  
 SR 0080 SEC 08B  
 EAST SIDE BOROUGH  
 CARBON COUNTY

**LIMIT OF WORK**  
 STA 2197+00.00 WB  
 SEG 2721 OFFSET 2330  
 SR 0080 SEC 08B  
 WHITE HAVEN BOROUGH  
 LUZERNE COUNTY

**LIMIT OF WORK**  
 STA 2249+70.00 WB  
 SEG 2731 OFFSET 2253  
 SR 0080 SEC 08B  
 EAST SIDE BOROUGH  
 CARBON COUNTY



**LIMIT OF WORK**  
 STA 1984+55.00 EB  
 SEG 2720 OFFSET 1097  
 SR 0080 SEC 08B  
 WHITE HAVEN BOROUGH  
 LUZERNE COUNTY

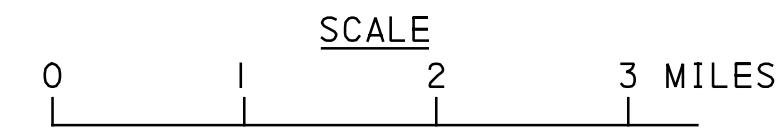
**LIMIT OF WORK**  
 STA 2049+06.00 EB  
 SEG 2730 OFFSET 2204  
 SR 0080 SEC 08B  
 EAST SIDE BOROUGH  
 CARBON COUNTY

**LIMIT OF WORK**  
 STA 2018+90.04 EB  
 SEG 2728 OFFSET 0558  
 SR 0080 SEC 08B  
 WHITE HAVEN BOROUGH  
 LUZERNE COUNTY

**LIMIT OF WORK**  
 STA 2018+90.04 EB  
 SEG 2728 OFFSET 0558  
 SR 0080 SEC 08B  
 EAST SIDE BOROUGH  
 CARBON COUNTY

**LEGEND**

- ===== LIMITED ACCESS HIGHWAY
- ==== STATE ROUTE
- TOWNSHIP ROAD
- TOWNSHIP BOUNDARY
- MUNICIPAL BOUNDARY
- PROJECT



USER: LSAJOM PLOT DRIVER: Pmndot.pdf\_Memo.plt: fsg PLOT DATE: 11-23-2021 12:37:56 PM  
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DISTRICT	COUNTY	ROUTE	SECTION	SHEET
5-0	*	0080	08B	5 OF 26
WHITE HAVEN & EAST SIDE BOROUGHS AND KIDDER TOWNSHIP				
REVISION NUMBER	REVISIONS	DATE	BY	

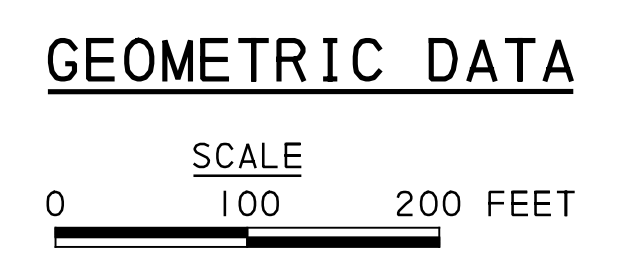
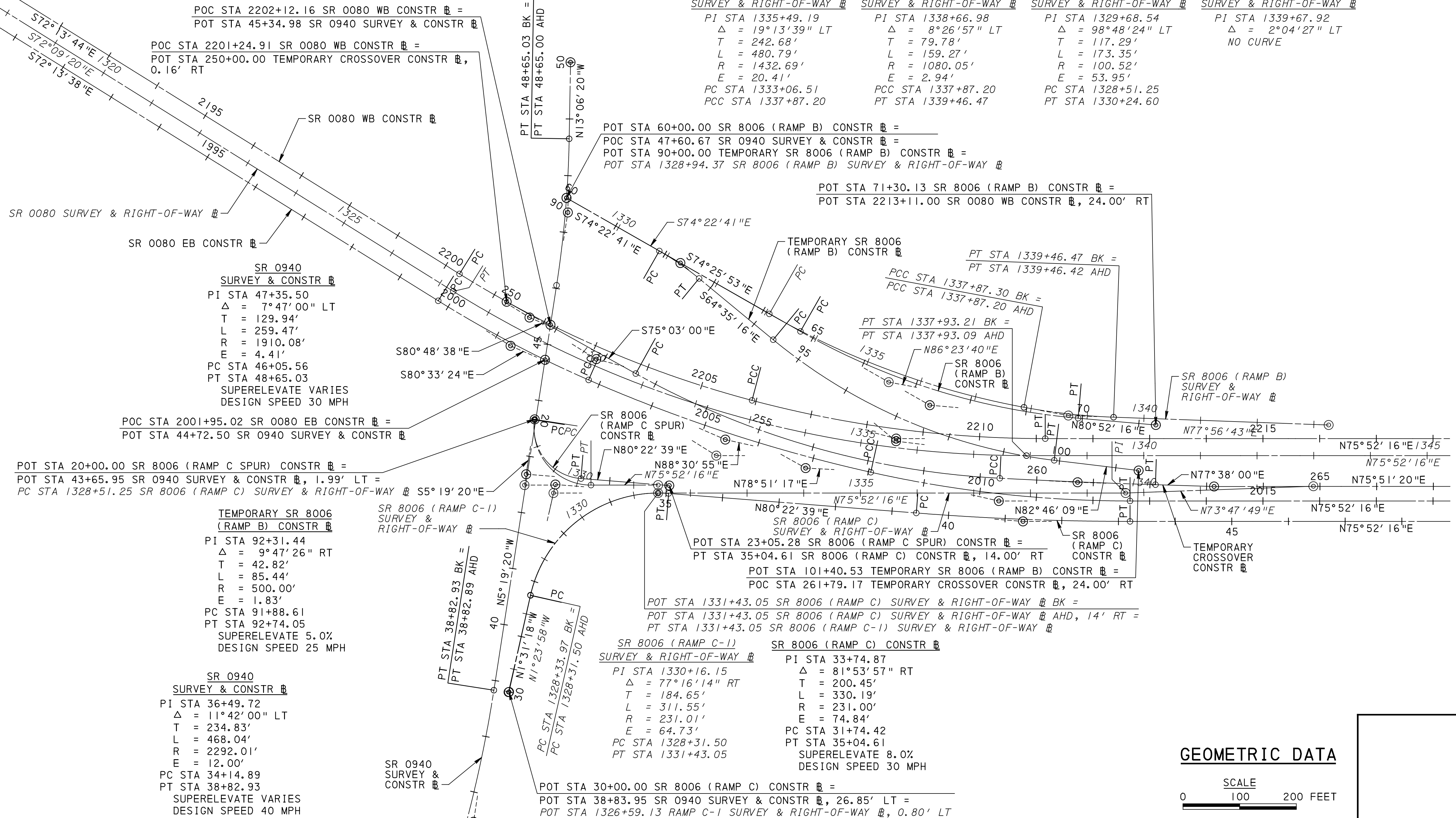
\* LUZERNE COUNTY  
CARBON COUNTY

<b>SR 0080 WB CONSTR</b> PI STA 2203+12.61 $\Delta = 17^{\circ}03'39''$ LT T = 284.28' L = 564.35' R = 1895.25' E = 21.20' PC STA 2200+28.33 PCC STA 2205+92.68 SUPERELEVATE 8% DESIGN SPEED 70 MPH	<b>SR 0080 WB CONSTR</b> PI STA 2208+55.73 $\Delta = 14^{\circ}50'20''$ LT T = 263.05' L = 523.16' R = 2020.00' E = 17.06' PCC STA 2205+92.68 PT STA 2211+15.84 SUPERELEVATE 8.0% DESIGN SPEED 70 MPH	<b>SR 0080 EB CONSTR</b> PI STA 2001+29.94 $\Delta = 8^{\circ}19'47''$ LT T = 150.33' L = 300.13' R = 2064.50' E = 5.47' PC STA 1999+79.61 PCC STA 2002+79.74 SUPERELEVATE N/A DESIGN SPEED 70 MPH	<b>SR 0080 EB CONSTR</b> PI STA 2005+42.79 $\Delta = 10^{\circ}55'41''$ LT T = 263.05' L = 524.51' R = 2750.00' E = 12.55' PCC STA 2002+79.74 PT STA 2008+04.25 SUPERELEVATE 6.8% DESIGN SPEED 70 MPH	<b>SR 0080 EB CONSTR</b> PI STA 2010+35.80 $\Delta = 12^{\circ}38'39''$ LT T = 231.55' L = 461.23' R = 2090.00' E = 12.79' PCC STA 2008+04.25 PT STA 2012+65.48 SUPERELEVATE 7.8% DESIGN SPEED 70 MPH	<b>SR 0080 SURVEY &amp; RIGHT-OF-WAY</b> PI STA 1332+74.54 $\Delta = 31^{\circ}58'24''$ LT T = 547.23' L = 1065.90' R = 1910.08' E = 76.84' PC STA 1327+27.31 PT STA 1337+93.09
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<b>SR 8006 (RAMP B) CONSTR</b> PI STA 67+37.90 $\Delta = 24^{\circ}41'52''$ LT T = 262.71' L = 517.27' R = 1200.00' E = 28.42' PC STA 64+75.18 PT STA 69+92.45 SUPERELEVATE 8.0% DESIGN SPEED 60 MPH	<b>SR 8006 (RAMP C SPUR) CONSTR</b> PI STA 20+97.96 $\Delta = 94^{\circ}18'01''$ LT T = 97.02' L = 148.13' R = 90.00' E = 42.34' PC STA 20+00.94 PT STA 21+49.06 SUPERELEVATE VARIES DESIGN SPEED 20 MPH	<b>SR 8006 (RAMP C) CONSTR</b> PI STA 41+31.81 $\Delta = 4^{\circ}30'23''$ LT T = 188.87' L = 377.54' R = 4800.00' E = 3.71' PC STA 39+42.94 PT STA 43+20.48 SUPERELEVATE 2.6% DESIGN SPEED 50 MPH	<b>TEMPORARY CROSSOVER CONSTR</b> PI STA 256+03.63 $\Delta = 26^{\circ}05'43''$ LT T = 342.98' L = 674.06' R = 1480.00' E = 39.22' PC STA 252+60.65 PCC STA 259+34.72 SUPERELEVATE 8.0% DESIGN SPEED 65 MPH	<b>TEMPORARY CROSSOVER CONSTR</b> PI STA 260+72.22 $\Delta = 1^{\circ}13'17''$ LT T = 137.51' L = 275.00' R = 12900.00' E = 0.73' PCC STA 259+34.72 PT STA 262+09.72 SUPERELEVATE NC DESIGN SPEED 65 MPH	<b>TEMPORARY CROSSOVER CONSTR</b> PI STA 263+13.15 $\Delta = 1^{\circ}46'40''$ LT NO CURVE SUPERELEVATE NC DESIGN SPEED 65 MPH	<b>TEMPORARY SR 8006 (RAMP B) CONSTR</b> PI STA 97+24.71 $\Delta = 32^{\circ}38'35''$ LT T = 281.12' L = 546.94' R = 960.00' E = 40.31' PC STA 94+43.59 PT STA 99+90.53 SUPERELEVATE 8.0% DESIGN SPEED 55 MPH
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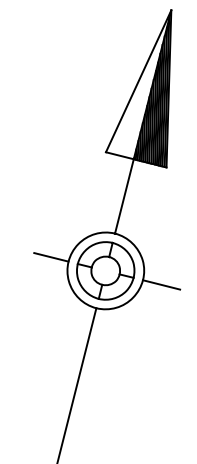
<b>SR 8006 (RAMP B) SURVEY &amp; RIGHT-OF-WAY</b> PI STA 1335+49.19 $\Delta = 19^{\circ}13'39''$ LT T = 242.68' L = 480.79' R = 1432.69' E = 20.41' PC STA 1333+06.51 PCC STA 1337+87.20	<b>SR 8006 (RAMP B) SURVEY &amp; RIGHT-OF-WAY</b> PI STA 1338+66.98 $\Delta = 8^{\circ}26'57''$ LT T = 79.78' L = 159.27' R = 1080.05' E = 2.94' PCC STA 1337+87.20 PT STA 1339+46.47	<b>SR 8006 (RAMP C) SURVEY &amp; RIGHT-OF-WAY</b> PI STA 1329+68.54 $\Delta = 98^{\circ}48'24''$ LT T = 117.29' L = 173.35' R = 100.52' E = 53.95' PC STA 1328+51.25 PT STA 1330+24.60	<b>SR 8006 (RAMP C) SURVEY &amp; RIGHT-OF-WAY</b> PI STA 1339+67.92 $\Delta = 2^{\circ}04'27''$ LT NO CURVE
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**SR 0080 SURVEY & RIGHT-OF-WAY**  
 PI STA 1303+92.13  
 $\Delta = 26^{\circ}55'05''$  RT  
 T = 783.58'  
 L = 1538.23'  
 R = 3274.17'  
 E = 92.46'  
 PC STA 1296+08.55  
 PT STA 1311+46.78



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DISTRICT	COUNTY	ROUTE	SECTION	SHEET	
5-0	*	0080	08B	6 OF 26	
WHITE HAVEN & EAST SIDE BOROUGHS AND KIDDER TOWNSHIP					
REVISION NUMBER	REVISIONS			DATE	BY



SR 1005 SURVEY #  
PI STA 22+90.45  
 $\Delta = 7^{\circ}45'43''$  RT  
T = 73.20'  
L = 146.17'  
R = 1079.00'  
E = 2.48'  
PC STA 22+17.25  
PT STA 23+63.42

SR 0080 WB CONSTR #  
PI STA 2224+58.96  
 $\Delta = 3^{\circ}05'48''$  LT  
T = 174.61'  
L = 349.13'  
R = 6460.00'  
E = 2.36'  
PC STA 2222+84.35  
PCC STA 2226+33.48  
SUPERELEVATE 3.2%  
DESIGN SPEED 70 MPH

SR 0080 WB CONSTR #  
PI STA 2232+17.85  
 $\Delta = 11^{\circ}30'14''$  LT  
T = 584.37'  
L = 1164.81'  
R = 5801.43'  
E = 29.36'  
PC STA 2226+33.48  
PT STA 2237+98.30  
SUPERELEVATE N/A  
DESIGN SPEED 70 MPH

SR 0080 WB SURVEY & RIGHT-OF-WAY #  
PI STA 11+73.30  
 $\Delta = 14^{\circ}37'35''$  LT  
T = 735.33'  
L = 1462.66'  
R = 5729.65'  
E = 46.99'  
PC STA 4+37.97  
PT STA 19+00.63

SR 1005 SURVEY #  
PI STA 20+37.71  
 $\Delta = 5^{\circ}42'46''$  RT  
T = 67.52'  
L = 134.92'  
R = 1353.14'  
E = 1.68'  
PC STA 19+70.19  
PT STA 21+05.11

TEMPORARY CROSSOVER CONSTR #  
PI STA 276+35.93  
 $\Delta = 14^{\circ}35'06''$  LT  
T = 318.64'  
L = 633.84'  
R = 2490.00'  
E = 20.31'  
PC STA 273+17.28  
PT STA 279+51.13  
SUPERELEVATE 6.4%  
DESIGN SPEED 65 MPH

SR 0080 EB CONSTR #  
PI STA 2025+76.65  
 $\Delta = 7^{\circ}35'47''$  LT  
T = 335.26'  
L = 669.53'  
R = 5050.00'  
E = 11.12'  
PC STA 2022+41.39  
PT STA 2029+10.92  
SUPERELEVATE 4.0%  
DESIGN SPEED 70 MPH

SR 0080 EB CONSTR #  
PI STA 2039+30.35  
 $\Delta = 6^{\circ}58'27''$  LT  
T = 466.71'  
L = 932.26'  
R = 7658.95'  
E = 14.21'  
PC STA 2034+63.65  
PT STA 2043+95.91  
SUPERELEVATE VAR  
DESIGN SPEED 70 MPH

SR 0080 SURVEY & RIGHT-OF-WAY #  
PI STA 15+33.57  
 $\Delta = 14^{\circ}37'35''$  LT  
T = 980.43'  
L = 1950.20'  
R = 7639.49'  
E = 62.66'  
PC STA 5+53.14  
PT STA 25+03.34

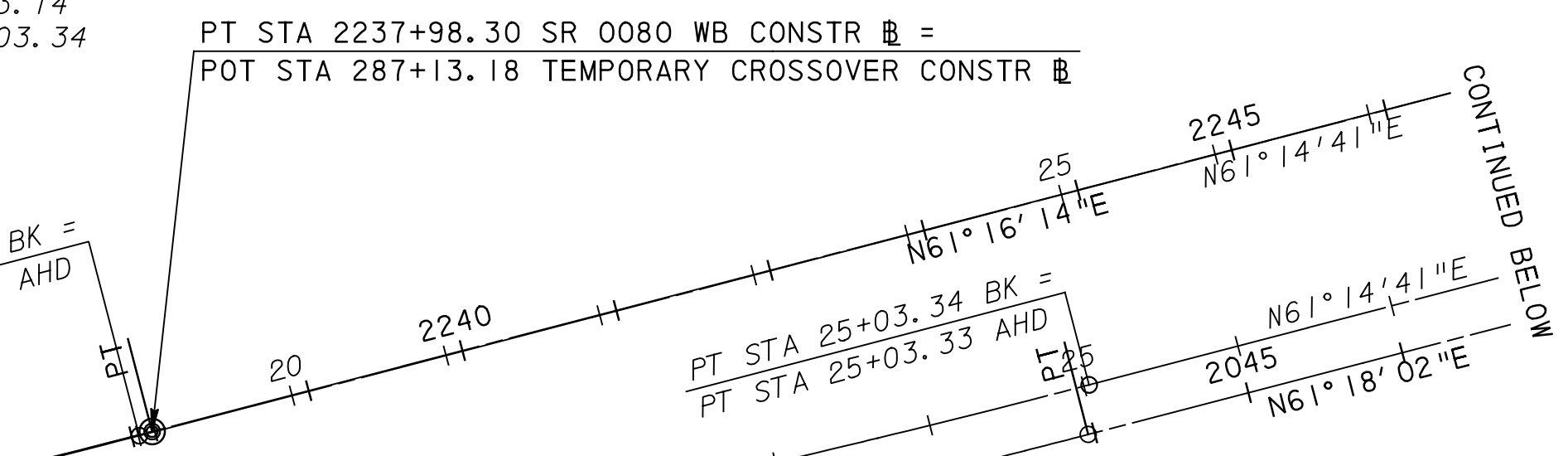
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PI STA 16+31.99  
 $\Delta = 4^{\circ}25'40''$  RT  
T = 46.97'  
L = 93.89'  
R = 1214.91'  
E = 0.91'  
PCC STA 15+85.02  
PT STA 16+78.91

SR 1005 SURVEY #  
PI STA 10+12.26  
 $\Delta = 10^{\circ}29'22''$  LT  
T = 112.75'  
L = 224.86'  
R = 1228.25'  
E = 5.16'  
PCC STA 8+99.51  
PT STA 11+24.37

SR 1005 SURVEY #  
PI STA 15+44.52  
 $\Delta = 10^{\circ}34'08''$  RT  
T = 40.74'  
L = 81.25'  
R = 440.45'  
E = 1.88'  
PCC STA 15+03.78  
PCC STA 15+85.02

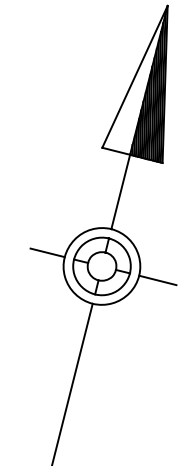
SR 1005 SURVEY #  
PI STA 14+18.05  
 $\Delta = 7^{\circ}33'55''$  RT  
T = 85.97'  
L = 171.70'  
R = 1300.35'  
E = 2.84'  
PC STA 13+32.08  
PCC STA 15+03.78

SR 0080 WB CONSTR #  
PI STA 2253+98.00  
 $\Delta = 9^{\circ}50'54''$  RT  
T = 493.64'  
L = 984.86'  
R = 5729.65'  
E = 21.23'  
PC STA 2249+04.36  
PT STA 2258+89.22  
SUPERELEVATE N/A  
DESIGN SPEED 70 MPH

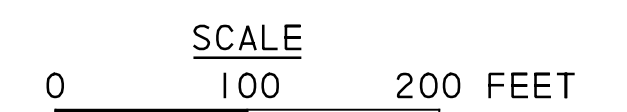


CONTINUE ON SHEET 5 OF 26

CONTINUED BELOW



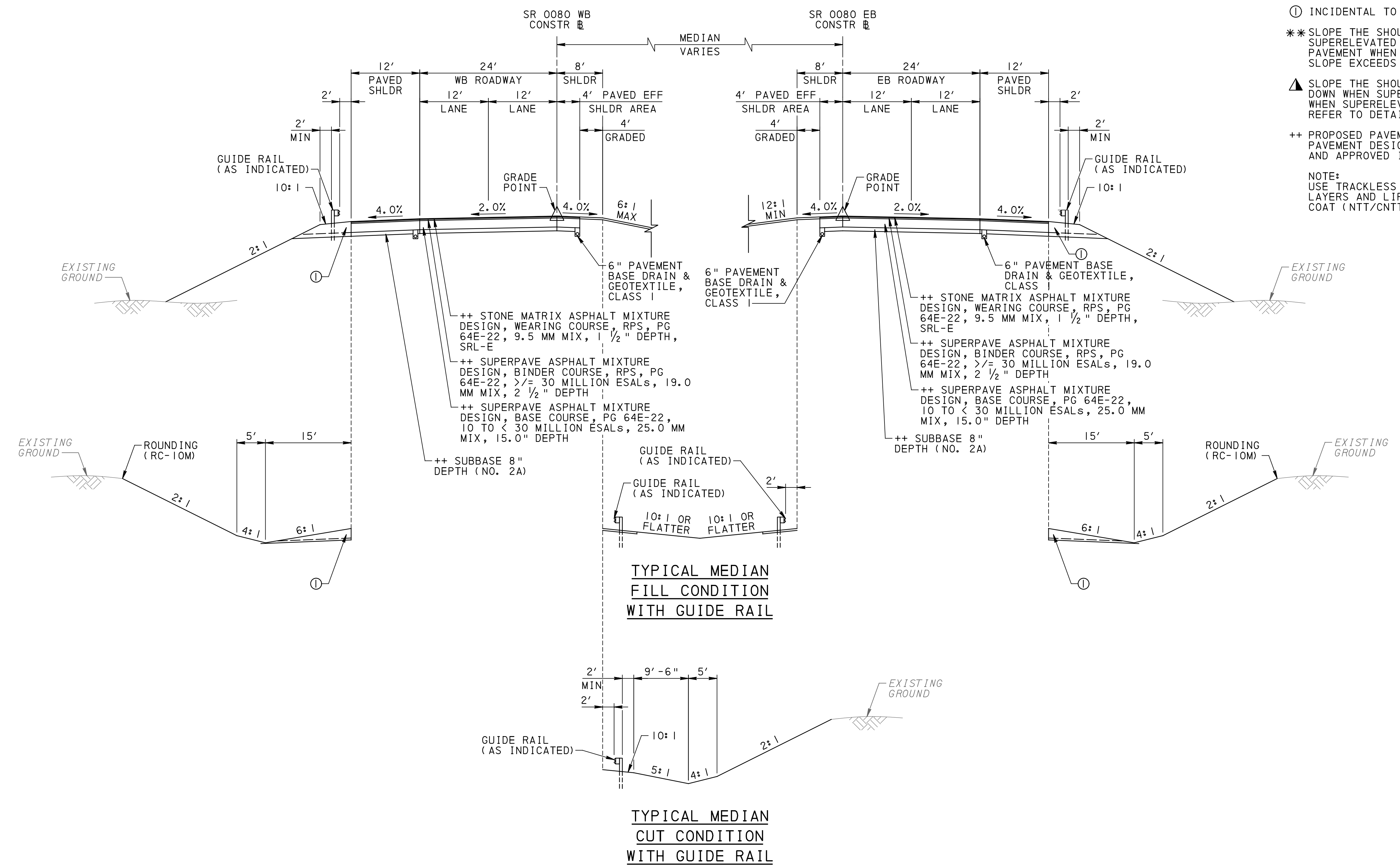
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DISTRICT	COUNTY	ROUTE	SECTION	SHEET
5-0	*	0080	08B	8 OF 26
WHITE HAVEN & EAST SIDE BOROUGHS AND KIDDER TOWNSHIP				
REVISION NUMBER	REVISIONS	DATE	BY	



- ① INCIDENTAL TO SUBBASE (NO. 2A)
- \*\* SLOPE THE SHOULDER ON THE LOW SIDE OF A SUPERELEVATED SECTION THE SAME RATE AS THE PAVEMENT WHEN THE RATE OF PAVEMENT CROSS SLOPE EXCEEDS 4.0%
- ▲ SLOPE THE SHOULDER ON THE HIGH SIDE AT 2.0% DOWN WHEN SUPERELEVATION IS 2.0% TO 6.0%. WHEN SUPERELEVATION IS GREATER THAN 6.0% REFER TO DETAIL A.
- ++ PROPOSED PAVEMENT SHOWN IS PRELIMINARY. A PAVEMENT DESIGN WILL NEED TO BE COMPLETED AND APPROVED IN FINAL DESIGN.
- NOTE:  
USE TRACKLESS TACK COAT BETWEEN ALL ASPHALT LAYERS AND LIFTS (0460-003 ASPHALT TACK COAT (NTT/CNTT))

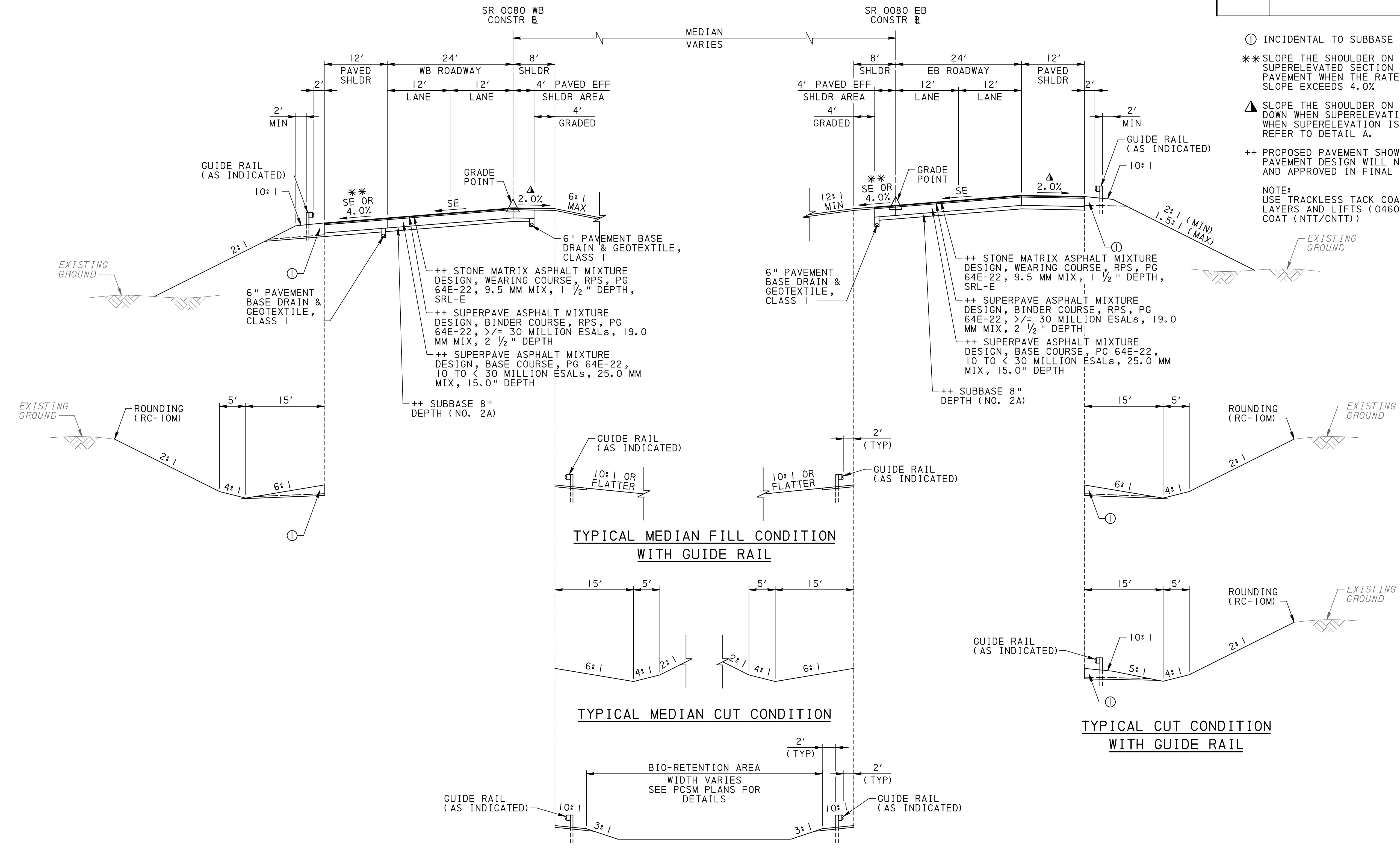
**SR 0080 TYPICAL TANGENT SECTION**  
NOT TO SCALE  
SR 0080 STA 2212+96.00 TO STA 2213+21.37 WB  
SR 0080 STA 2221+51.37 TO STA 2222+48.00 WB

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 FILE: 008008B-RD-TYP01.dgn



\* LUZERNE COUNTY  
CARBON COUNTY

DISTRICT	COUNTY	ROUTE	SECTION	SHEET	
5-0	*	0080	08B	9 OF 26	
WHITE HAVEN & EAST SIDE BOROUGHS AND KIDDER TOWNSHIP					
REVISION NUMBER	REVISIONS			DATE	BY



- ① INCIDENTAL TO SUBBASE (NO. 2A)
- \*\* SLOPE THE SHOULDER ON THE LOW SIDE OF A SUPERELEVATED SECTION THE SAME RATE AS THE PAVEMENT WHEN THE RATE OF PAVEMENT CROSS SLOPE EXCEEDS 4.0%
- ▲ SLOPE THE SHOULDER ON THE HIGH SIDE AT 2.0% DOWN WHEN SUPERELEVATION IS 2.0% TO 6.0%. WHEN SUPERELEVATION IS GREATER THAN 6.0% REFER TO DETAIL A.
- ++ PROPOSED PAVEMENT SHOWN IS PRELIMINARY. A PAVEMENT DESIGN WILL NEED TO BE COMPLETED AND APPROVED IN FINAL DESIGN.
- NOTE: USE TRACKLESS TACK COAT BETWEEN ALL ASPHALT LAYERS AND LIFTS (0460-0003 ASPHALT TACK COAT (NTT/CNTT))

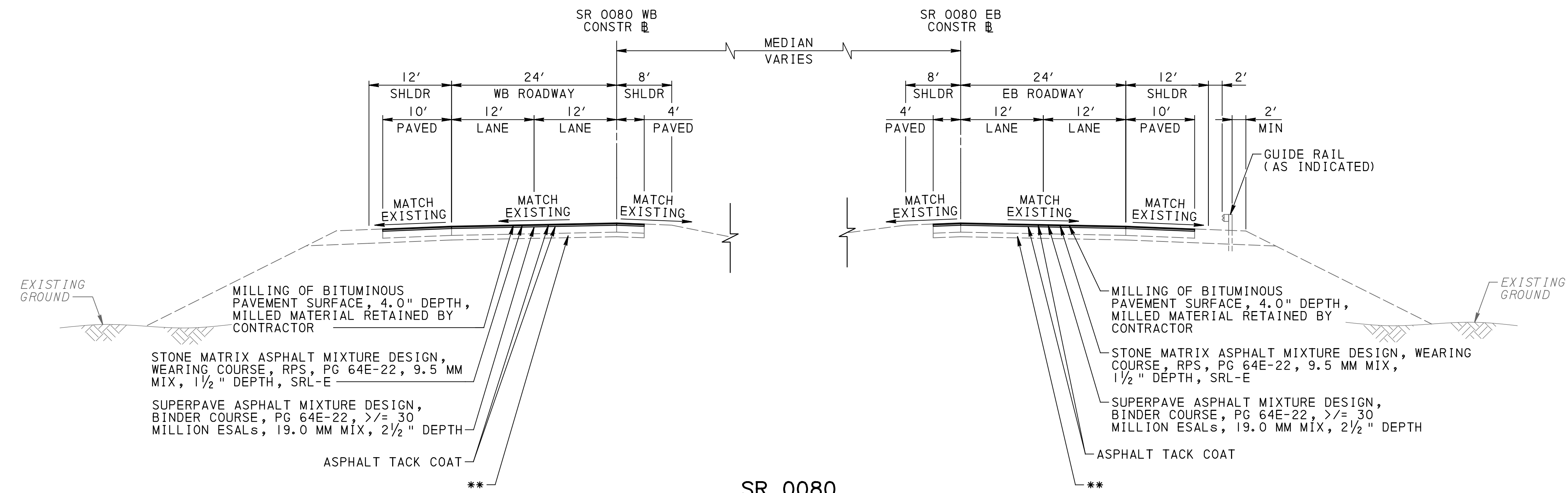
**SR 0080 TYPICAL SUPERELEVATED SECTION**

NOT TO SCALE  
 SR 0080 STA 2002+75.00 TO STA 2012+76.39 EB  
 SR 0080 STA 2021+91.39 TO STA 2034+75.00 EB  
 SR 0080 STA 2205+50.00 TO STA 2212+96.00 WB  
 SR 0080 STA 2222+48.00 TO STA 2227+00.00 WB

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\* LUZERNE COUNTY  
CARBON COUNTY

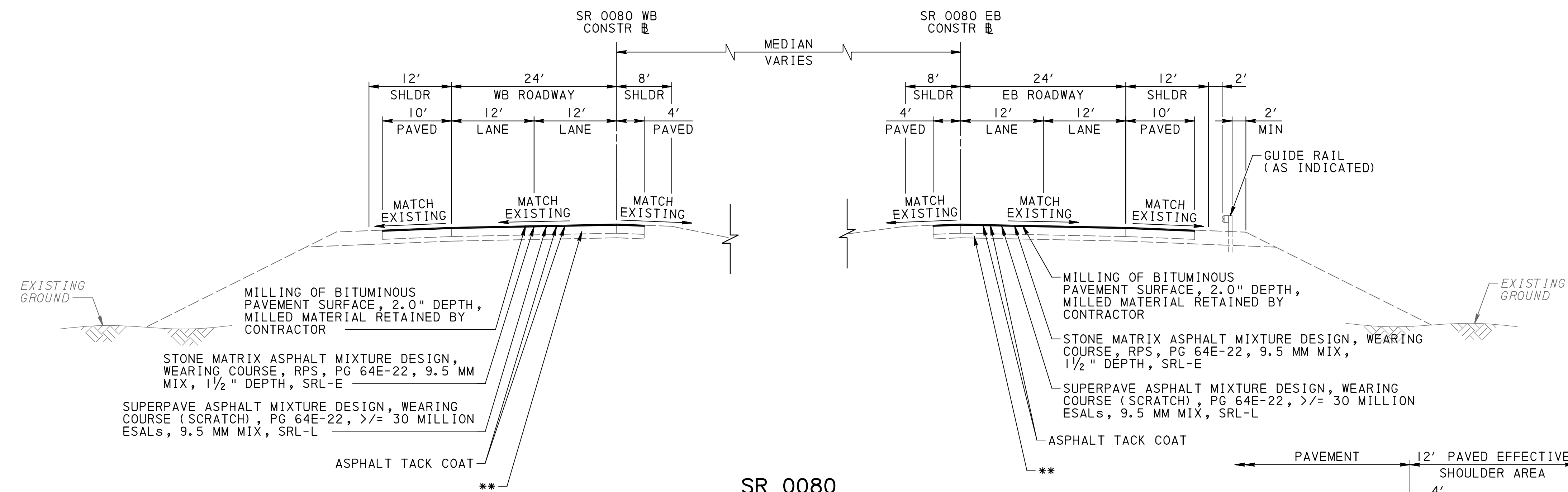
DISTRICT	COUNTY	ROUTE	SECTION	SHEET	
5-0	*	0080	08B	10 OF 26	
WHITE HAVEN & EAST SIDE BOROUGHS AND KIDDER TOWNSHIP					
REVISION NUMBER	REVISIONS			DATE	BY



**SR 0080**  
**TYPICAL MILL AND OVERLAY SECTION (LUZERNE COUNTY)**  
NOT TO SCALE

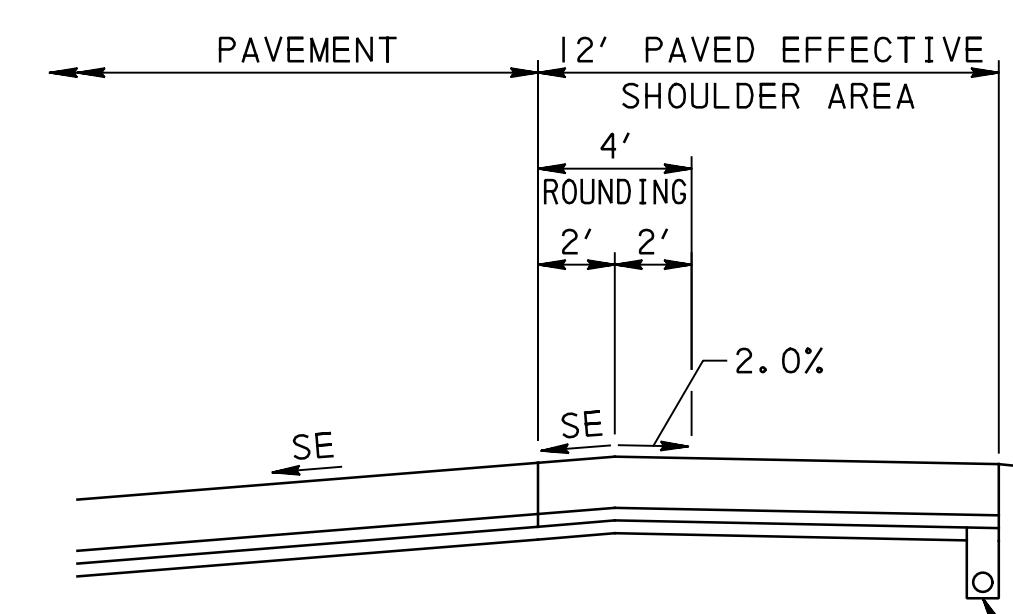
SR 0080 STA 1999+35.00 EB TO STA 2002+75.00 EB  
SR 0080 STA 2198+60.00 WB TO STA 2205+50.00 WB

- \*\* EXISTING PAVEMENT (LUZERNE COUNTY)  
STA 1999+35.00 EB TO STA 2002+75.00 EB AND  
STA 2198+60.00 WB TO STA 2203+15.00 WB
- STONE MATRIX ASPHALT MIXTURE DESIGN, HMA WEARING COURSE, RPS, PG 76-22, >= 30 MILLION ESALS, 9.5 MM MIX, 1 1/2" DEPTH, SRL-E
- ULTRA-THIN BONDED WEARING COURSE, GRADATION TYPE C, PG 76-22, SRL-E, 1" DEPTH
- PLAIN CEMENT CONCRETE PAVEMENT, 13" DEPTH
- OGS SUBBASE, 4" DEPTH
- SUBBASE, NO 2A, 8" DEPTH
- \*\* EXISTING PAVEMENT (CARBON COUNTY)  
STA 2034+75.00 EB TO STA 2038+15.00 EB AND  
STA 2227+00.00 WB TO STA 2240+00.00 WB
- STONE MATRIX ASPHALT MIXTURE DESIGN, WMA WEARING COURSE, RPS, PG 76-22, 9.5 MM MIX, 1 1/2" DEPTH, SRL-E
- SUPERPAVE ASPHALT MIXTURE DESIGN, WMA BINDER COURSE, RPS, PG 76-22, >= 30 MILLION ESALS, 19.0 MM MIX, 2 1/2" DEPTH
- SUPERPAVE ASPHALT MIXTURE DESIGN, WMA BASE COURSE, PG 64+22, >= 30 MILLION ESALS, 25.0 MM MIX, 5" DEPTH
- REINFORCED CONCRETE CEMENT PAVEMENT, 13" DEPTH, CRACKED AND SEATED
- OGS SUBBASE, 12" DEPTH



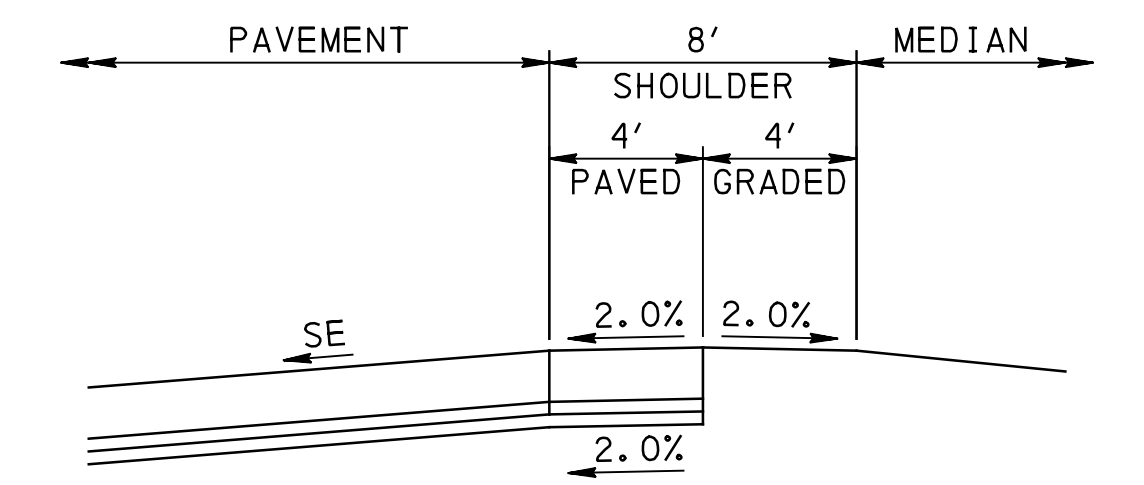
**SR 0080**  
**TYPICAL MILL AND OVERLAY SECTION (CARBON COUNTY)**  
NOT TO SCALE

SR 0080 STA 2034+75.00 EB TO STA 2038+15.00 EB  
SR 0080 STA 2227+00.00 WB TO STA 2240+00.00 WB



OUTSIDE SHOULDER

**DETAIL A**  
**SE > 6.0%**  
NOT TO SCALE

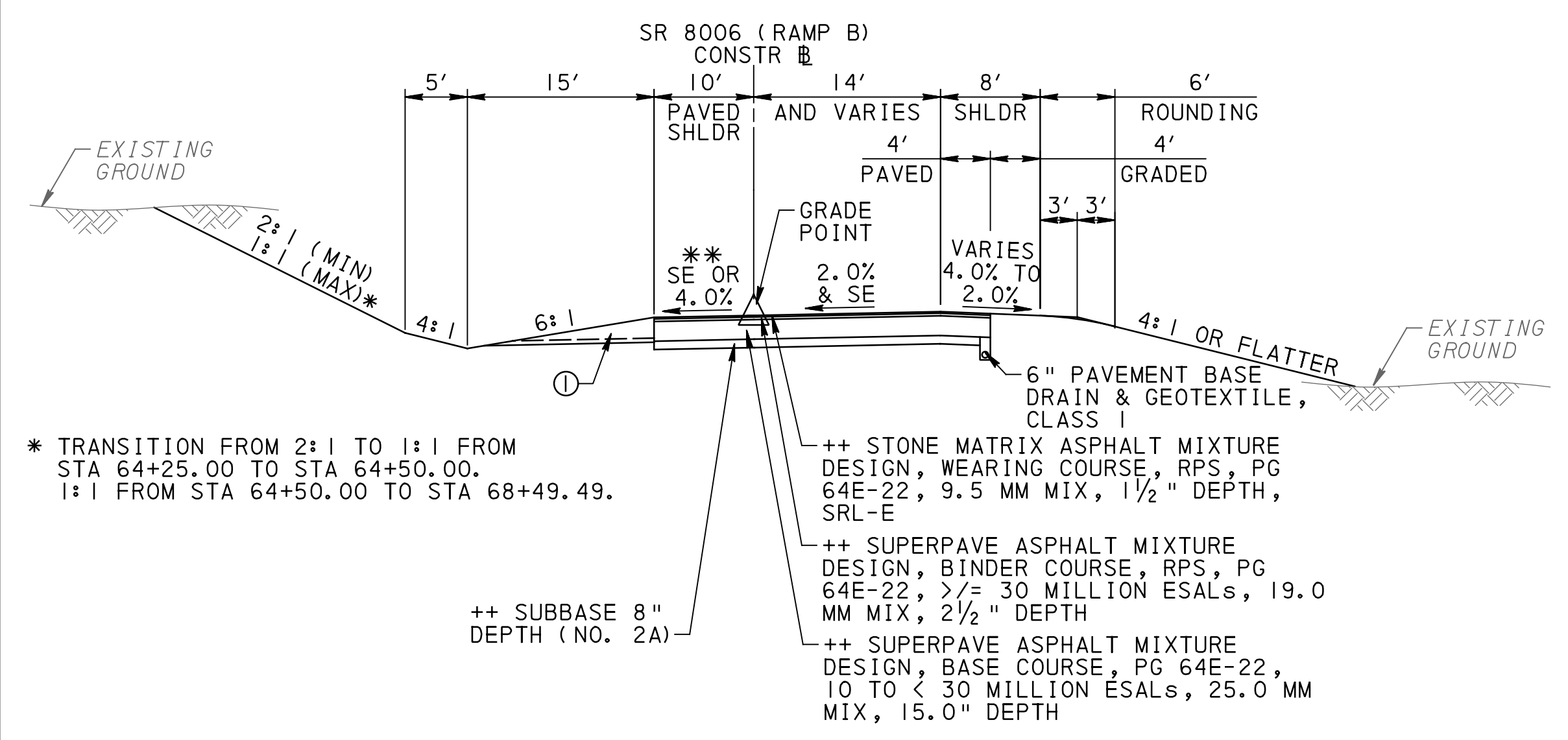


MEDIAN SHOULDER

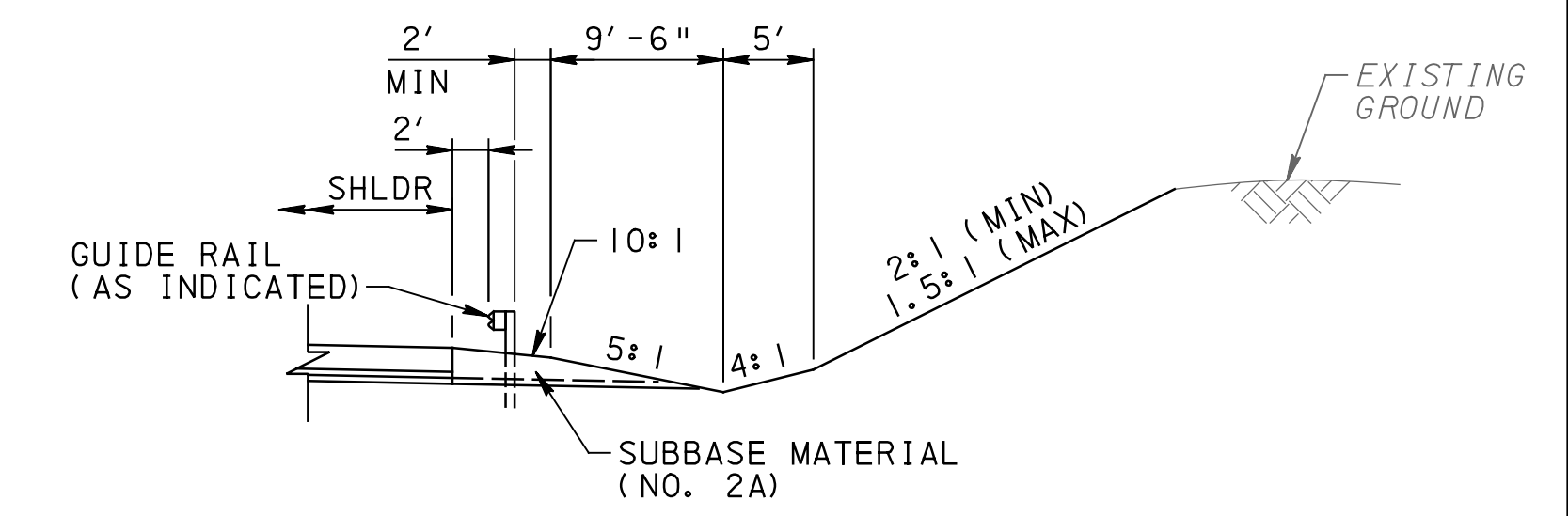
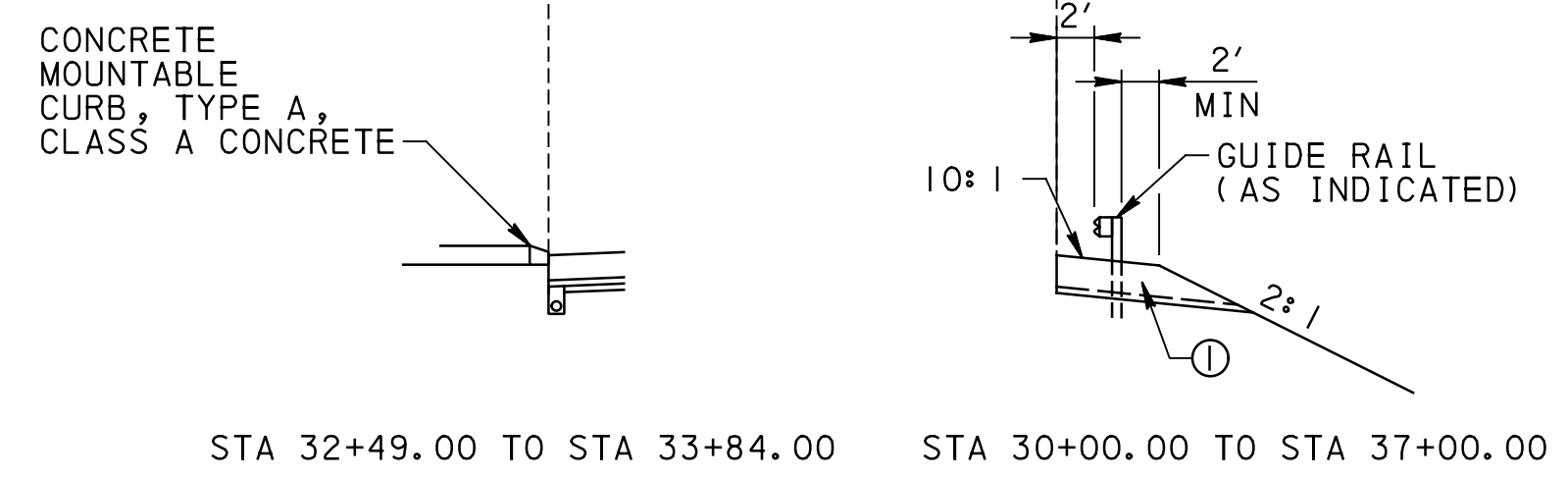
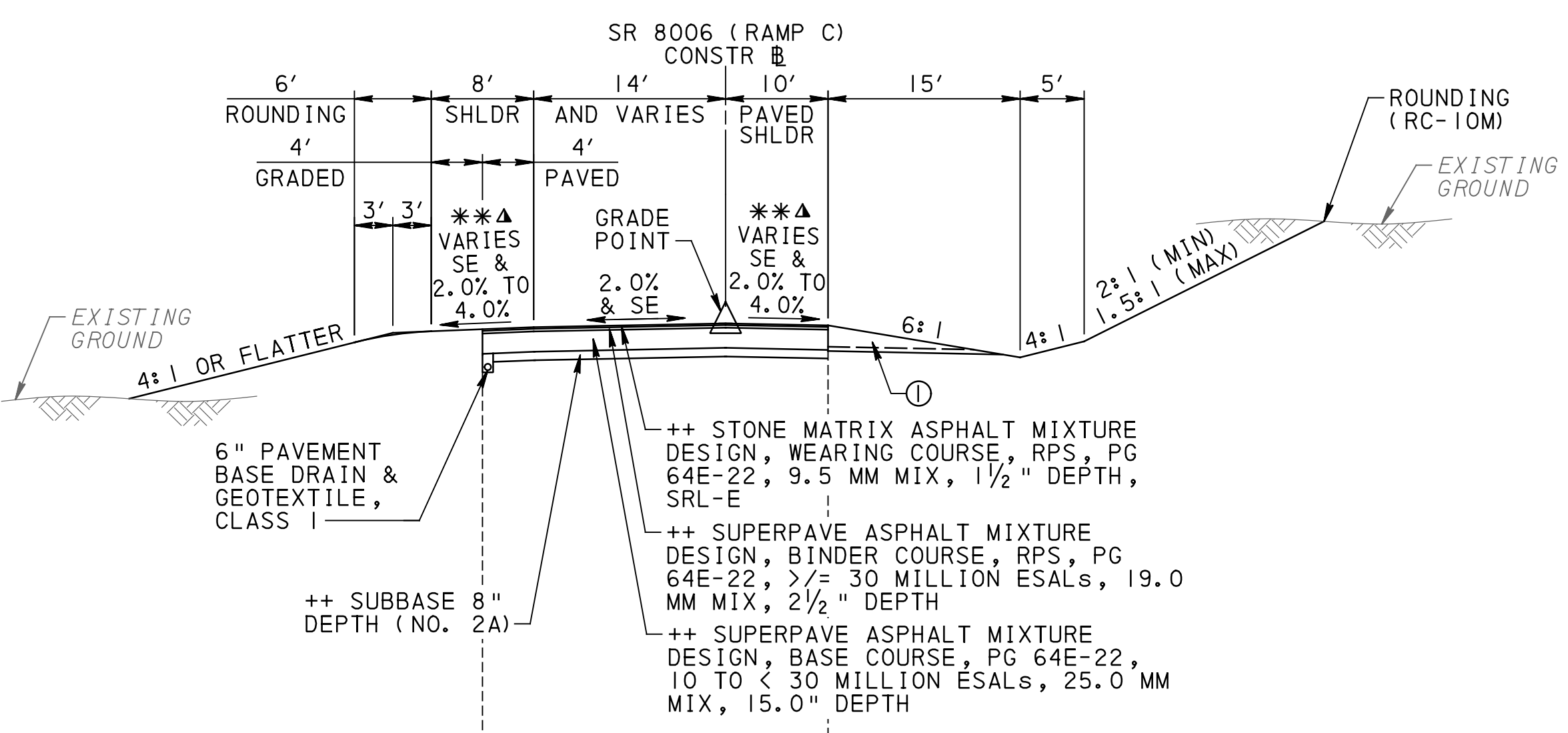
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PLOT DATE: 11-23-2021 12:43:46 PM  
MODEL: 08B.fault

\* LUZERNE COUNTY  
CARBON COUNTY

DISTRICT	COUNTY	ROUTE	SECTION	SHEET	
5-0	*	0080	08B	11 OF 26	
WHITE HAVEN & EAST SIDE BOROUGHS AND KIDDER TOWNSHIP					
REVISION NUMBER	REVISIONS			DATE	BY

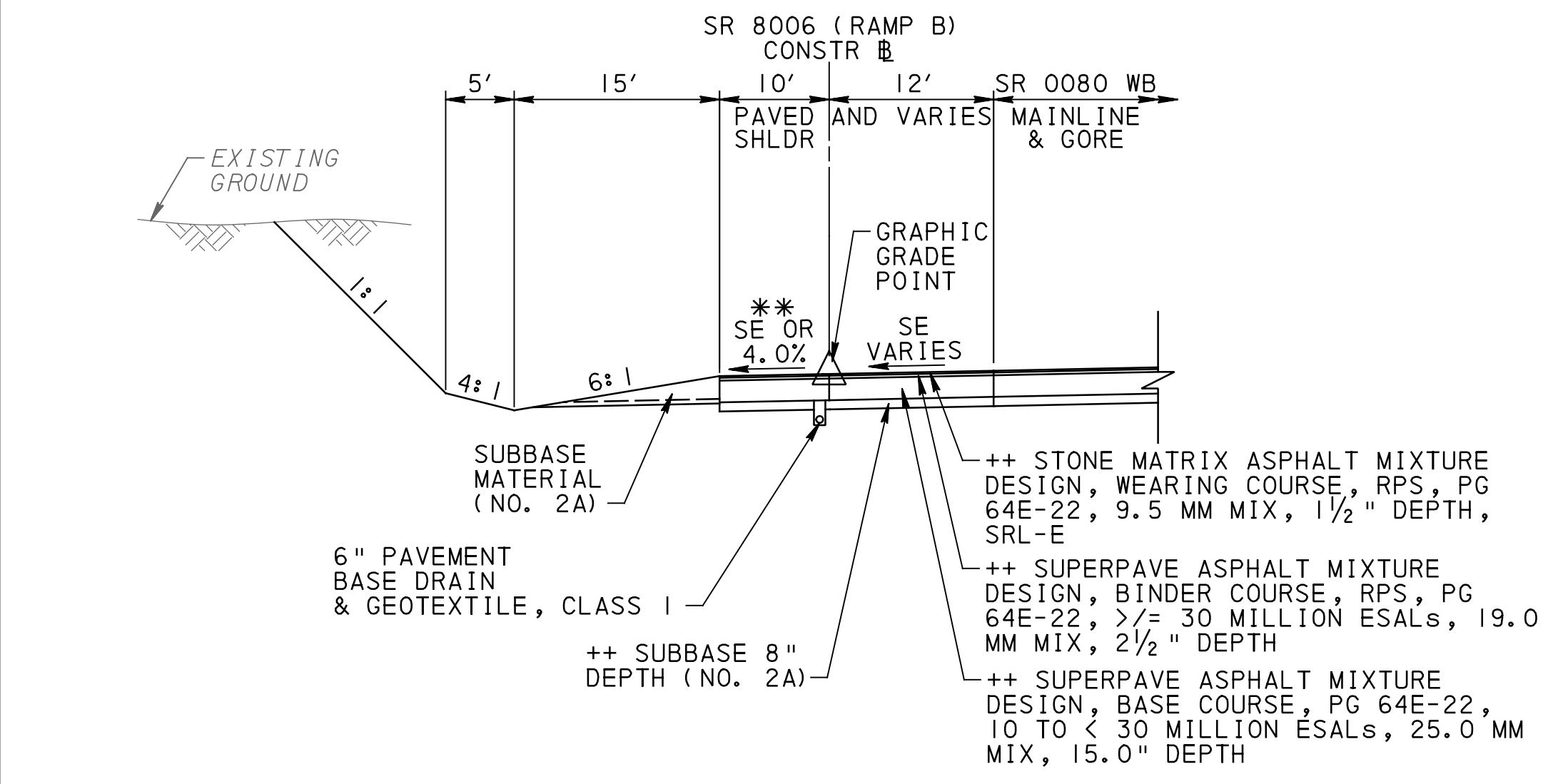


\* TRANSITION FROM 2:1 TO 1:1 FROM STA 64+25.00 TO STA 64+50.00.  
1:1 FROM STA 64+50.00 TO STA 68+49.49.

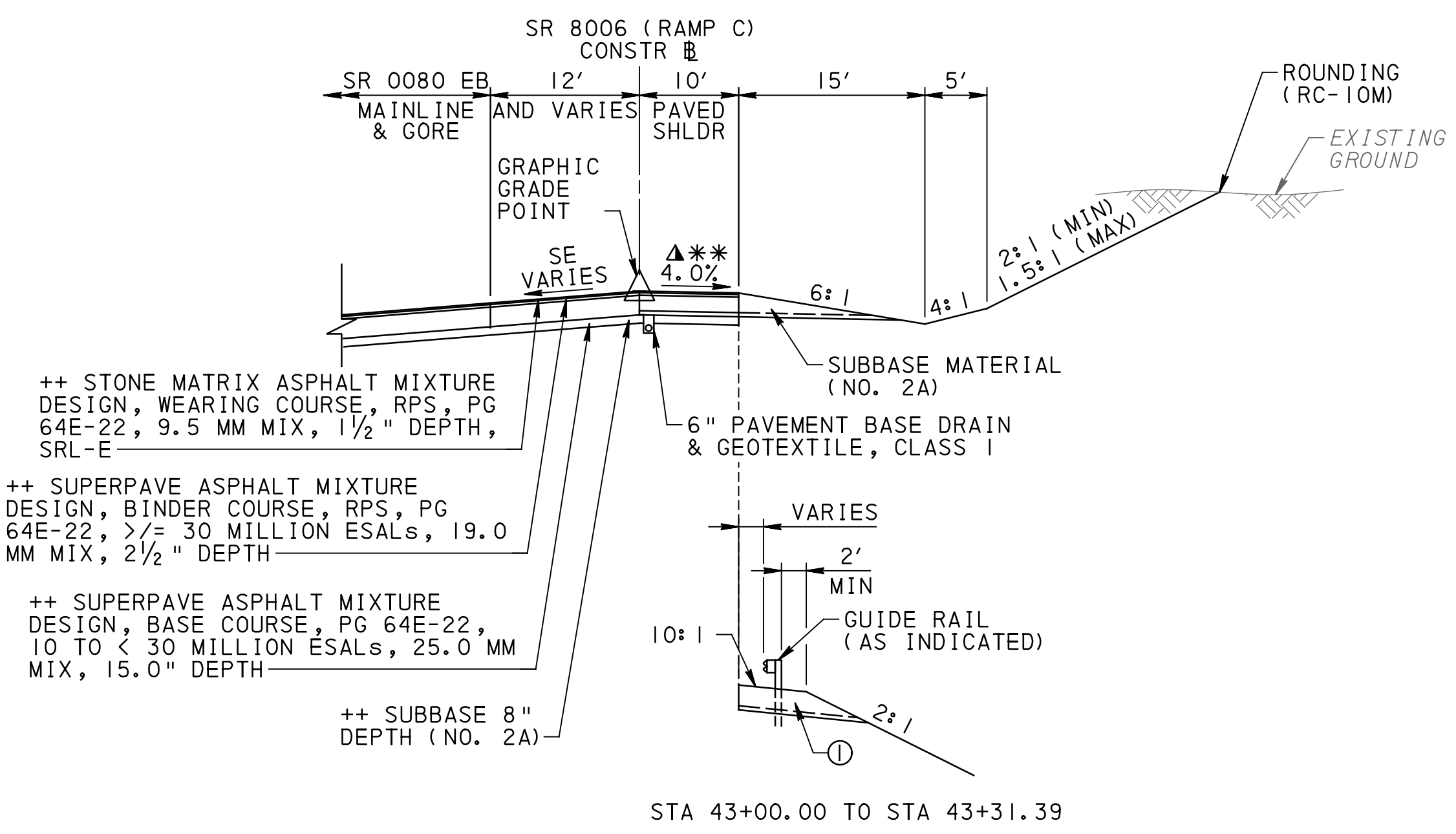


SR 8006 (RAMP C) TYPICAL SECTION  
NOT TO SCALE  
STA 30+00.00 TO STA 39+49.87

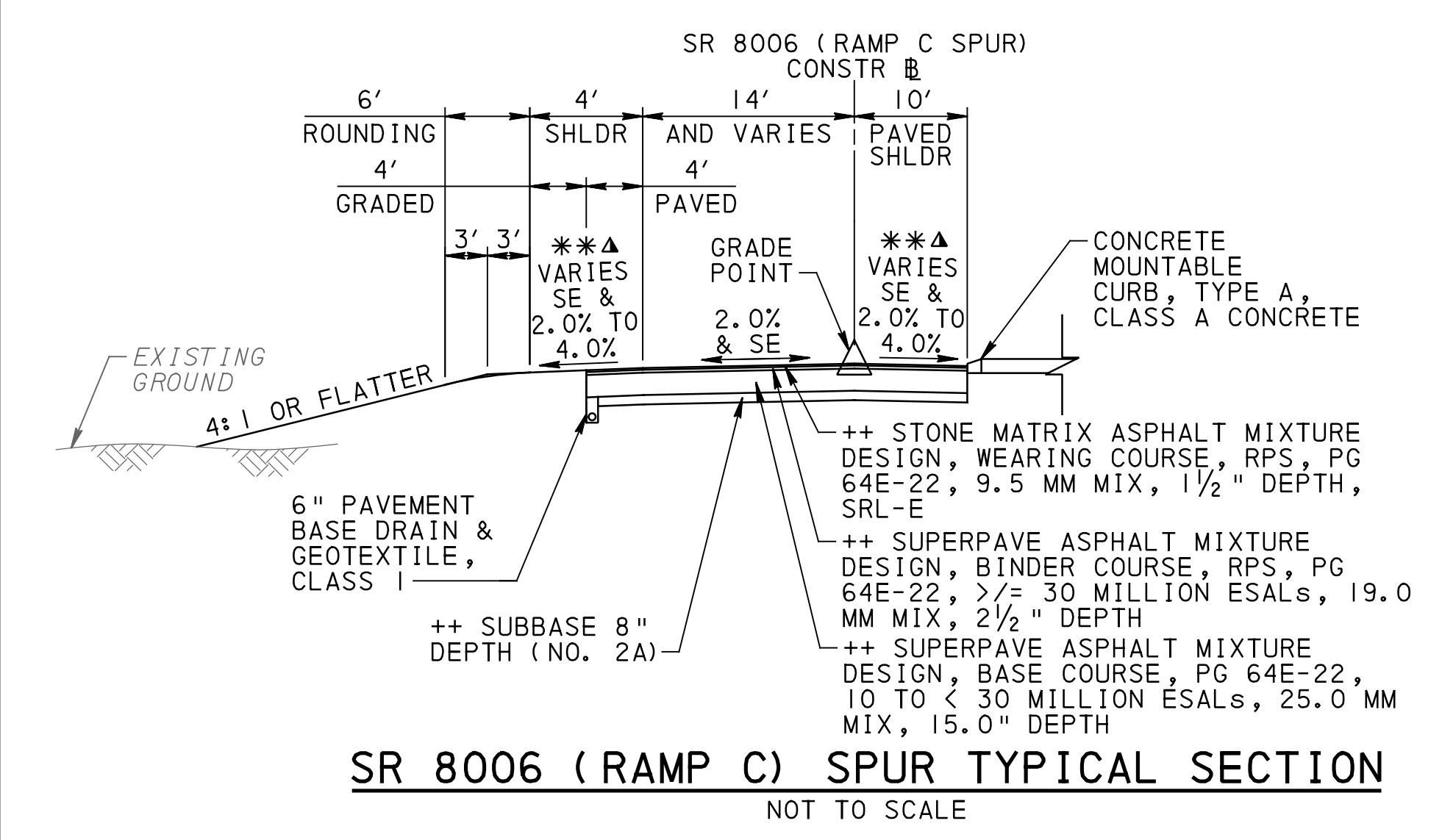
TYPICAL RAMP CUT CONDITION WITH GUIDE RAIL  
NOT TO SCALE



SR 8006 (RAMP B) TYPICAL SUPERELEVATED SECTION ADJACENT TO MAINLINE PAVEMENT  
NOT TO SCALE  
STA 68+49.49 TO STA 71+30.13



SR 8006 (RAMP C) TYPICAL SUPERELEVATED SECTION ADJACENT TO MAINLINE PAVEMENT  
NOT TO SCALE  
STA 39+49.87 TO STA 43+31.39

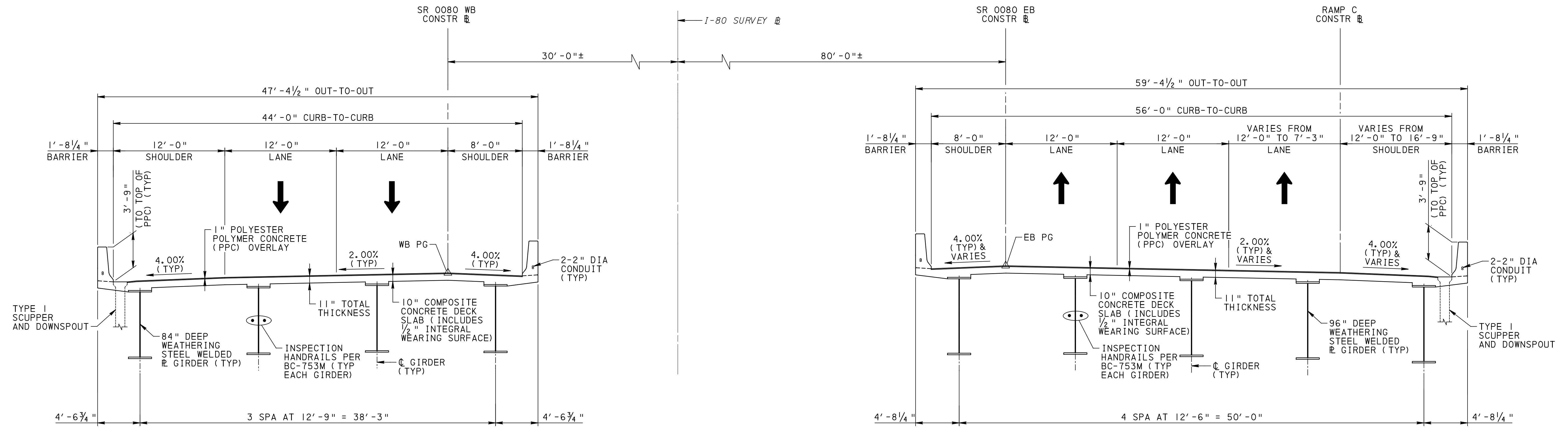


SR 8006 (RAMP C) SPUR TYPICAL SECTION  
NOT TO SCALE  
STA 20+73.61 TO STA 23+05.28

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 PLOT DATE: 11-23-2021 12:43:49 PM  
 MODEL: 08B.fault

\* LUZERNE COUNTY  
CARBON COUNTY

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
5-0	*	0080	08B	12 OF 26
WHITE HAVEN & EAST SIDE BOROUGHS AND KIDDER TOWNSHIP				
REVISION NUMBER	REVISIONS	DATE	BY	

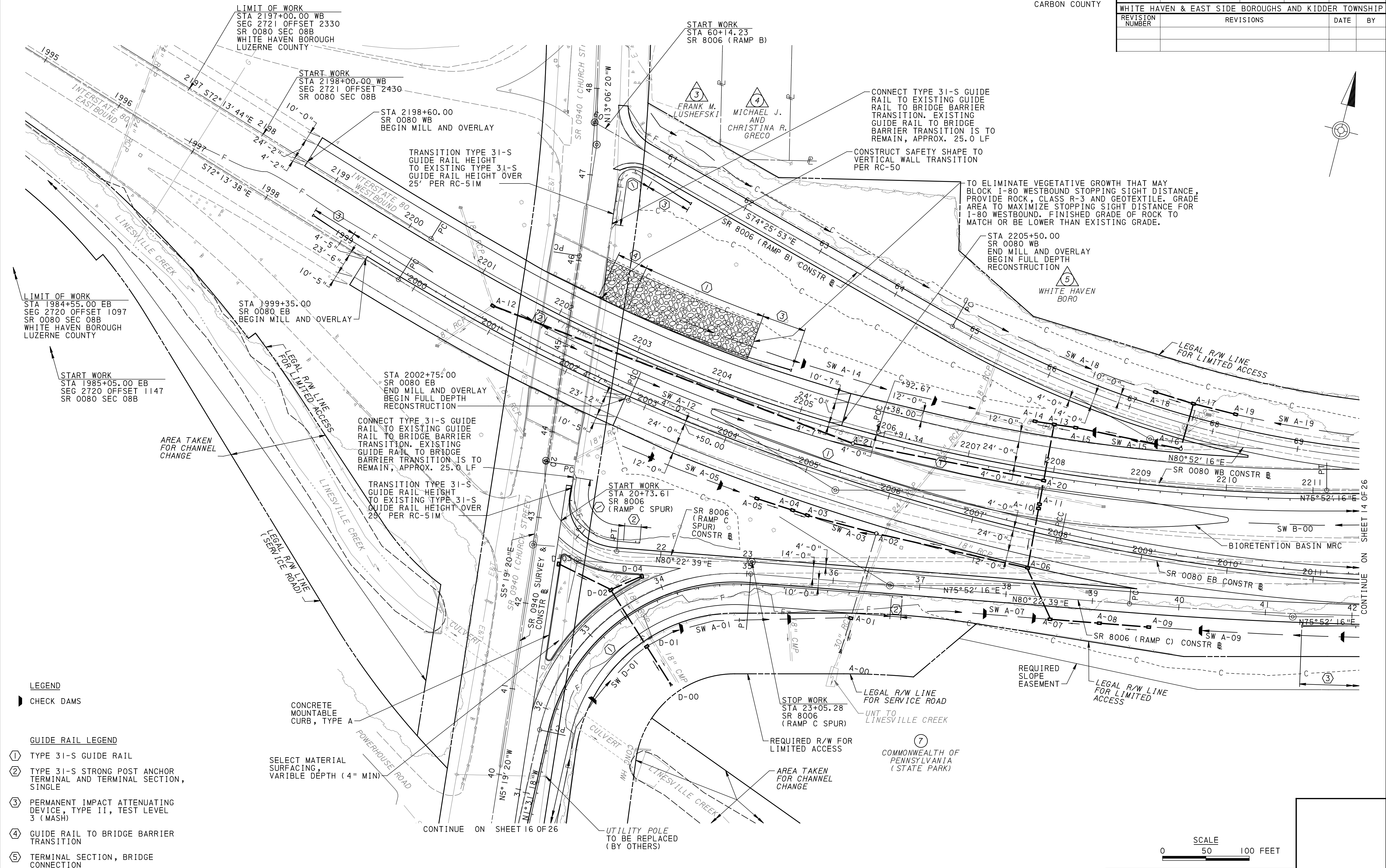
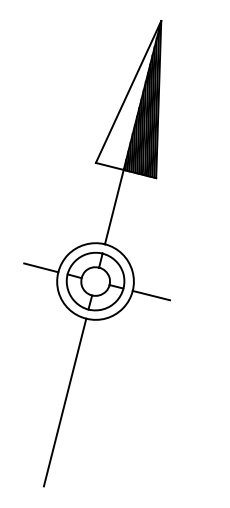


**BRIDGE TYPICAL SECTION**  
NOT TO SCALE  
STA 2012+76.39 EB TO STA 2021+91.39 EB  
STA 2213+21.37 WB TO STA 2221+51.37 WB

USER: LSALOUIM PLOT DRIVER: Penndot\_PDF\_Memo.plt PLOT DATE: 11-23-2021 12:43:52 PM  
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 MODEL: DeFault

\* LUZERNE COUNTY  
CARBON COUNTY

DISTRICT	COUNTY	ROUTE	SECTION	SHEET	
5-0	*	0080	08B	13 OF 26	
WHITE HAVEN & EAST SIDE BOROUGHS AND KIDDER TOWNSHIP					
REVISION NUMBER	REVISIONS			DATE	BY



**LEGEND**

■ CHECK DAMS

**GUIDE RAIL LEGEND**

- ① TYPE 31-S GUIDE RAIL
- ② TYPE 31-S STRONG POST ANCHOR, TERMINAL AND TERMINAL SECTION, SINGLE
- ③ PERMANENT IMPACT ATTENUATING DEVICE, TYPE II, TEST LEVEL 3 (MASH)
- ④ GUIDE RAIL TO BRIDGE BARRIER TRANSITION
- ⑤ TERMINAL SECTION, BRIDGE CONNECTION

CONCRETE MOUNTABLE CURB, TYPE A

SELECT MATERIAL SURFACING, VARIABLE DEPTH (4" MIN)

STOP WORK STA 23+05.28 SR 8006 (RAMP C SPUR)

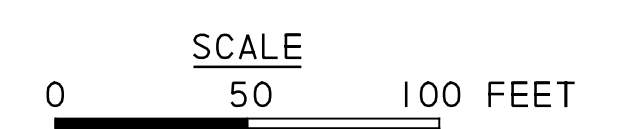
REQUIRED R/W FOR LIMITED ACCESS

AREA TAKEN FOR CHANNEL CHANGE

REQUIRED SLOPE EASEMENT

LEGAL R/W LINE FOR LIMITED ACCESS

COMMONWEALTH OF PENNSYLVANIA (STATE PARK)



CONTINUE ON SHEET 16 OF 26

UTILITY POLE TO BE REPLACED (BY OTHERS)

CONTINUE ON SHEET 14 OF 26

USER: L5AL00M PLOT DRIVER: PcmDOT\_PDF\_Memo.plt fsg PLOT DATE: 11-23-2021 12:43:57 PM  
 PATH: G:\pwork\eng\veg\11\140742833\FILE:008008B-RD-PL101.dgn MODEL: road plan 1

**HYDRAULIC DATA**

DRAINAGE AREA= 311 SQ MI  
 DESIGN FLOOD (50 YEAR):  
 Q = 13,502 CFS  
 VELOCITY= 7.2 FPS  
 WS EL= 1086.14  
 100 YEAR FLOOD:  
 Q = 15,373 CFS  
 VELOCITY= 7.4 FPS  
 WS EL= 1086.79

SELECTED BORROW  
 EXCAVATION ROCK,  
 CLASS R-4 AND  
 GEOTEXTILE,  
 CLASS 4, TYPE A  
 STA 2213+21.37 WB  
 END FULL DEPTH  
 RECONSTRUCTION  
 BEGIN BRIDGE  
 APPROACH SLAB

STOP WORK  
 STA 71+30.13  
 SR 8006  
 (RAMP B)

START WORK/  
 LIMIT OF WORK  
 STA 2218+90.04 WB  
 SEG 2729 OFFSET 0533  
 SR 0080 SEC 08B  
 EAST SIDE BOROUGH  
 CARBON COUNTY

STOP WORK/  
 LIMIT OF WORK  
 STA 2218+90.04 WB  
 SEG 2729 OFFSET 0533  
 SR 0080 SEC 08B  
 WHITE HAVEN BOROUGH  
 LUZERNE COUNTY

UTILITY POLE  
 TO BE REPLACED  
 (BY OTHERS)

**NOTES:**

1. SLOPE PROTECTION. PER RC-50M PLACE SELECTED BORROW EXCAVATION ROCK, CLASS AS NOTED AND GEOTEXTILE, CLASS 4, TYPE A. USE SELECTED BORROW EXCAVATION ROCK CLASS R-3 ADJACENT TO ROADWAY TO A DISTANCE OF 2' BEHIND GUIDERAIL POST. REMAINING ROCK TO BE SELECTED BORROW EXCAVATION ROCK. CLASS R-4.

STA 2221+51.37 WB  
 END BRIDGE APPROACH SLAB  
 BEGIN FULL DEPTH RECONSTRUCTION

LEGAL R/W LINE  
 FOR LIMITED ACCESS

UNKNOWN  
 OWNER

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
5-0	*	0080	08B	14 OF 26

REVISION NUMBER	REVISIONS	DATE	BY

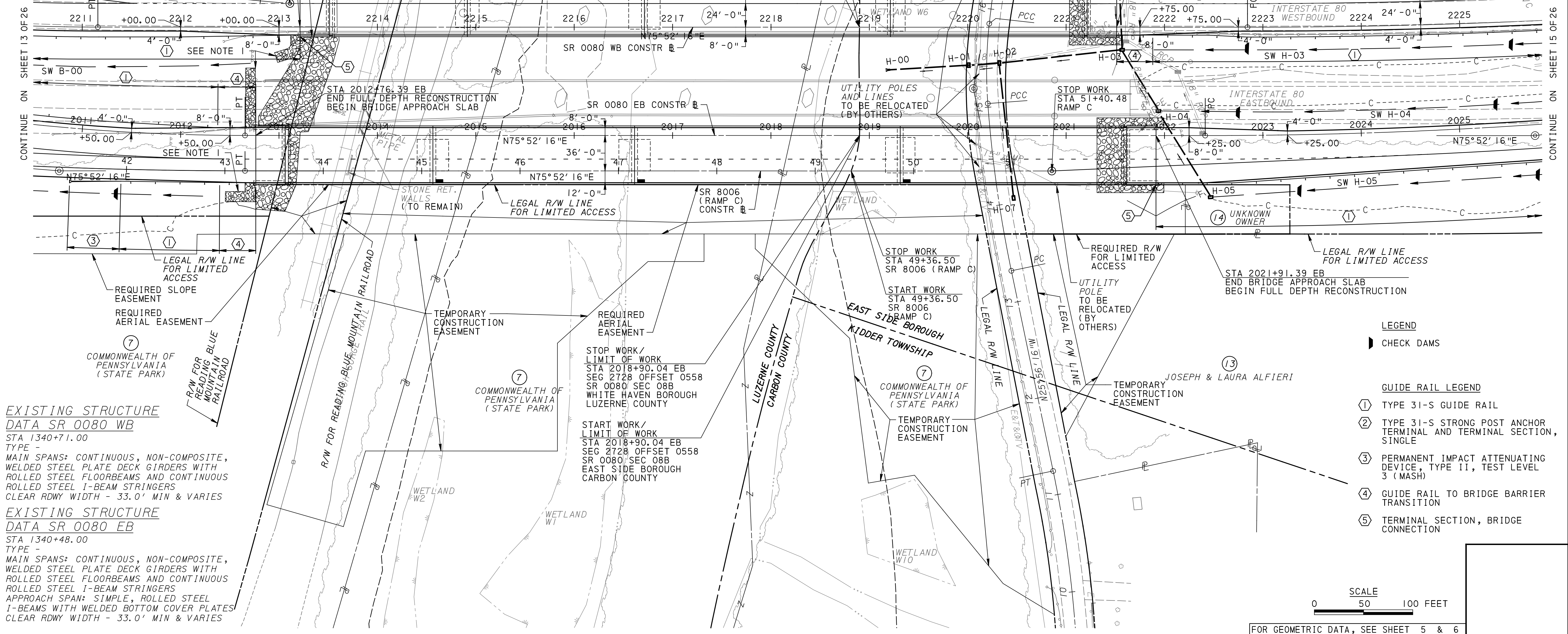
USER: RUBUCKLE | PLOT DRIVER: PLOTDRIVER | PLOT DATE: 11-23-2021 | 4:23:28 PM  
 PATH: G:\pwork\k\ing\veg\01\1\0742833 | FILE: 008008B-RD-PLAN1.dgn  
 MODEL: road plan 2

**EXISTING STRUCTURE DATA SR 0080 WB**

STA 1340+71.00  
 TYPE -  
 MAIN SPANS: CONTINUOUS, NON-COMPOSITE, WELDED STEEL PLATE DECK GIRDERS WITH ROLLED STEEL FLOORBEAMS AND CONTINUOUS ROLLED STEEL I-BEAM STRINGERS  
 CLEAR RDWY WIDTH - 33.0' MIN & VARIES

**EXISTING STRUCTURE DATA SR 0080 EB**

STA 1340+48.00  
 TYPE -  
 MAIN SPANS: CONTINUOUS, NON-COMPOSITE, WELDED STEEL PLATE DECK GIRDERS WITH ROLLED STEEL FLOORBEAMS AND CONTINUOUS ROLLED STEEL I-BEAM STRINGERS  
 APPROACH SPAN: SIMPLE, ROLLED STEEL I-BEAMS WITH WELDED BOTTOM COVER PLATES  
 CLEAR RDWY WIDTH - 33.0' MIN & VARIES

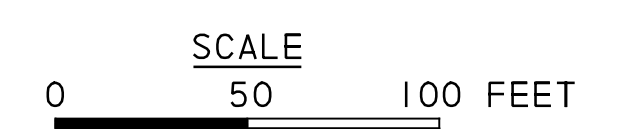


**LEGEND**

▬ CHECK DAMS

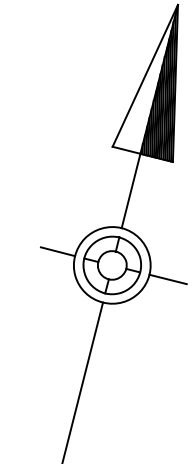
**GUIDE RAIL LEGEND**

- ① TYPE 31-S GUIDE RAIL
- ② TYPE 31-S STRONG POST ANCHOR TERMINAL AND TERMINAL SECTION, SINGLE
- ③ PERMANENT IMPACT ATTENUATING DEVICE, TYPE II, TEST LEVEL 3 (MASH)
- ④ GUIDE RAIL TO BRIDGE BARRIER TRANSITION
- ⑤ TERMINAL SECTION, BRIDGE CONNECTION



FOR GEOMETRIC DATA, SEE SHEET 5 & 6  
 SURVEY BOOK NO. 25003

LUZERNE COUNTY  
CARBON COUNTY



DISTRICT	COUNTY	ROUTE	SECTION	SHEET	
5-0	*	0080	08B	15 OF 26	
WHITE HAVEN & EAST SIDE BOROUGHS AND KIDDER TOWNSHIP					
REVISION NUMBER	REVISIONS			DATE	BY

10  
GIUSEPPE PECORARI

LIMIT OF WORK  
STA 2249+70.00 WB  
SEG 2731 OFFSET 2253  
SR 0080 SEC 08B  
EAST SIDE BOROUGH  
CARBON COUNTY

STOP WORK  
STA 2248+70.00 WB  
SEG 2731 OFFSET 2153  
SR 0080 SEC 08B

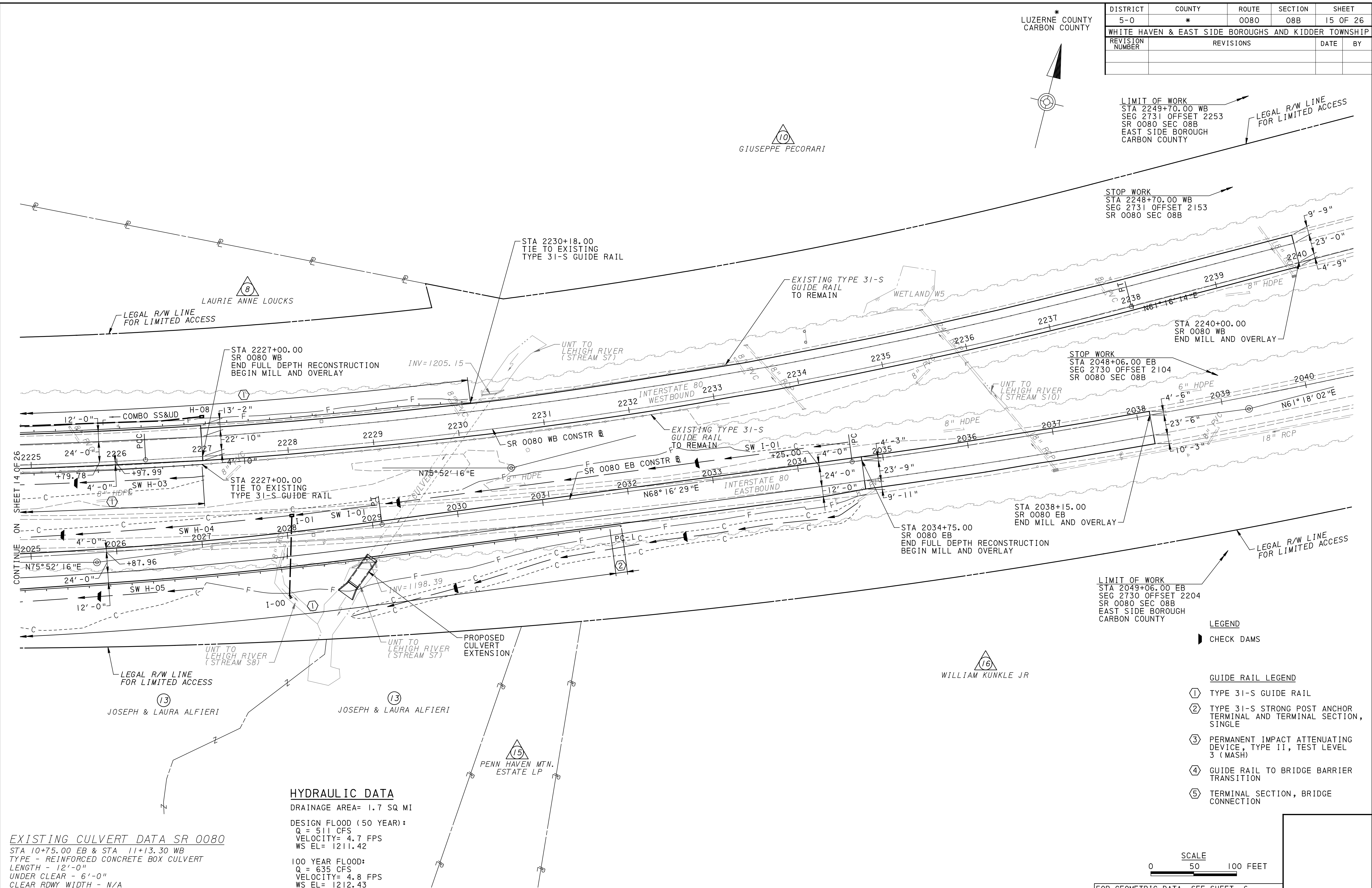
STOP WORK  
STA 2048+06.00 EB  
SEG 2730 OFFSET 2104  
SR 0080 SEC 08B

LIMIT OF WORK  
STA 2049+06.00 EB  
SEG 2730 OFFSET 2204  
SR 0080 SEC 08B  
EAST SIDE BOROUGH  
CARBON COUNTY

- LEGEND**
- ▬ CHECK DAMS
- GUIDE RAIL LEGEND**
- ① TYPE 31-S GUIDE RAIL
  - ② TYPE 31-S STRONG POST ANCHOR, TERMINAL AND TERMINAL SECTION, SINGLE
  - ③ PERMANENT IMPACT ATTENUATING DEVICE, TYPE II, TEST LEVEL 3 (MASH)
  - ④ GUIDE RAIL TO BRIDGE BARRIER TRANSITION
  - ⑤ TERMINAL SECTION, BRIDGE CONNECTION

SCALE  
0 50 100 FEET

FOR GEOMETRIC DATA, SEE SHEET 6



**HYDRAULIC DATA**  
DRAINAGE AREA= 1.7 SQ MI  
DESIGN FLOOD (50 YEAR):  
Q = 511 CFS  
VELOCITY= 4.7 FPS  
WS EL= 1211.42  
100 YEAR FLOOD:  
Q = 635 CFS  
VELOCITY= 4.8 FPS  
WS EL= 1212.43

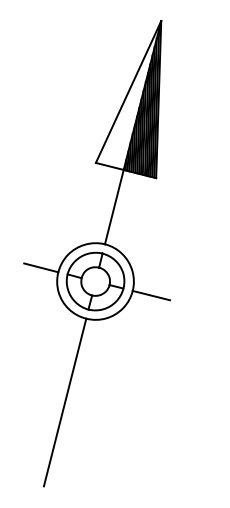
**EXISTING CULVERT DATA SR 0080**  
STA 10+75.00 EB & STA 11+13.30 WB  
TYPE - REINFORCED CONCRETE BOX CULVERT  
LENGTH - 12'-0"  
UNDER CLEAR - 6'-0"  
CLEAR RDWY WIDTH - N/A

DES: RPB DWG: DMY CKD: JEB

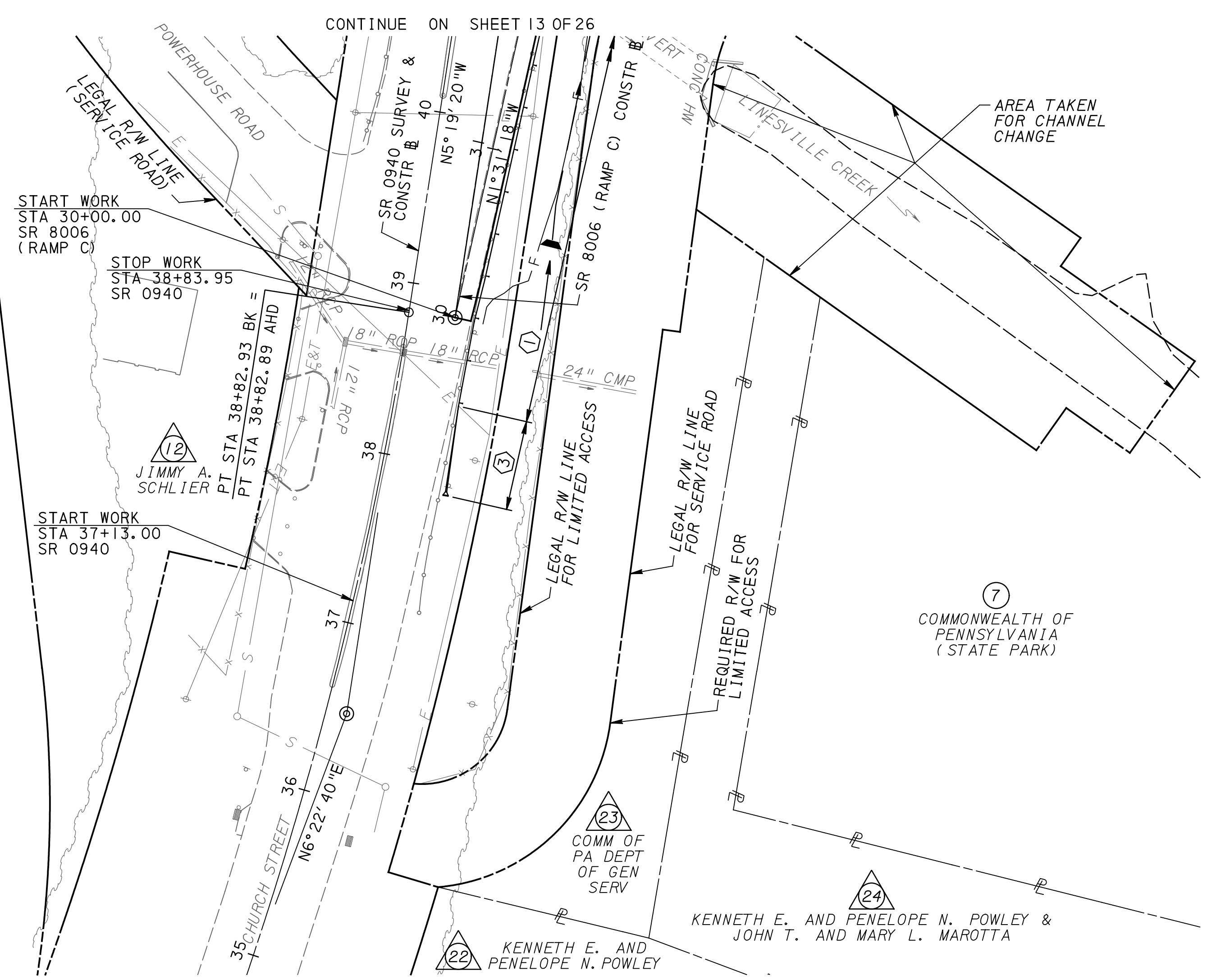
FOR SR 0080 EB PROFILE, SEE SHEET 20 OF 26 FOR SR 0080 WB PROFILE, SEE SHEET 23 OF 26 SURVEY BOOK NO. 25003

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 PATH: G:\pwworking\louis\10742833\FILE:008008B-RD-PLN01.dgn MODEL: road plan 3

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
5-0	*	0080	08B	16 OF 26
WHITE HAVEN & EAST SIDE BOROUGHS AND KIDDER TOWNSHIP				
REVISION NUMBER	REVISIONS	DATE	BY	

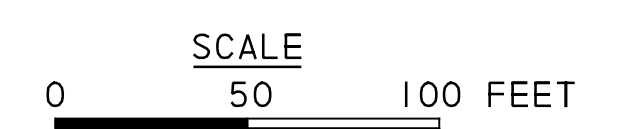


USER: LSALCUM PLOT DRIVER: PdmDOT\_PDF\_Memo.plt:fg PLOT DATE: 11-23-2021 12:44:00 PM  
 PATH: s:\pwork\k\ing\veg\101\10742833\ FILE: 008008B-RD-PLAN1.dgn MODEL: road plan 4



**LEGEND**

- ▮ CHECK DAMS
  
- GUIDE RAIL LEGEND**
- ① TYPE 31-S GUIDE RAIL
- ② TYPE 31-S STRONG POST ANCHOR, TERMINAL AND TERMINAL SECTION, SINGLE
- ③ PERMANENT IMPACT ATTENUATING DEVICE, TYPE II, TEST LEVEL 3 (MASH)
- ④ GUIDE RAIL TO BRIDGE BARRIER TRANSITION
- ⑤ TERMINAL SECTION, BRIDGE CONNECTION

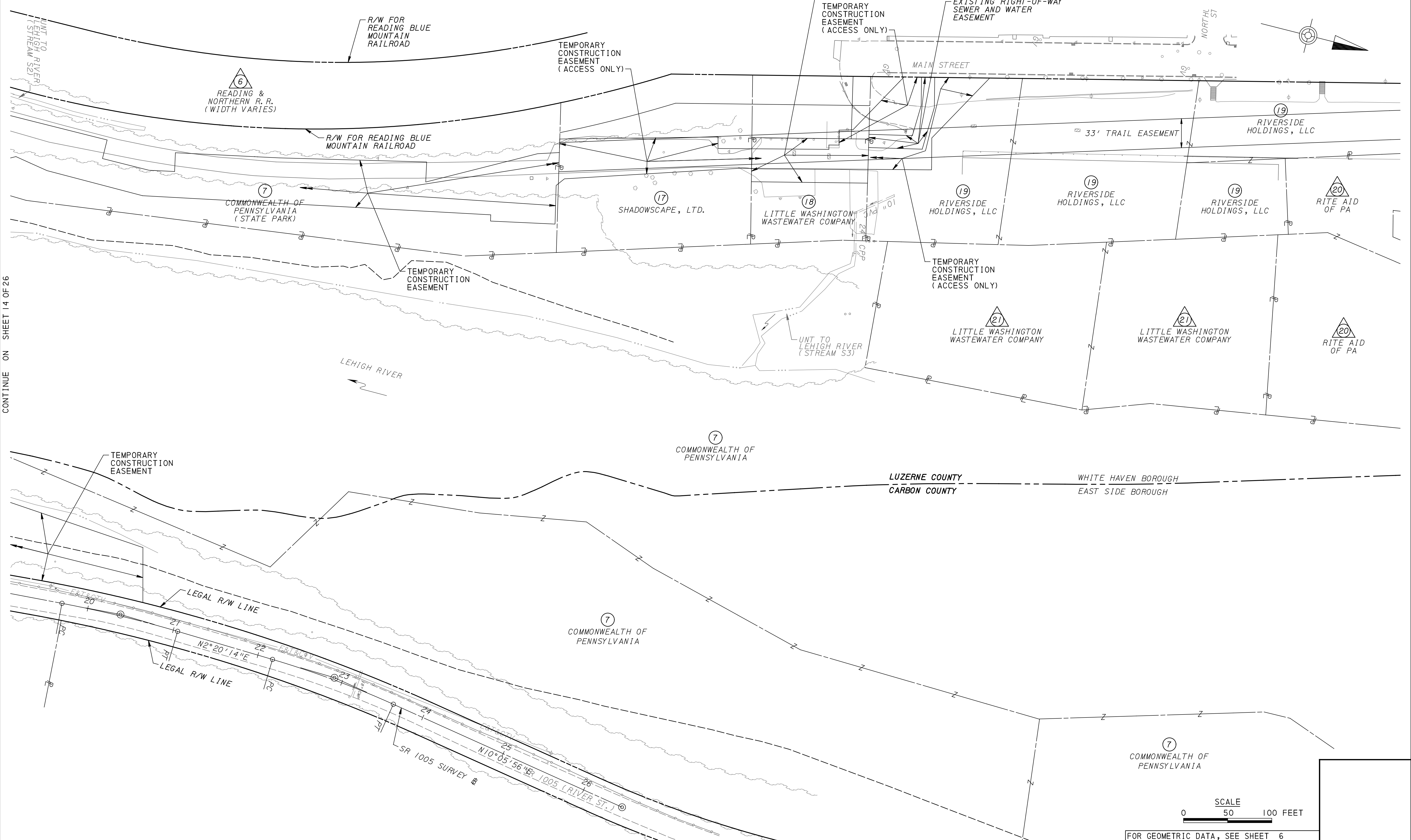


FOR GEOMETRIC DATA, SEE SHEET 5  
 SURVEY BOOK NO. 25003



LUZERNE COUNTY  
CARBON COUNTY

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
5-0	*	0080	08B	17 OF 26
WHITE HAVEN & EAST SIDE BOROUGHS AND KIDDER TOWNSHIP				
REVISION NUMBER	REVISIONS		DATE	BY



CONTINUE ON SHEET 14 OF 26

USER: LSALOUIM PLOT DRIVER: PennDOT\_PDF\_Memo.plt.ctb PLOT DATE: 11-23-2021 12:44:01 PM  
 PATH: c:\pwworking\ins\es+01\1\40742833\FILE: 008008B-RD-PL101.dgn  
 MODEL: road plan 5

SCALE  
0 50 100 FEET

FOR GEOMETRIC DATA, SEE SHEET 6  
SURVEY BOOK NO. 25003

DES: RPB DWG: DMY CKD: JEB

LUZERNE COUNTY  
CARBON COUNTY

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
5-0	*	0080	08B	18 OF 26
WHITE HAVEN & EAST SIDE BOROUGHS AND KIDDER TOWNSHIP				
REVISION NUMBER	REVISIONS	DATE	BY	

# SR 0080 EASTBOUND

← LIMIT OF WORK  
STA 1984+55.00 EB  
SEG 2720 OFFSET 1097  
SR 0080 SEC 08B  
WHITE HAVEN BOROUGH  
LUZERNE COUNTY

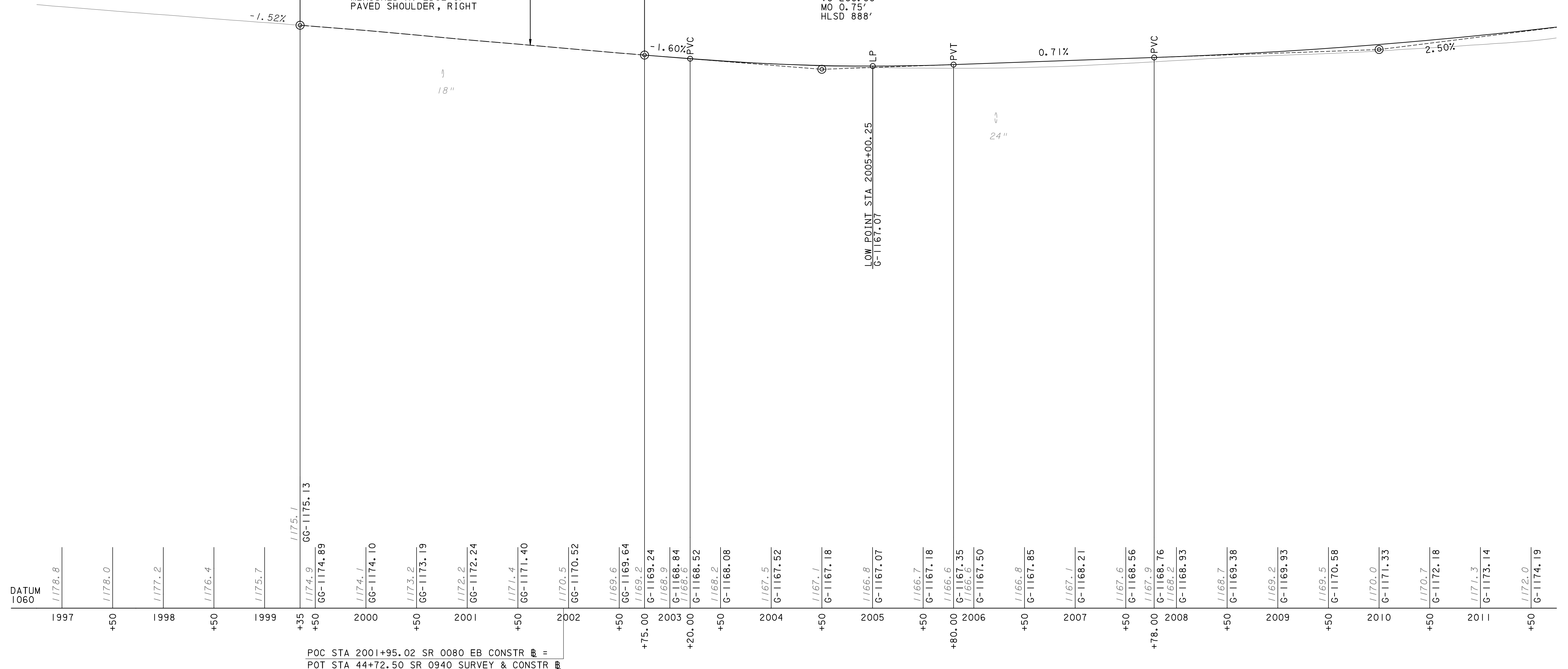
← START WORK  
STA 1985+05.00 EB  
SEG 2720 OFFSET 1147  
SR 0080 SEC 08B

SR 0940 (CHURCH STREET)

GRAPHIC GRADE  
MINIMUM VERTICAL CLEARANCE  
16.61' (EXISTING & PROPOSED)  
16.50' REQUIRED  
MEASURED AT EDGE OF  
PAVED SHOULDER, RIGHT

PVI STA 2004+50.00  
ELEV 1166.43  
VC 260.00'  
MO 0.75'  
HLSD 888'

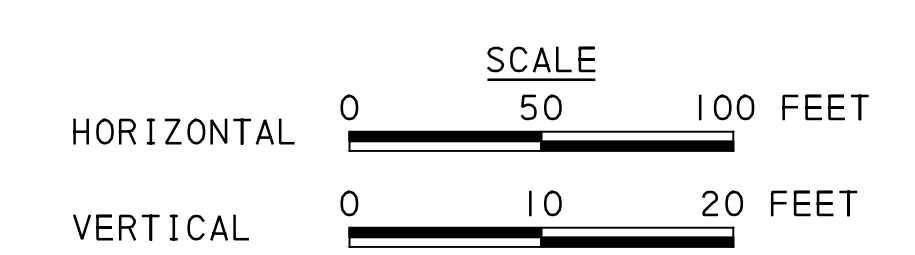
PVI STA 2010+00.00  
ELEV 1170.34  
VC 444.00'  
MO 0.99'  
HLSD 14368'



USER: LSAJLDMU PLOT DRIVER: PcmdDOT\_PDF\_Memo.pltcfgr PLOT DATE: 11-23-2021 12:44:04 PM  
PATH: c:\pwork\lmg\res\10\100742833\ FILE: 00808B-RD-PR01.dgn MODEL: SHEET 1

DATUM  
1060

POC STA 2001+95.02 SR 0080 EB CONSTR # =  
POT STA 44+72.50 SR 0940 SURVEY & CONSTR # =



DES: RPB DWG: DMY CKD: JEB

FOR PLAN, SEE SHEET 13 OF 26

SURVEY BOOK NO. 25003

CONTINUE ON SHEET 19 OF 26

SR 0080 EASTBOUND

\* LUZERNE COUNTY  
CARBON COUNTY

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
5-0	*	0080	08B	19 OF 26
WHITE HAVEN & EAST SIDE BOROUGHS AND KIDDER TOWNSHIP				
REVISION NUMBER	REVISIONS	DATE	BY	

STOP WORK /  
LIMIT OF WORK  
STA 2018+90.04 EB  
SEG 2728 OFFSET 0558  
SR 0080 SEC 08B  
WHITE HAVEN BOROUG  
LUZERNE COUNTY

START WORK /  
LIMIT OF WORK  
STA 2018+90.04 EB  
SEG 2728 OFFSET 0558  
SR 0080 SEC 08B  
EAST SIDE BOROUG  
CARBON COUNTY

PVI STA 2010+00.00  
ELEV 1170.34  
VC 444.00'  
MO 0.99'  
HLSD 14368'

2.50%

2.50%

29'-1" VERT CLR PROVIDED (23'-0" REQUIRED)

46'-11" VERT CLR

READING BLUE MT. RR

LEHIGH GORGE TRAIL

SR 1005 (RIVER ROAD)

CONTINUE ON SHEET 18 OF 26

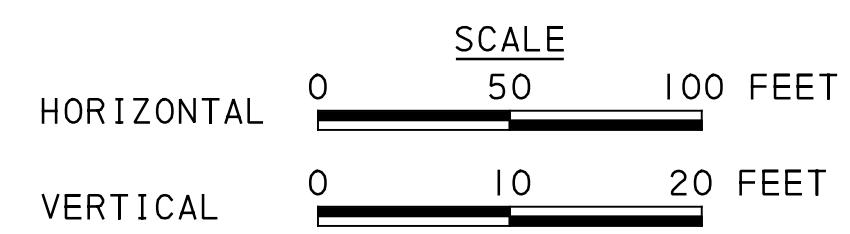
CONTINUE ON SHEET 20 OF 26

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DATUM 1060	1171.3 G-1173.14	1172.0 G-1174.19	1173.3 G-1175.35	1173.7 G-1175.89	1174.2 G-1176.59	1167.3 G-1177.84	1139.8 G-1179.09	1122.2 G-1180.34	1118.9 G-1181.59	1096.7 G-1182.84	1083.7 G-1184.09	1083.8 G-1185.34	1081.0 G-1186.59	1076.8 G-1187.84	1075.5 G-1189.10	1075.5 G-1190.35	1079.9 G-1191.60	1087.3 G-1192.85	1105.9 G-1194.10	1132.4 G-1195.35	1147.4 G-1196.60	1168.2 G-1197.85	1179.4 G-1199.10	1195.2 G-1200.35	1199.2 G-1201.60	1206.3 G-1202.85	1202.6 G-1204.10	1207.6 G-1205.35	1208.8 G-1206.61	1208.3 G-1207.86	1208.5 G-1209.11
	2011	+50	2012	+50	2013	+50	2014	+50	2015	+50	2016	+50	2017	+50	2018	+50	2019	+50	2020	+50	2021	+50	2022	+50	2023	+50	2024	+50	2025	+50	

PT STA 2012+65.48 SR 0080 EB CONSTR # =  
PT STA 43+20.48, 36.00' RT RAMP C #

POT STA 2020+23.24 SR 0080 EB CONSTR # =  
POC STA 14+71.15 SR 1005 SURVEY #



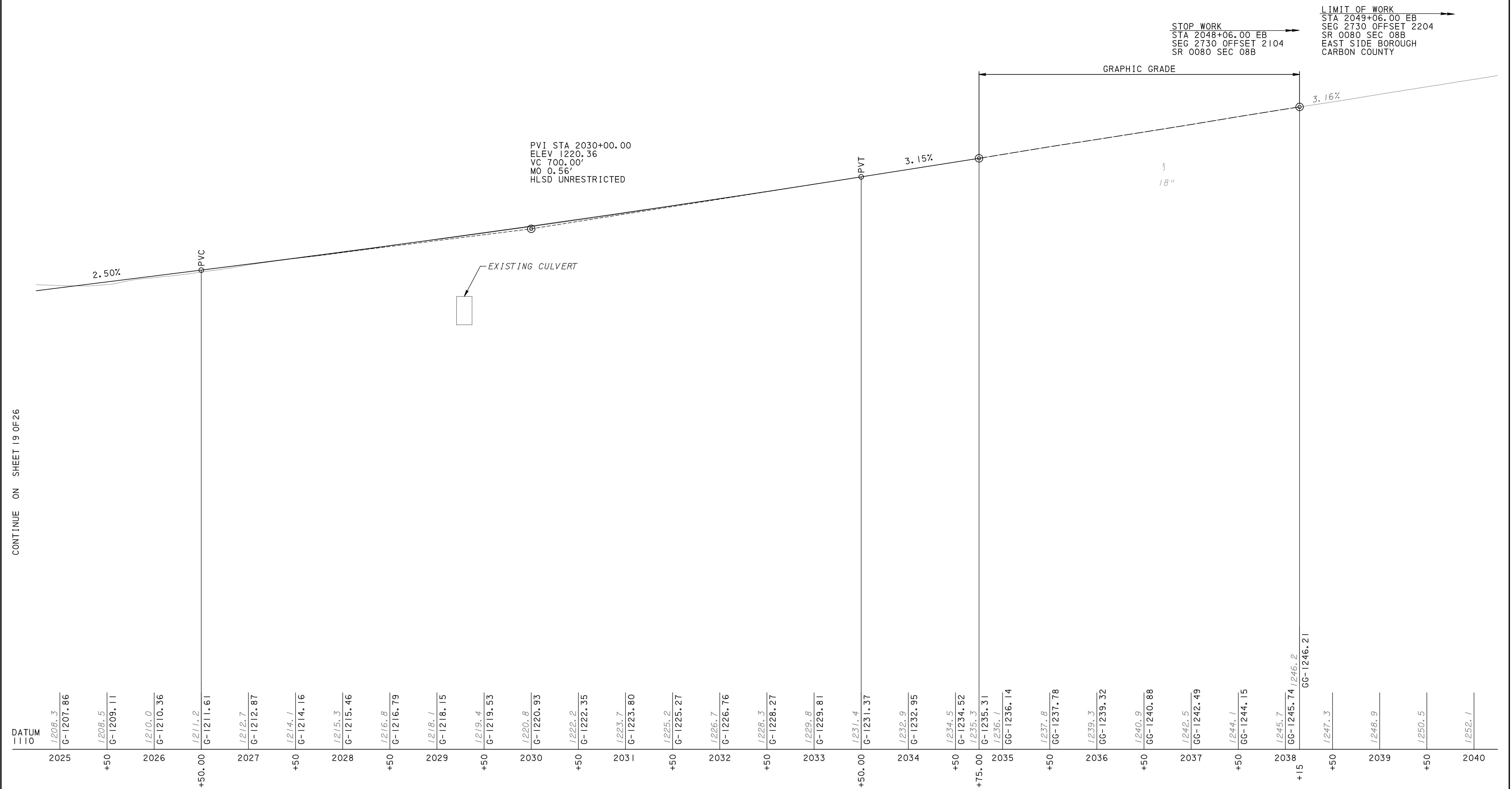
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CONTINUE ON SHEET 19 OF 26

SR 0080 EASTBOUND

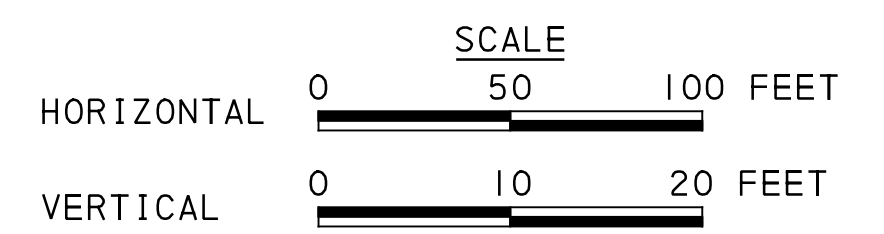
LUZERNE COUNTY  
 CARBON COUNTY

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
5-0	*	0080	08B	20 OF 26
WHITE HAVEN & EAST SIDE BOROUGHS AND KIDDER TOWNSHIP				
REVISION NUMBER	REVISIONS	DATE	BY	



STOP WORK  
 STA 2048+06.00 EB  
 SEG 2730 OFFSET 2104  
 SR 0080 SEC 08B

LIMIT OF WORK  
 STA 2049+06.00 EB  
 SEG 2730 OFFSET 2204  
 SR 0080 SEC 08B  
 EAST SIDE BOROUGH  
 CARBON COUNTY



# SR 0080 WESTBOUND

LUZERNE COUNTY  
CARBON COUNTY

DISTRICT	COUNTY	ROUTE	SECTION	SHEET	
5-0	*	0080	08B	21 OF 26	
WHITE HAVEN & EAST SIDE BOROUGHS AND KIDDER TOWNSHIP					
REVISION NUMBER	REVISIONS			DATE	BY

LIMIT OF WORK  
STA 2197+00.00 WB  
SEG 2721 OFFSET 2330  
SR 0080 SEC 08B  
WHITE HAVEN BOROUGH  
LUZERNE COUNTY

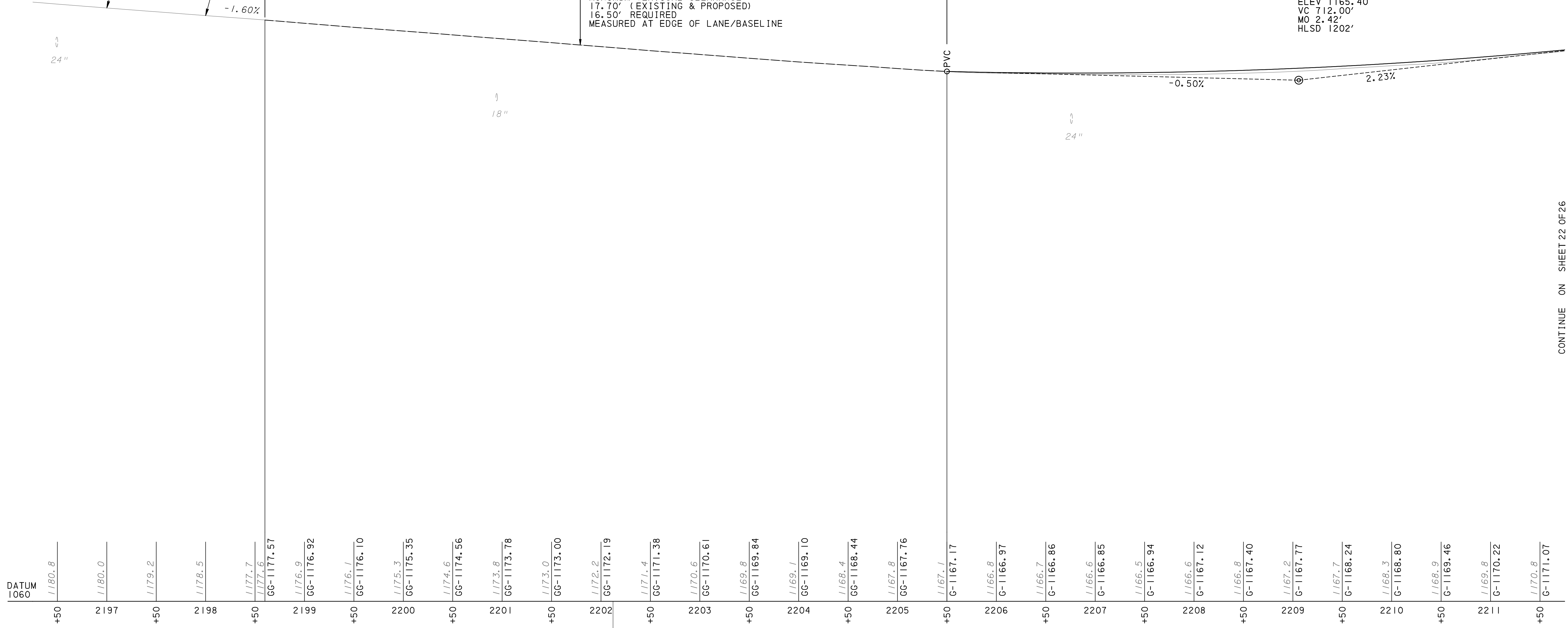
START WORK  
STA 2198+00.00 WB  
SEG 2721 OFFSET 2430  
SR 0080 SEC 08B

SR 0940 (CHURCH STREET)

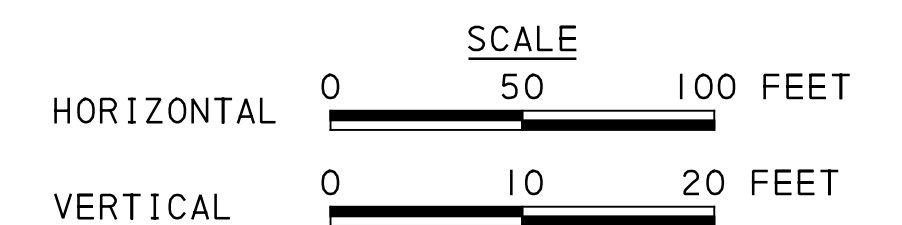
GRAPHIC GRADE

MINIMUM VERTICAL CLEARANCE  
17.70' (EXISTING & PROPOSED)  
16.50' REQUIRED  
MEASURED AT EDGE OF LANE/BASELINE

PVI STA 2209+06.00  
ELEV 1165.40  
VC 712.00'  
MO 2.42'  
HLSD 1202'



POC STA 2202+12.16 SR 0080 WB CONSTR # =  
POT STA 45+35.31 SR 0940 SURVEY & CONSTR #



USER: LSAJDMU PLOT DRIVER: Pcmdot.plt PLOT DATE: 11-23-2021 12:44:09 PM  
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MODEL SHEET 1

DES: RPB DWG: DMY CKD: JEB

FOR PLAN, SEE SHEET 13 OF 26

SURVEY BOOK NO. 25003

CONTINUE ON SHEET 22 OF 26

SR 0080 WESTBOUND

\* LUZERNE COUNTY  
CARBON COUNTY

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
5-0	*	0080	08B	22 OF 26
WHITE HAVEN & EAST SIDE BOROUGHS AND KIDDER TOWNSHIP				
REVISION NUMBER	REVISIONS			DATE

STOP WORK/  
LIMIT OF WORK  
STA 2218+90.04 WB  
SEG 2729 OFFSET 0533  
SR 0080 SEC 08B  
WHITE HAVEN BOROUGH  
LUZERNE COUNTY

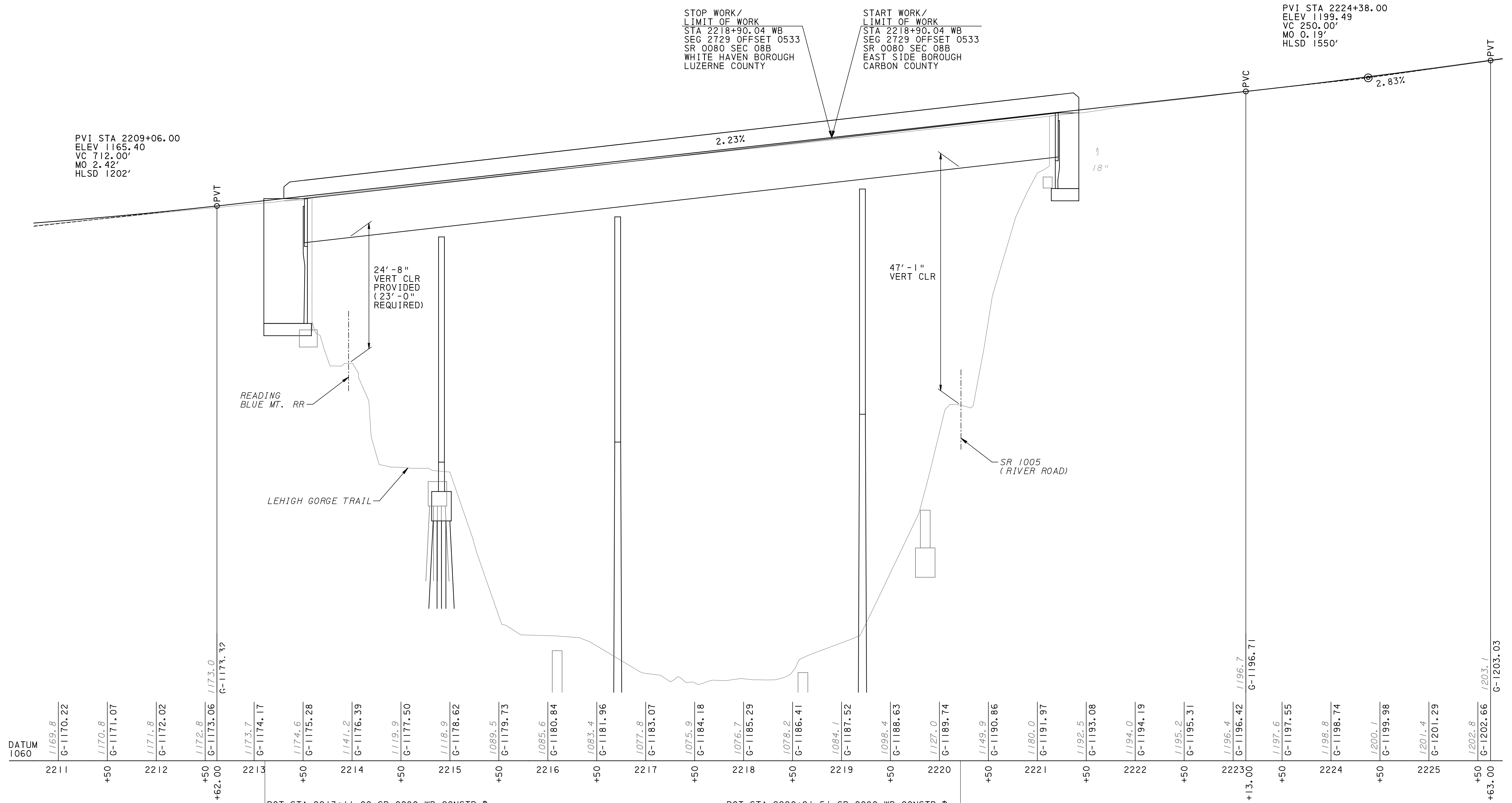
START WORK/  
LIMIT OF WORK  
STA 2218+90.04 WB  
SEG 2729 OFFSET 0533  
SR 0080 SEC 08B  
EAST SIDE BOROUGH  
CARBON COUNTY

PVI STA 2224+38.00  
ELEV 1195.49  
VC 250.00'  
MO 0.19'  
HLSD 1550'

PVI STA 2209+06.00  
ELEV 1165.40  
VC 712.00'  
MO 2.42'  
HLSD 1202'

CONTINUE ON SHEET 21 OF 26

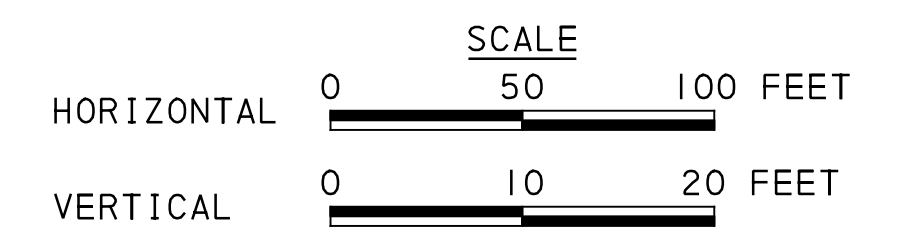
CONTINUE ON SHEET 23 OF 26



DATUM 1060	1169.8	1170.8	1171.8	1172.8	1173.7	1174.6	1175.6	1176.6	1177.6	1178.6	1089.5	1085.6	1083.4	1077.8	1075.9	1076.7	1078.2	1084.1	1098.4	1127.0	1149.9	1180.0	1192.5	1194.0	1195.2	1196.4	1197.6	1198.8	1200.1	1201.4	1202.8
	G-11170.22	G-11171.07	G-11172.02	G-11173.06	G-11174.17	G-11175.28	G-11176.39	G-11177.50	G-11178.62	G-11179.73	G-11180.84	G-11181.96	G-11183.07	G-11184.18	G-11185.29	G-11186.41	G-11187.52	G-11188.63	G-11189.74	G-11190.86	G-11191.97	G-11193.08	G-11194.19	G-11195.31	G-11196.42	G-11197.55	G-11198.74	G-11199.98	G-1201.29	G-1202.66	
				G-11173.32																					G-11196.71					G-1203.1	

POT STA 2213+11.00 SR 0080 WB CONSTR # =  
POT STA 71+30.13 RAMP B CONSTR #, 24.00' LT

POT STA 2220+21.51 SR 0080 WB CONSTR # =  
POC STA 15+81.41 SR 1005 SURVEY #



SURVEY BOOK NO. 25003

DES: RPB DWG: DMY CKD: JEB

FOR PLAN, SEE SHEET 14 OF 26

USER: LSAJDMU PLOT DRIVER: PcmDOT\_PDF\_Memo.plt PLOT DATE: 11-23-2021 12:44:09 PM  
PATH: c:\pwork\img\res\10\1\0742833\ FILE: 00808B-RD-PR02.dgn MODEL: SHEET 2

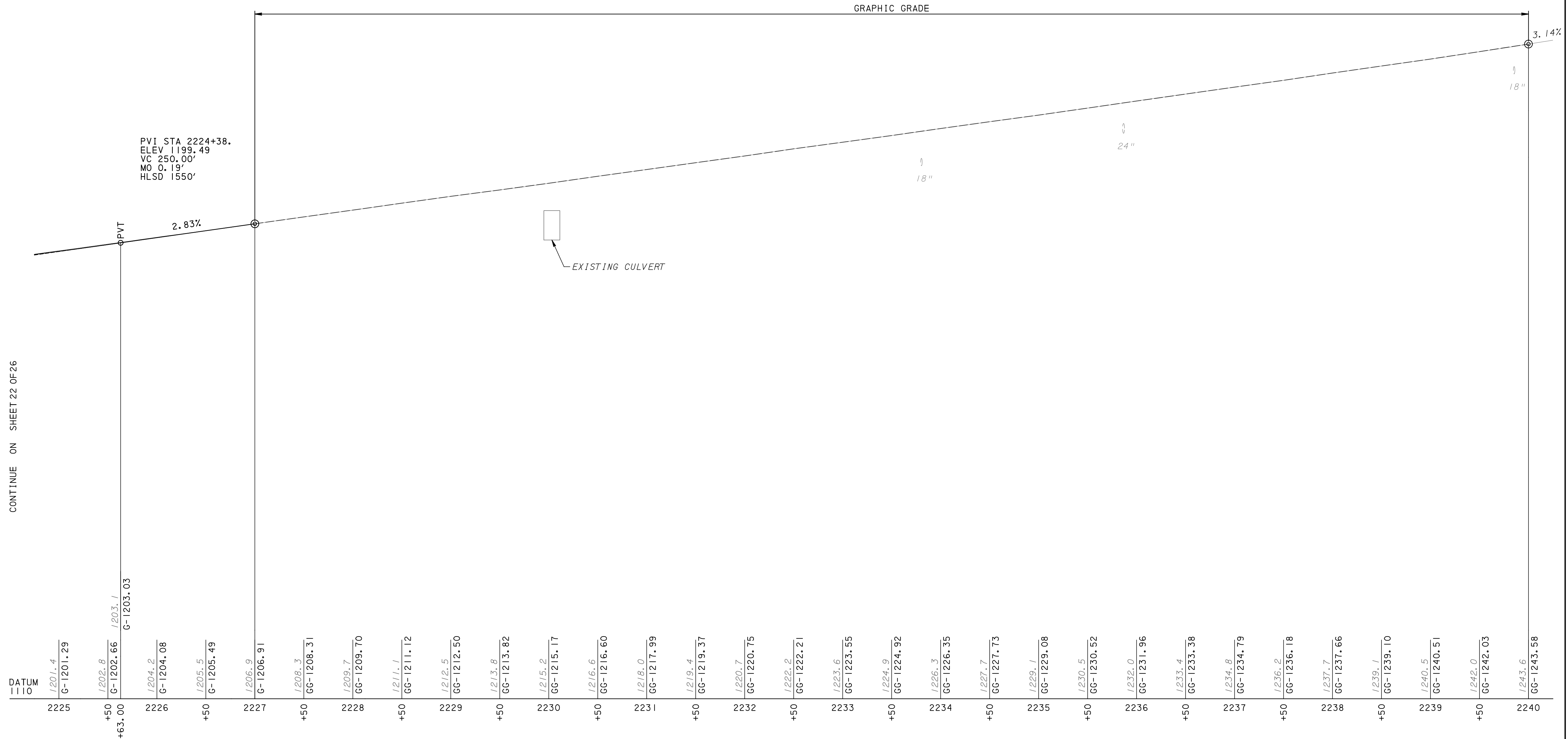
LUZERNE COUNTY  
CARBON COUNTY

SR 0080 WESTBOUND

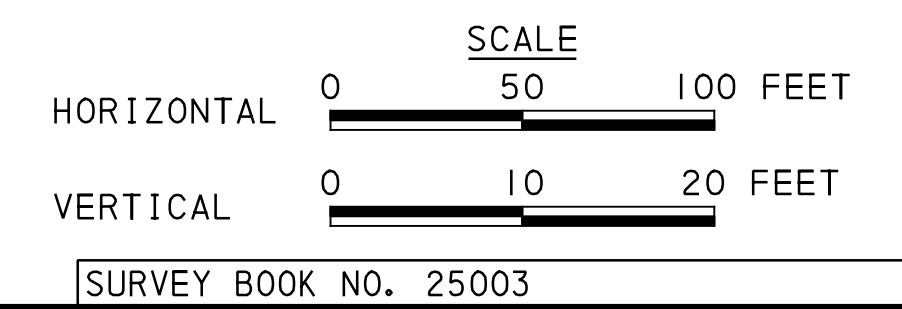
DISTRICT	COUNTY	ROUTE	SECTION	SHEET
5-0	*	0080	08B	23 OF 26
WHITE HAVEN & EAST SIDE BOROUGHS AND KIDDER TOWNSHIP				
REVISION NUMBER	REVISIONS			DATE
				BY

STOP WORK  
STA 2248+70.00 WB  
SEG 2731 OFFSET 2153  
SR 0080 SEC 08B

LIMIT OF WORK  
STA 2249+70.00 WB  
SEG 2731 OFFSET 2253  
SR 0080 SEC 08B  
EAST SIDE BOROUGHS  
CARBON COUNTY



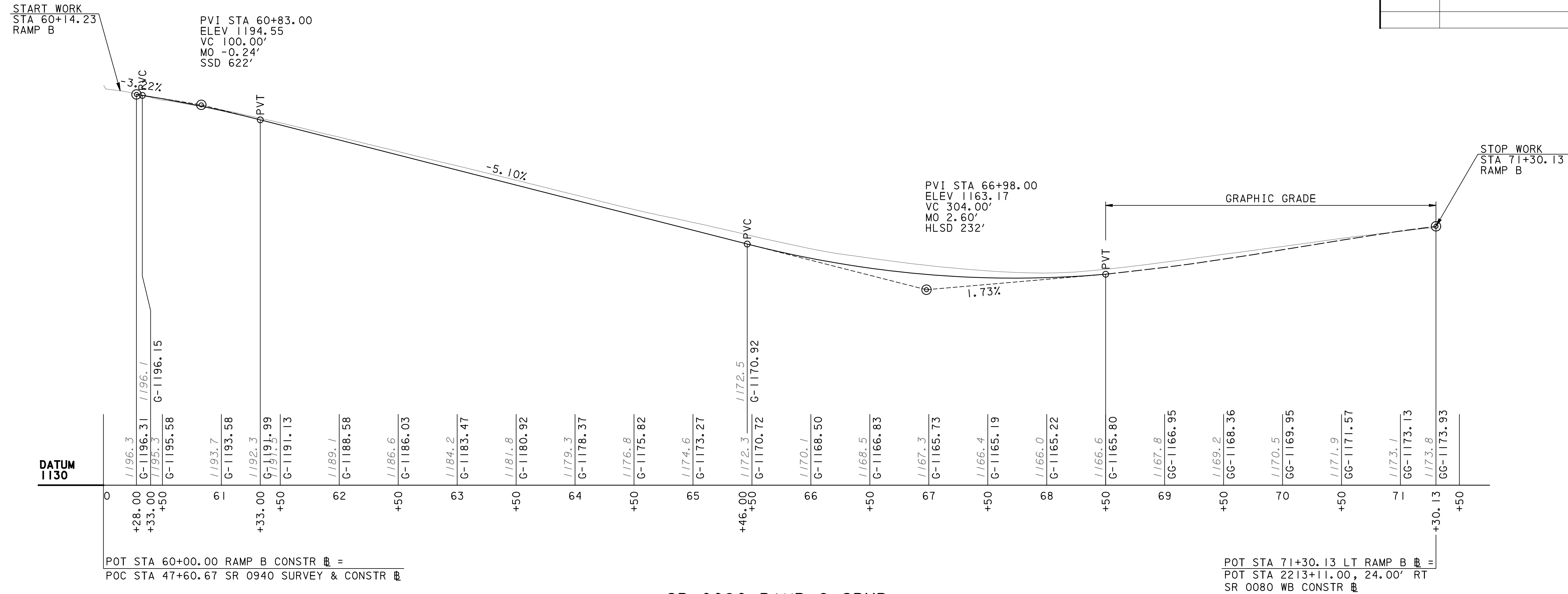
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PATH: c:\pwork\king\2021\11\2021\0742833\  
FILE: 008008B-RD-PR02.dgn  
PLOT DATE: 11-23-2021 12:44:10 PM  
MODEL: SHEET 3



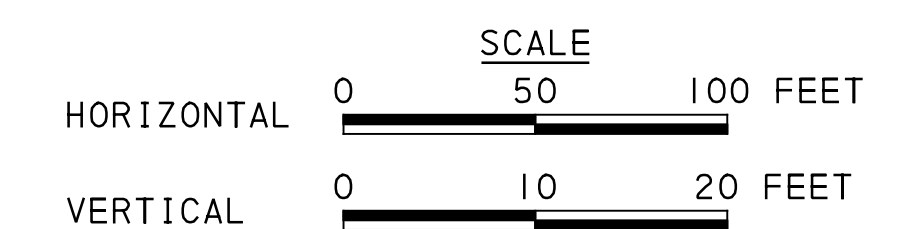
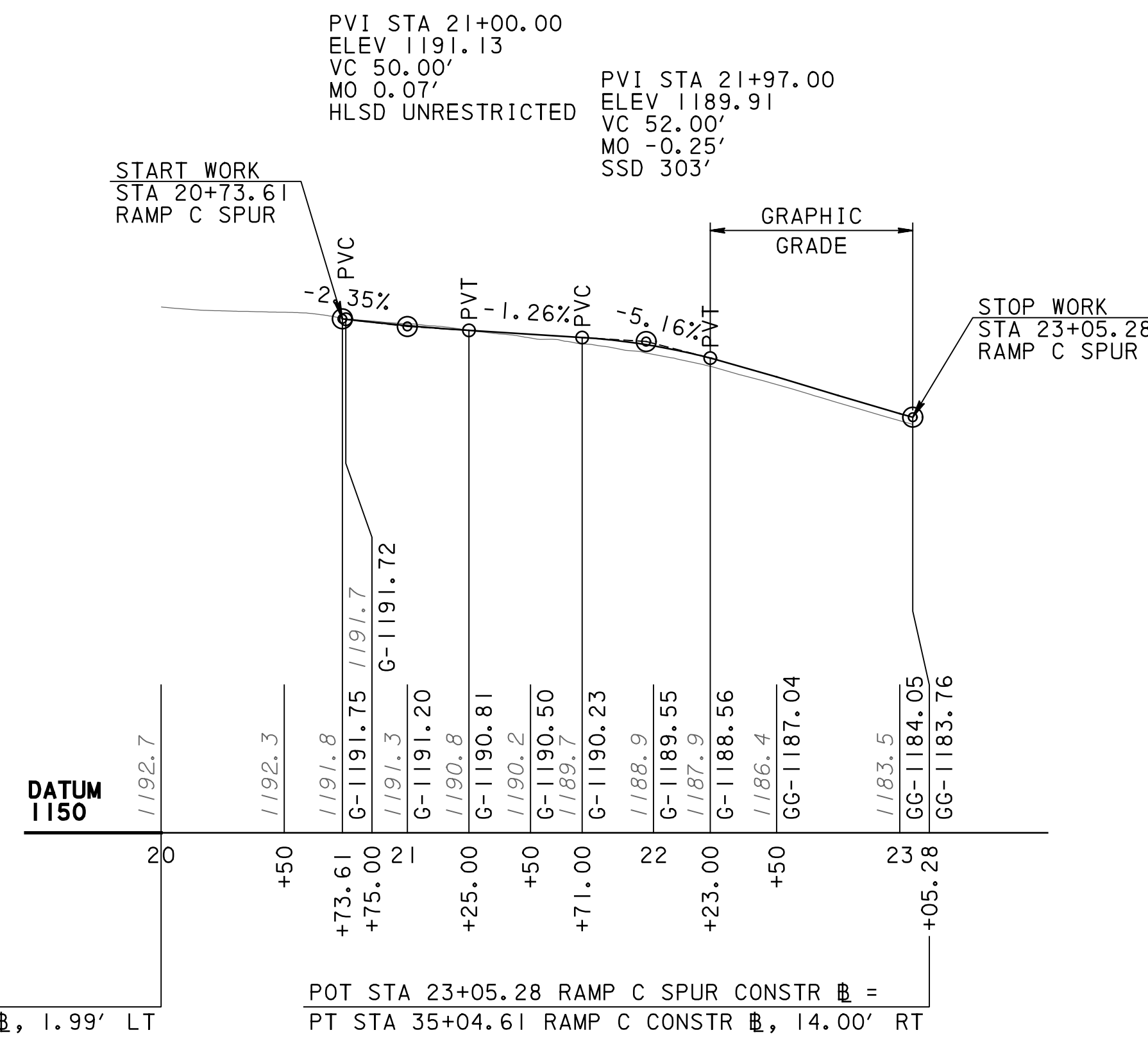
LUZERNE COUNTY  
CARBON COUNTY

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
5-0	*	0080	08B	24 OF 26
WHITE HAVEN & EAST SIDE BOROUGHS AND KIDDER TOWNSHIP				
REVISION NUMBER	REVISIONS	DATE	BY	

### SR 0080 RAMP B



### SR 0080 RAMP C SPUR



USER: LSAJOURN PLOT DRIVER: PcmDOT\_PDF\_Memo.plt PLOT DATE: 11-23-2021 12:44:14 PM  
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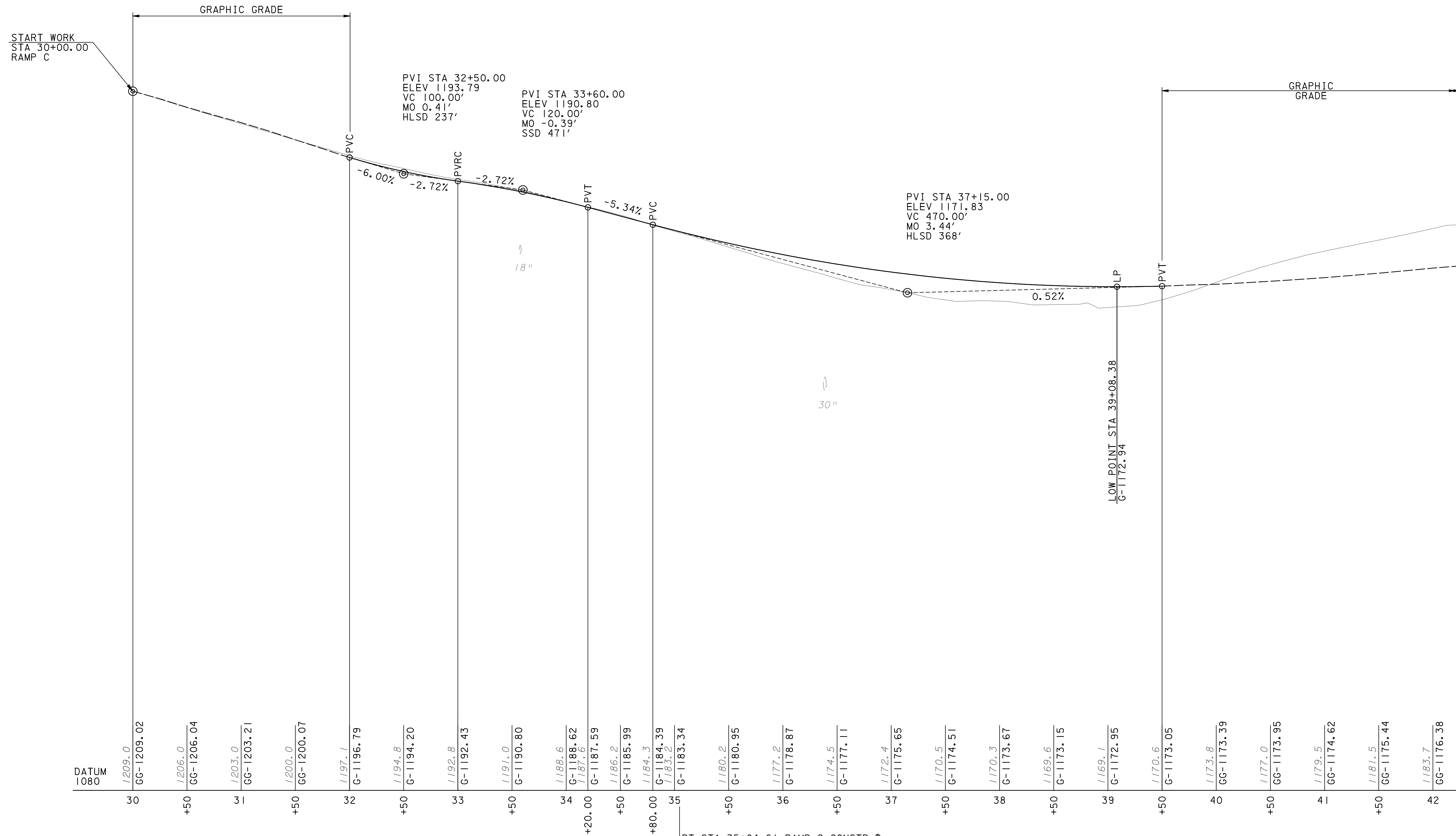
MODEL SHEET 1



SR 0080 RAMP C

LUZERNE COUNTY  
CARBON COUNTY

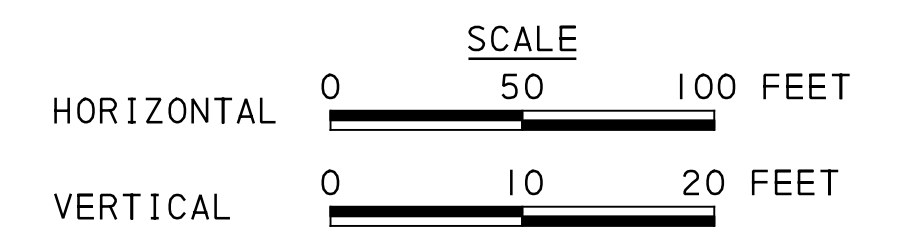
DISTRICT	COUNTY	ROUTE	SECTION	SHEET
5-0	*	0080	08B	25 OF 26
WHITE HAVEN & EAST SIDE BOROUGHS AND KIDDER TOWNSHIP				
REVISION NUMBER	REVISIONS	DATE	BY	



CONTINUE ON SHEET 26 OF 26

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FILE: 00808B-RD-PR04.dgn

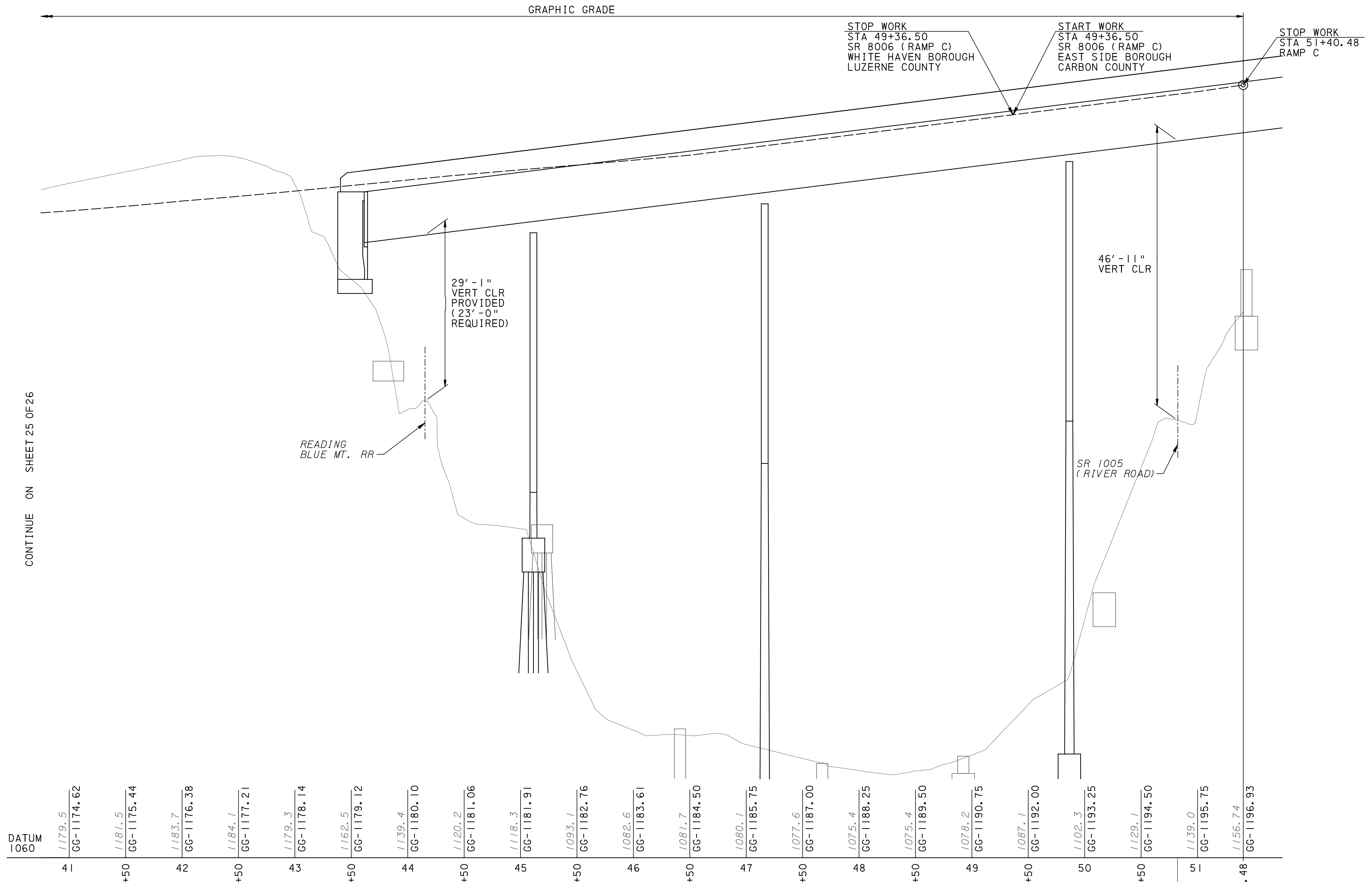
MODEL: tone sheet



SR 0080 RAMP C

\* LUZERNE COUNTY  
CARBON COUNTY

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
5-0	*	0080	08B	26 OF 26
WHITE HAVEN & EAST SIDE BOROUGHS AND KIDDER TOWNSHIP				
REVISION NUMBER	REVISIONS	DATE	BY	

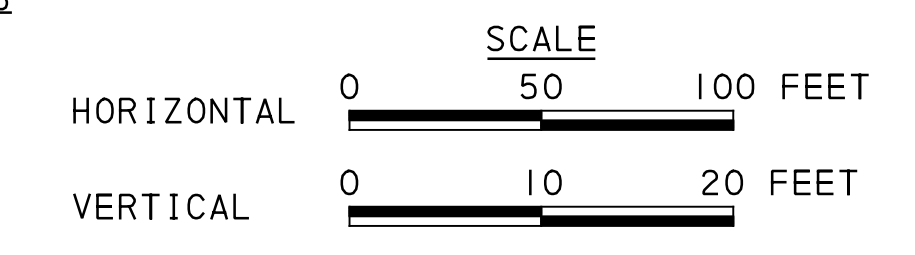


STATION	GG-1174.62	GG-1175.44	GG-1176.38	GG-1177.21	GG-1178.14	GG-1179.12	GG-1180.10	GG-1181.06	GG-1181.91	GG-1182.76	GG-1183.61	GG-1184.50	GG-1185.75	GG-1187.00	GG-1188.25	GG-1189.50	GG-1190.75	GG-1192.00	GG-1193.25	GG-1194.50	GG-1195.75	GG-1196.93
DATUM 1060	1179.5	1181.5	1183.7	1184.1	1179.3	1162.5	1139.4	1120.2	1118.3	1093.1	1082.6	1081.7	1080.1	1077.6	1075.4	1075.4	1078.2	1087.1	1102.3	1129.1	1139.0	1156.74

POT STA 50+82.33 RAMP C CONSTR =  
 POC STA 14+34.91 SR 1005 SURVEY =  
 POT STA 51+40.48 RAMP C =  
 POT STA 2020+85.48, 36.00' LT  
 SR 0080 EB CONSTR =

CONTINUE ON SHEET 25 OF 26

USER: LSAJDM  
 PATH: s:\pwork\eng\res\10\140742833\  
 FILE: 00808B-RD-PR04.dgn  
 PLOT DATE: 11-23-2021 12:44:17 PM  
 MODEL: SHEET 2



**Appendix D**  
**Toll Facility Design Plans**

4/28/2021  
PLOTTED:

PLAN PREPARATION

PENNDOT PROJECT MANAGER: BRIAN E. SHUNK, PE  
WSP USA PROJECT MANAGER: TOM FRIEL, PE

DISTRICT	COUNTY	TOWNSHIP	BOROUGH	ROUTE	SECTION	TOTAL SHEETS
4-0	LUZERNE		WHITE HAVEN	0080	08B	6
5-0	CARBON		EAST SIDE	0080	08B	
	CARBON	KIDDER		0080	08B	

SR 0080 PREVIOUSLE KNOW AS LR 1009

MPMS: XXXXXX  
ECMS: EXXXXX

# COMMONWEALTH OF PENNSYLVANIA



## DEPARTMENT OF TRANSPORTATION

### DRAWINGS FOR CONSTRUCTION

### OF OPEN ROAD CASHLESS TOLLING FACILITY

STATE ROUTE 0080 SECTION 08B  
IN CARBON COUNTY

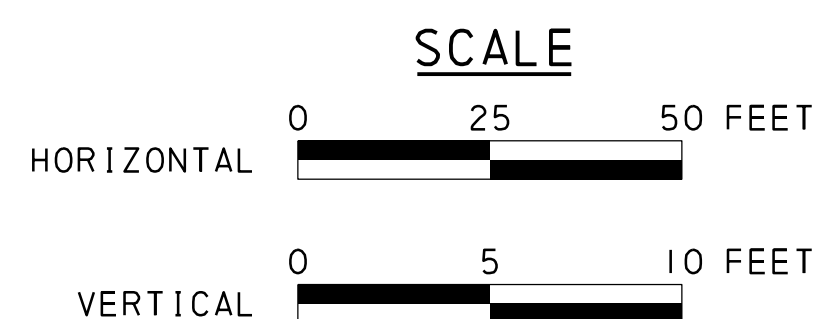
FROM STA 2056+00 TO STA 2065+00 EASTBOUND LENGTH 5042.00 FT 0.955 MI  
FROM SEG 2734 OFFSET 1500 TO SEG 2734 OFFSET 2645  
FROM STA 2256+00 TO STA 2265+00 WESTBOUND  
FROM SEG 2735 OFFSET 1300 TO SEG 2735 OFFSET 1900

Toll Facility Plans are being updated.  
Decision has been made to toll in eastbound  
direction only. The P3 Development Entity  
will construct tolling gantry over eastbound  
lanes only.

ALSO INCLUDED:

SIGNING AND PAVEMENT MARKING	27 SHEETS
ELECTRICAL AND COMMUNICATIONS SERVICE	5 SHEETS
STRUCTURE	XX SHEETS
S-XXXXX	XX SHEETS
ARCHITECTURAL	XX SHEETS
CROSS SECTIONS	XX SHEETS

SR 0080 ESTABLISHED AS A LIMITED ACCESS HIGHWAY FROM  
STATION 115+45.00 TO STATION 366+16.00 EASTBOUND &  
STATION 116+76.00 TO STATION 366+16.00 WESTBOUND BY  
PLAN FOR LEGISLATIVE ROUTE 1009 SECTION 89-R/W  
APPROVED BY THE GOVERNOR ON SEPTEMBER 25, 1962.



DESIGN DESIGNATION

SR 0080 EASTBOUND TRAFFIC DATA

CURRENT ADT - 16740 (2023)  
 DESIGN YEAR ADT - 25366 (2043)  
 DHV - 2030  
 D - 100%  
 T - 35%  
 HIGHWAY CLASSIFICATION - RURAL INTERSTATE  
 DESIGN SPEED - 70 MPH  
 PAVEMENT WIDTH - 24'-0" (2-12' LANES)  
 SHOULDER WIDTH - 8'-0" LT AND 12'-0" RT  
 MEDIAN WIDTH - 36'-0"

SR 0080 WESTBOUND TRAFFIC DATA

CURRENT ADT - 16553 (2023)  
 DESIGN YEAR ADT - 25083 (2043)  
 DHV - 2007  
 D - 100%  
 T - 37%  
 HIGHWAY CLASSIFICATION - RURAL INTERSTATE  
 DESIGN SPEED - 70 MPH  
 PAVEMENT WIDTH - 24'-0" (2-12' LANES)  
 SHOULDER WIDTH - 8'-0" LT AND 12'-0" RT  
 MEDIAN WIDTH - 36'-0"

# 30% DESIGN SUBMISSION

OPERATOR: JSLJL677935  
FILE NAME: \$FILES#

PREPARED BY: WSP USA 4 PENN CENTER 1600 JFK BLVD., SUITE 510 PHILADELPHIA, PA. 19103	RECOMMENDED DATE: _____
	_____ DISTRICT EXECUTIVE
	RECOMMENDED DATE: _____
	_____ DEPUTY SECRETARY
REGISTERED PROFESSIONAL ENGINEER	APPROVED DATE: _____
DATE: _____	SECRETARY OF TRANSPORTATION (ON BEHALF OF THE GOVERNOR AS WELL AS THE SECRETARY)

4/27/2021 PLOTTED:

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
4-0	LUZERNE	0080	08B	2 OF 27
5-0	CARBON	0080	08B	
**				
REVISION NUMBER	REVISIONS	DATE	BY	

\*\* BOROUGH OF WHITE HAVEN  
BOROUGH OF EAST SIDE  
TOWNSHIP OF KIDDER

**TABULATION OF OVERALL LENGTH**

SR 0080 STA EB 2056+00 TO STA EB 2065+00 = 900 FT = 0.170 MI  
AND

SR 0080 STA WB 2256+00 TO STA WB 2265+00 = 900 FT = 0.170 MI

**TABULATION OF CONSTRUCTION LENGTH**

SR 0080 STA EB 2058+05.67 TO STA EB 2063+00.00 = 494.33 FT = 0.093 MI  
SR 0080 STA WB 2259+20.19 TO STA WB 2262+13.09 = 292.9 FT = 0.055 MI  
TOTAL = 787.23 FT = 0.149 MI

**LIST OF EQUALITIES**

NONE

**EARTHWORK SUMMARY ENTIRE PROJECT**

<p>THE INFORMATION ON ESTIMATED AMOUNTS OF EARTHWORK HAS BEEN USED IN THE PRELIMINARY ESTIMATE. DO NOT USE AS A WAIVER OF ANY PROVISIONS OF THE SPECIFICATIONS AND CONTRACTS.</p>												<p>* INCLUDES ALL BORROW ITEMS ** INCLUDED IN LUMP SUM STRUCTURE ITEM *** INCLUDES 47 CY OF EXIST PAV'T AND 107 CY UNSUITABLE MAT'L</p>		<p>▲ QUANTITY BASED ON 1.5 TONS/CY ▲▲ ITEM NO 9000-1000 REMOVAL, STOCKPILING, AND PLACEMENT OF EXIST STREAMBED MAT'L</p>	
CUBIC YDS OF EXC		CUBIC YDS OF COMPLETED EMBANKMENT	CUBIC YDS OF SEL BORROW EXC, STRUCTURE BACKFILL	CUBIC YDS OF SEL BORROW EXC, COARSE AGGR, NO. 2A	CUBIC YDS OF SEL BORROW EXC, COARSE AGGR, NO. 8	CUBIC YDS OF NO. 57 COARSE AGGR	CUBIC YDS OF SEL BORROW EXC, COARSE AGGR, LIMESTONE ▲	CUBIC YDS OF SEL BORROW EXC, COARSE AGGR, LIMESTONE ▲	CUBIC YDS OF STREAMBED MAT'L ▲▲	CUBIC YDS OF SEL BORROW EXC, ROCK, CLASS R-6, LIMESTONE, GROUTED ▲	CUBIC YDS OF SEL BORROW EXC, 206 ROCK	CUBIC YDS OF SUBBASE (NO. 2A)	CUBIC YDS OF WASTE		
CLASS 1	CLASS 3														
-	-	-	-	-	-	-	-	-	-	-	-	-	-		

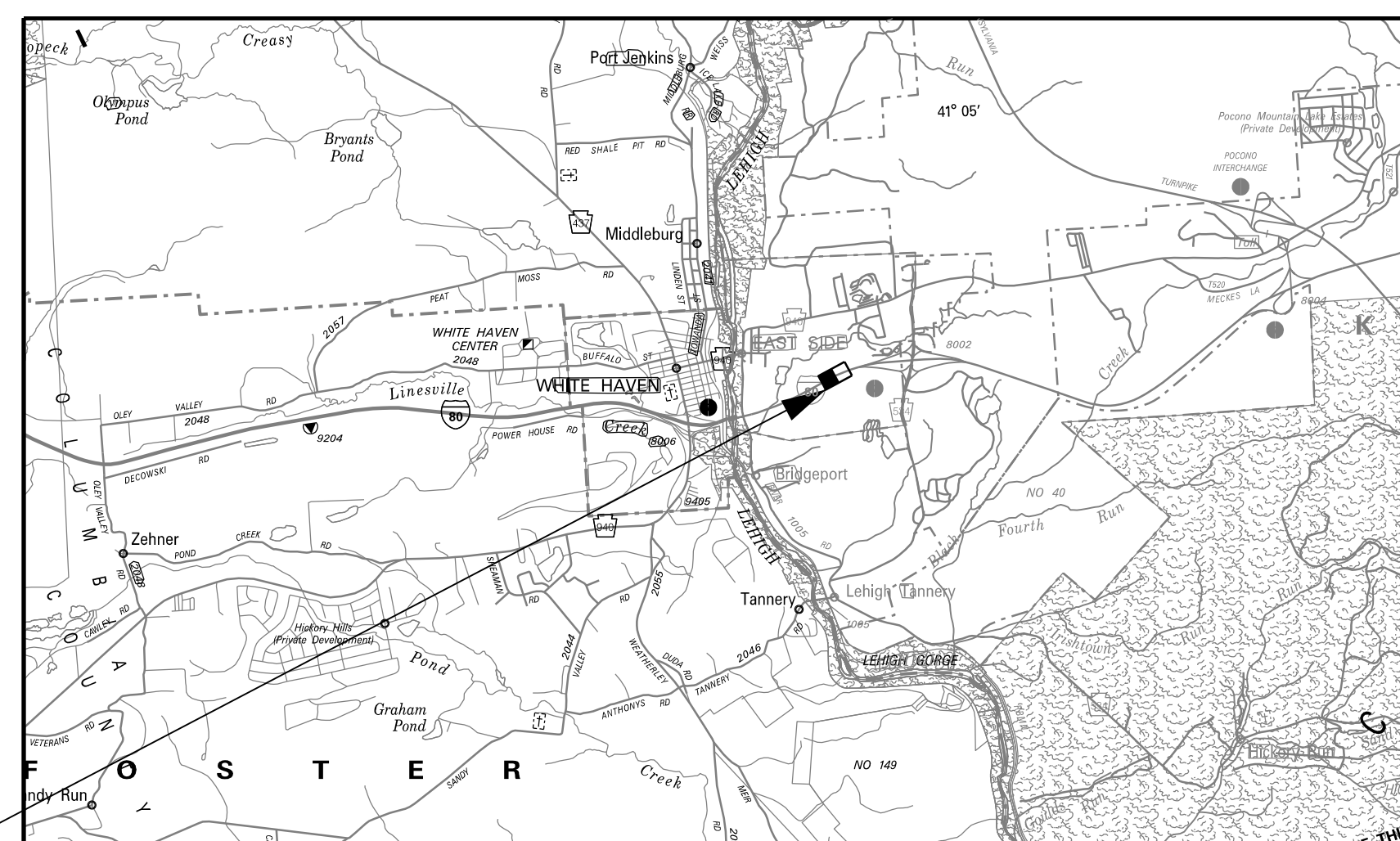
**INDEX OF DRAWINGS**

TITLE	SHEET(S)
TITLE SHEET	1
GENERAL NOTES	2
TYPICAL SECTIONS	3
ROADWAY PLAN	4
PROFILES	5-6

**SUMMARY OF PROJECT COORDINATES**

THE HORIZONTAL CONTROL FOR THIS PROJECT IS BASED ON THE PENNSYLVANIA STATE PLANE COORDINATE SYSTEM NORTH ZONE (SPCS 83)

DIRECTION	RTE	ELEMENT	STATION	POINT	COORDINATES		BEARING	DELTA	RADIUS	LENGTH	TANGENT	EXTERNAL
					NORTH	EAST						
EB	SR 0080	CIRCULAR	2053+75.10	PC	331422.31	2517734.15	N 61°18'2" E	24°27'55" R	4560.119	1947.181	988.658	105.943
			2063+63.76	PI	331897.07	2518601.36						
		LINEAR	2073+22.28	PT	331970.07	2519587.32	N 85°45'57" E	-	-	746.790	-	-
			2080+69.07	POE	332025.20	2520332.07						
WB	SR 0080	CIRCULAR	2248+96.16	PC	331338.69	2517266.07	N 61°13'17" E	24°30'32" R	5742.620	2456.466	1247.310	133.989
			2261+43.47	PI	331939.18	2518359.32						
		LINEAR	2273+52.63	PT	332032.05	2519603.17	N 85°43'49" E	-	-	970.967	-	-
			2283+23.59	POE	332104.34	2520571.44						



LIMIT OF WORK  
STA XXX+XX.XX  
SEG XXXX OFF XXXX  
SR 0080 SEC 352  
BOROUGH OF EAST SIDE  
CARBON COUNTY

**LOCATION MAP**  
SCALE IN MILES



**LEGEND**

- PROJECT
- INTERSTATE HIGHWAY
- STATE HIGHWAY
- TOWNSHIP OR LOCAL ROAD
- MUNICIPAL BOUNDARY

**30% DESIGN SUBMISSION**

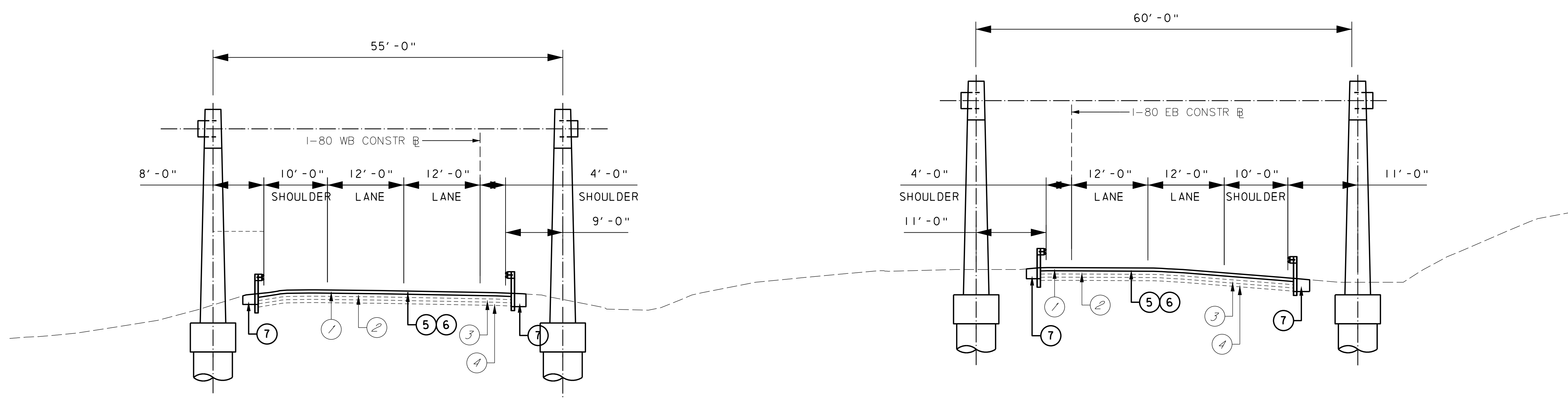
4/28/2021  
PLOTTED:

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
4-0	LUZERNE	0080	08B	3 OF 6
5-0	CARBON	0080	08B	

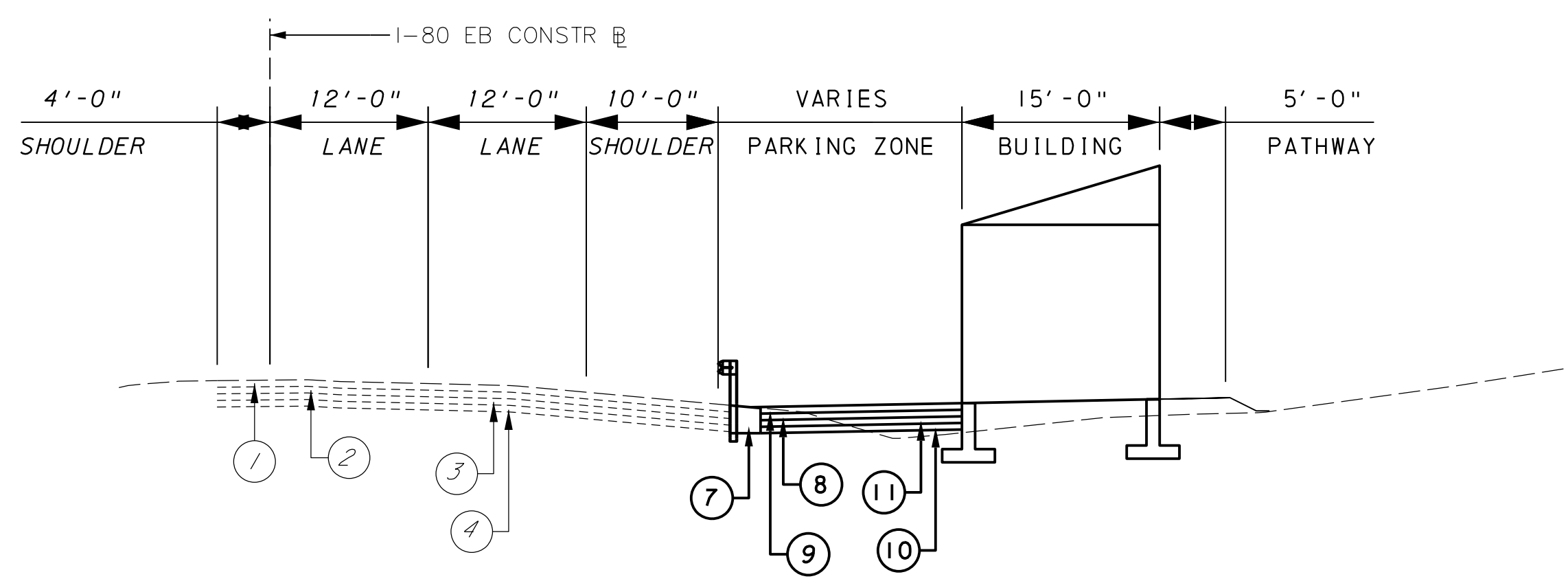
REVISION NUMBER	REVISIONS	DATE	BY

\*\* BOROUGH OF WHITE HAVEN  
BOROUGH OF EAST SIDE  
TOWNSHIP OF KIDDER



### TYPICAL SECTION - GANTRY

NOT TO SCALE  
EB STA. 2059+26 TO STA. 2060+78  
WB STA. 2059+20 TO STA. 2060+70



### TYPICAL SECTION - TOLL BUILDING

NOT TO SCALE  
EB STA. 2061+20 TO STA. 2061+60

### PAVEMENT LEGEND

- 1 SUPERPAVE ASPHALT MIXTURE DESIGN, HMA WEARING COURSE, PG 64-28, 10 TO < 30 MILLION ESALS, 12.5 MM MIX, SRL-E, 1 1/2" DEPTH (TYP.)
- 2 SUPERPAVE ASPHALT MIXTURE DESIGN, HMA BINDER COURSE, PG 76-22, 10 TO < 30 MILLION ESALS, 19.0 MM MIX, 2 1/2" DEPTH
- 3 SUPERPAVE ASPHALT MIXTURE DESIGN, HMA BASE COURSE, PG 64-22, 10 TO < 30 MILLION ESALS, 25.0 MM MIX, 5" DEPTH
- 4 SUBBASE 10" DEPTH
- 5 MILLING OF BITUMINOUS PAVEMENT SURFACE, 2" DEPTH
- 6 SUPERPAVE ASPHALT MIXTURE DESIGN, HMA WEARING COURSE, PG 64-28, 10 TO < 30 MILLION ESALS, 12.5 MM MIX, SRL-E, 2" DEPTH (TYP.)
- 7 SUBBASE UNDER GUIDE RAIL
- 8 SUPERPAVE ASPHALT MIXTURE DESIGN, HMA BINDER COURSE, PG 64-22, 10 TO < 30 MILLION ESALS, 19.0 MM MIX, 3" DEPTH
- 9 SUPERPAVE ASPHALT MIXTURE DESIGN, HMA WEARING COURSE, PG 64-22, 10 TO < 30 MILLION ESALS, 12.5 MM MIX, 2" DEPTH SRL-E
- 10 SUBBASE 6" DEPTH
- 11 SUPERPAVE ASPHALT MIXTURE DESIGN, HMA BASE COURSE, PG 64-22, 10 TO < 30 MILLION ESALS, 25.0 MM MIX, 4" DEPTH

TYPICAL SECTIONS

**30% DESIGN  
SUBMISSION**

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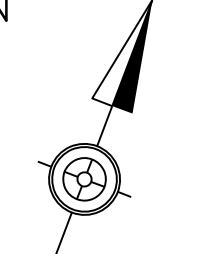
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PLOTTED:

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
4-0	LUZERNE	0080	08B	4 OF 6
5-0	CARBON	0080	08B	

REVISION NUMBER	REVISIONS	DATE	BY

\*\* BOROUGH OF WHITE HAVEN  
BOROUGH OF EAST SIDE  
TOWNSHIP OF KIDDER



N/F  
JAMES L. LACHETTE JR.  
PARCEL # 89-10-B15  
BOOK 407 PAGE 544  
REC. DATE: OCTOBER 1, 1979  
INST# \*\*\*  
AREA = 9.45 AC. (D)

SR 0080 WB  
CONSTR @  
PI STA 2261+43.47  
DELTA = 24°30'32"RT  
L = 2456.47'  
T = 1247.31'  
R = 5742.62'  
E = 133.90'  
PC STA 2248+96.16  
PT STA 2273+52.63

TYPE 31 STRONG POST  
ANCHOR TERMINAL  
NORTHING: XXXXXXXX  
EASTING: XXXXXX

TYPE 31 STRONG POST  
ANCHOR TERMINAL  
NORTHING: XXXXXXXX  
EASTING: XXXXXX

PAVEMENT MILL AND OVERLAY  
FROM STA. XXX+XX TO STA. XXX+XX

REMOVE EXISTING GUIDE  
RAIL END TREATMENT

NORTHING: XXXXXXXX  
EASTING: XXXXXX

END TYPE 31-SC GUIDE RAIL  
TIE TO EXISTING GUIDE RAIL

PERMANENT IMPACT ATTENUATING  
DEVICE, TYPE II, TEST LEVEL  
3, TANGENT (MASH) MODIFIED  
NORTHING: XXXXXXXX  
EASTING: XXXXXXXX

SURVEY AND CONSTRUCTION @  
SR 0800 WESTBOUND

SR 0080 WESTBOUND  
2258

TYPE 31-SC  
GUIDE RAIL

PERMANENT IMPACT ATTENUATING  
DEVICE, TYPE II, TEST LEVEL  
3, TANGENT (MASH) MODIFIED  
NORTHING: XXXXXXXX  
EASTING: XXXXXXXX

SURVEY AND CONSTRUCTION @  
SR 0800 EASTBOUND

SR 0080 EASTBOUND  
2058

TYPE 31-SC  
GUIDE RAIL

NORTHING: XXXXXXXX  
EASTING: XXXXXXXX

NORTHING: XXXXXXXX  
EASTING: XXXXXXXX

TYPE 31 STRONG POST  
ANCHOR TERMINAL  
NORTHING: XXXXXXXX  
EASTING: XXXXXXXX

INSTALL NEW POWER AND  
COMMUNICATION FOR  
TOLLING SYSTEM. SEE  
ELECTRICAL AND  
COMMUNICATION SERVICE  
DRAWINGS FOR DETAILS

TYPE 31 STRONG POST ANCHOR TERMINAL  
NORTHING: XXXXXXXX  
EASTING: XXXXXXXX

OFF RAMP TO RT-534

SR 0080 EB  
CONSTR @  
PI STA 2063+63.76  
DELTA = 24°27'55"RT  
L = 1947.18'  
T = 988.66'  
R = 4560.12'  
E = 105.94'  
PC STA 2053+75.10  
PT STA 2073+22.28

PERMANENT IMPACT ATTENUATING  
DEVICE, TYPE II, TEST LEVEL  
3, TANGENT (MASH) MODIFIED  
NORTHING: XXXXXXXX  
EASTING: XXXXXXXX

PAVEMENT MILL AND OVERLAY  
FROM STA. XXX+XX TO STA. XXX+XX

NORTHING: XXXXXXXX  
EASTING: XXXXXXXX

NORTHING: XXXXXXXX  
EASTING: XXXXXXXX

NORTHING: XXXXXXXX  
EASTING: XXXXXXXX

NORTHING: XXXXXXXX  
EASTING: XXXXXXXX

NORTHING: XXXXXXXX  
EASTING: XXXXXXXX

LEGAL R/W LINE  
FOR LIMITED ACCESS



ROADWAY PLAN

**30% DESIGN  
SUBMISSION**

DES: XXX DWG: XXX CKD: XXX

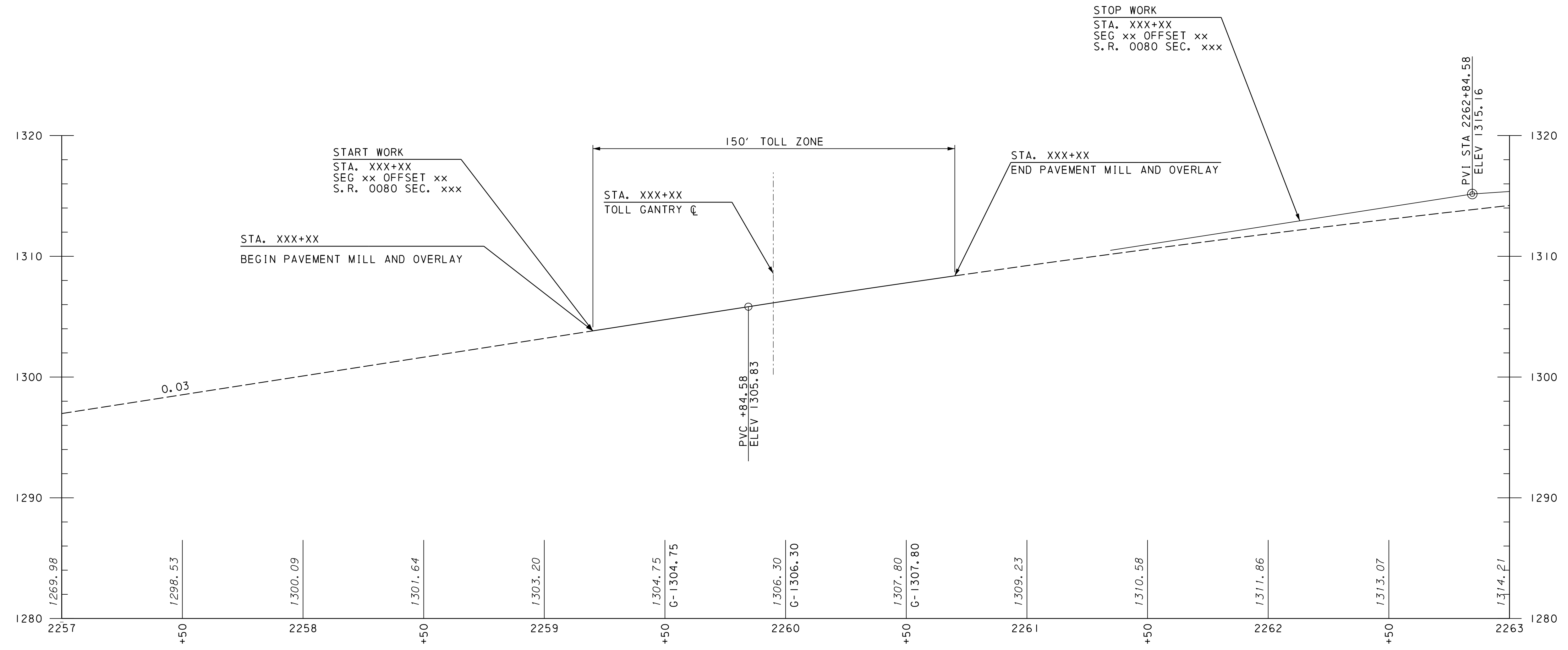
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4/27/2021  
PLOTTED:

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
4-0	LUZERNE	0080	08B	5 OF 27
5-0	CARBON	0080	08B	
**				
REVISION NUMBER	REVISIONS	DATE	BY	

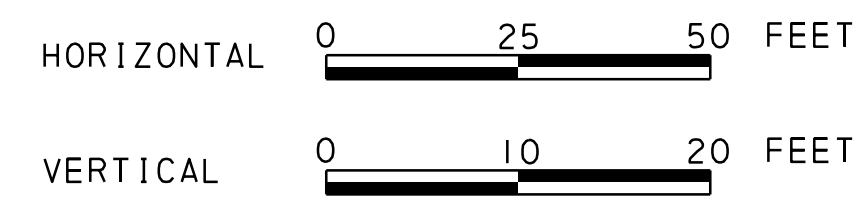
\*\* BOROUGH OF WHITE HAVEN  
BOROUGH OF EAST SIDE  
TOWNSHIP OF KIDDER

VC = 600.00'  
MO = -1.29'



CONSTR B PROFILE - SR 0080 WB

SCALE



PROFILE - WB

**30% DESIGN  
SUBMISSION**

4008008B\_R0pr\_01.d

DES: XXX DWG: XXX CKD: XXX



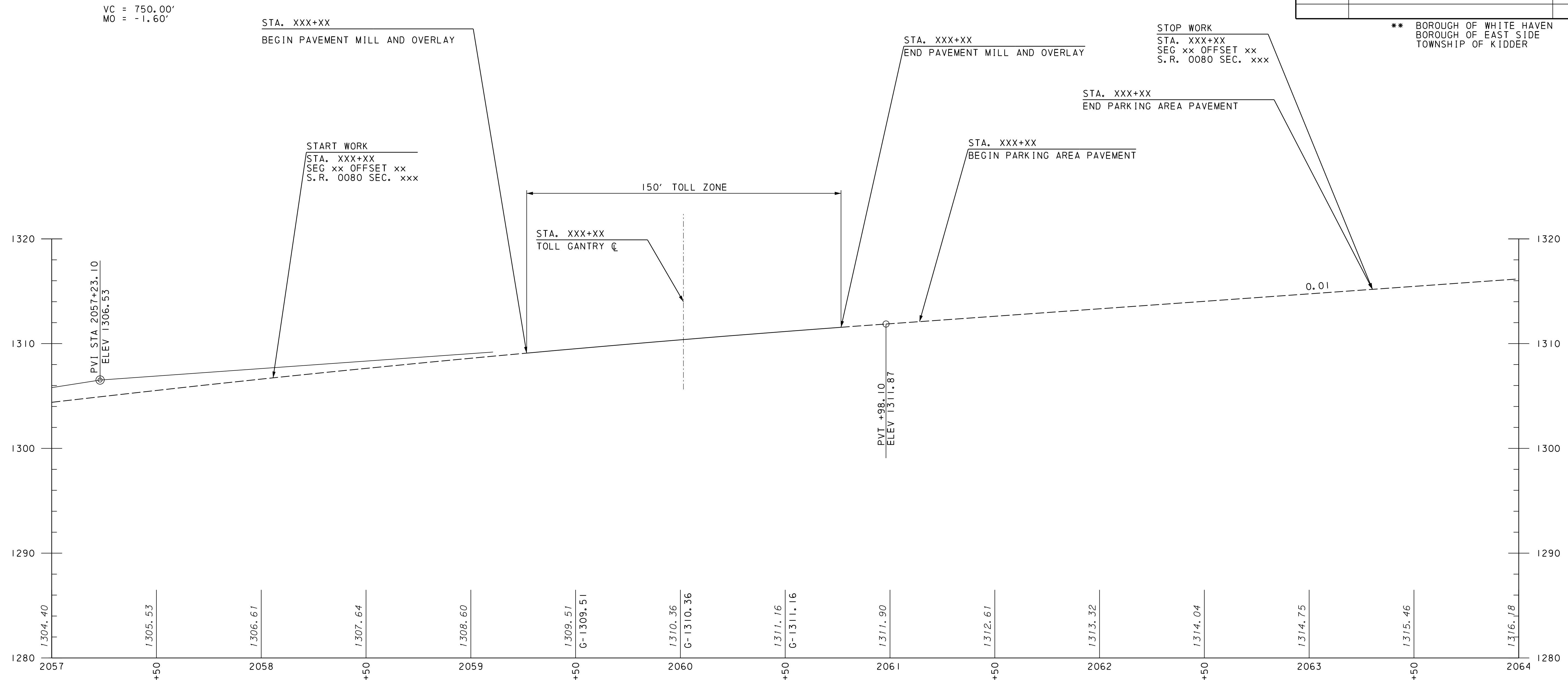
4/27/2021  
PLOTTED:

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
4-0	LUZERNE	0080	08B	6 OF 27
5-0	CARBON	0080	08B	

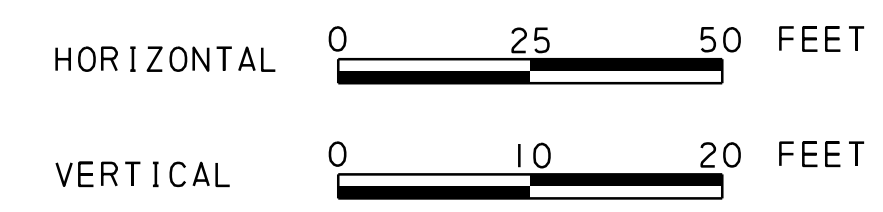
REVISION NUMBER	REVISIONS	DATE	BY

\*\* BOROUGH OF WHITE HAVEN  
BOROUGH OF EAST SIDE  
TOWNSHIP OF KIDDER



CONSTR B PROFILE - SR 0080 EB

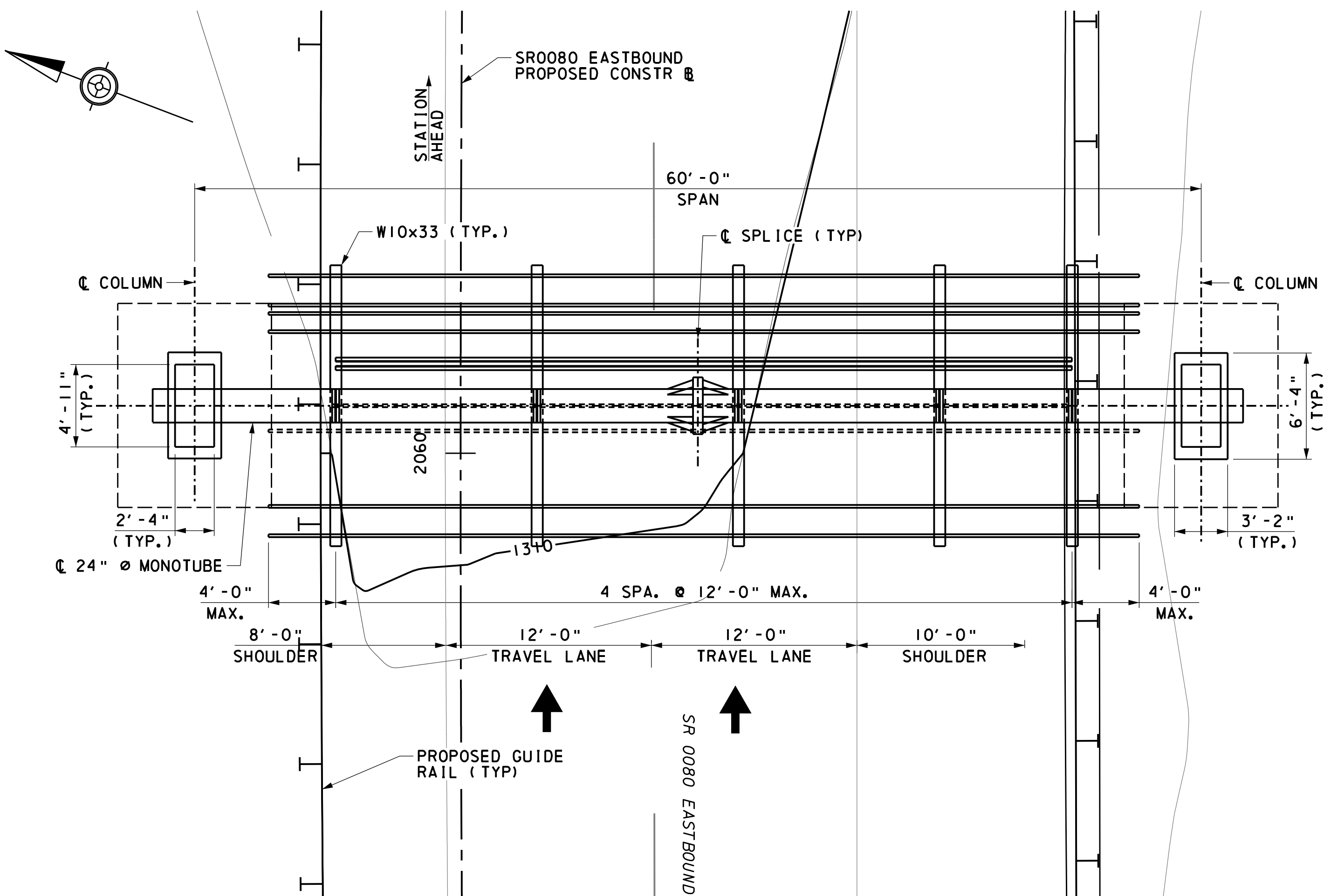
SCALE



PROFILE - EB

**30% DESIGN  
SUBMISSION**

4008008B\_RDpr\_02.d

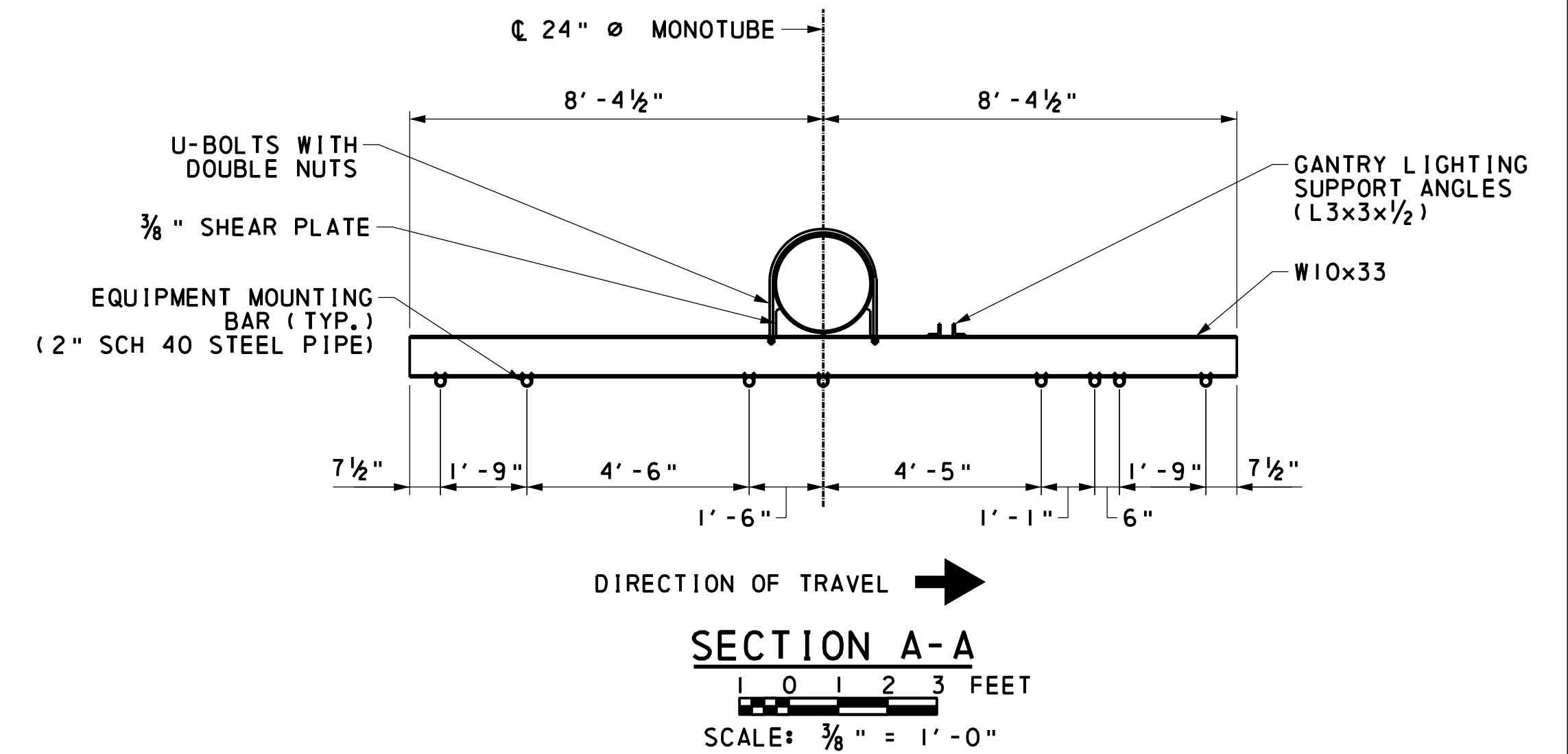


**PLAN VIEW**  
 2 0 2 4 6 FEET  
 SCALE: 3/16" = 1'-0"

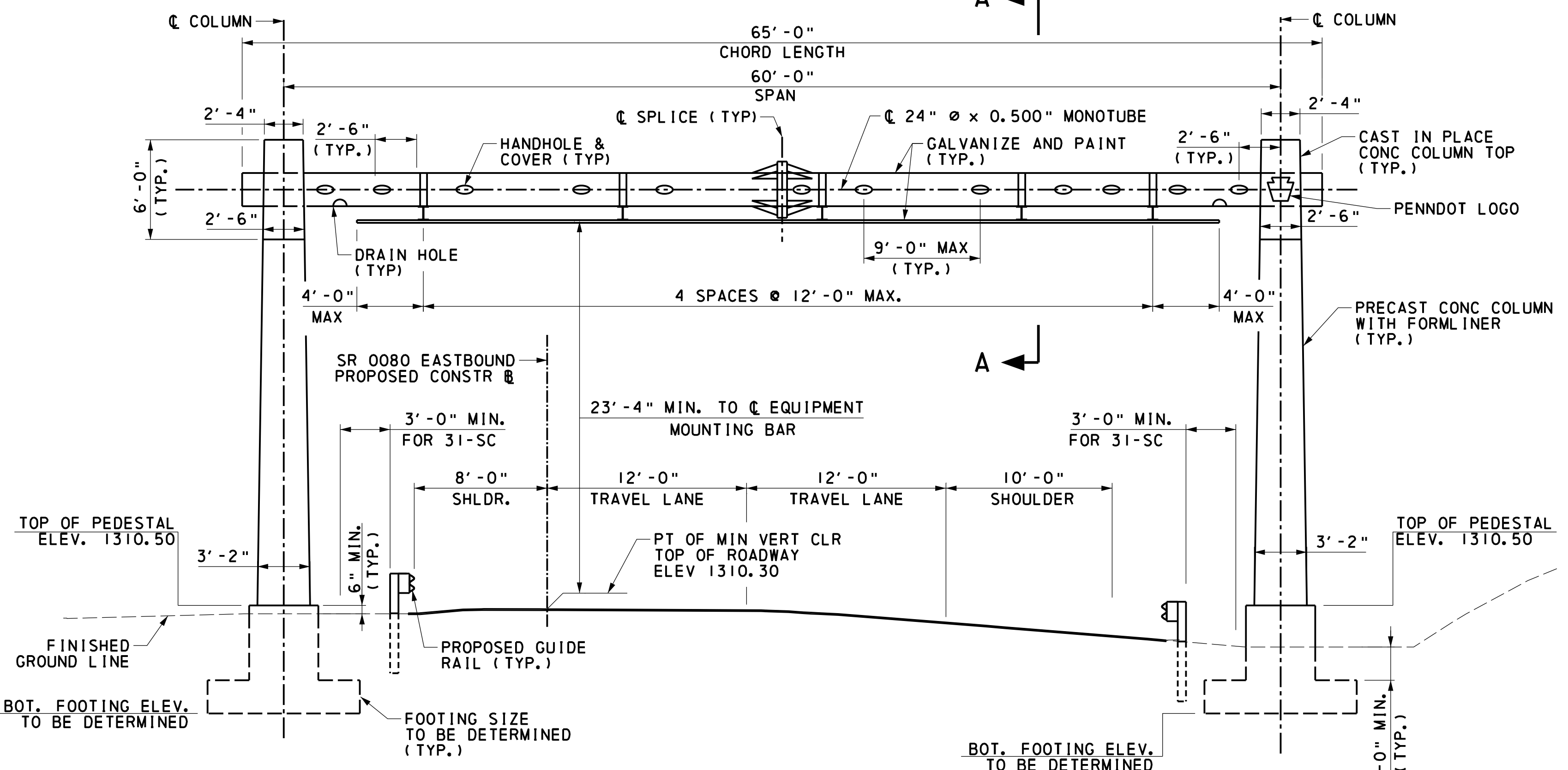
NOTE: WESTBOUND ORT GANTRY NOT SHOWN FOR CLARITY.

Toll Facility Plans are being updated. Decision has been made to toll in eastbound direction only. The P3 Development Entity will construct tolling gantry over eastbound lanes only.

- GENERAL NOTES:**
1. PROVIDE MATERIALS AND PERFORM WORK IN ACCORDANCE WITH SPECIFICATIONS, PUBLICATION 408/2020, AASHTO/AWS D1.5M/D1.5-2015 BRIDGE WELDING CODE (USE AASHTO/AWS D1.1/D1.1M-2015 FOR WELDING NOT COVERED IN AASHTO D1.5M/D1.1-2015), AND CONTRACT SPECIAL PROVISIONS.
  2. DESIGN SPECIFICATIONS
    - \* AASHTO 1ST EDITION LRFD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES AND TRAFFIC SIGNALS (2015).
    - \* AASHTO 8TH EDITION LRFD BRIDGE DESIGN SPECIFICATION (2017).
    - \* PENNDOT PUBLICATION 15M, DM-4.
  3. DESIGN IS IN ACCORDANCE WITH THE LRFD METHOD.
  4. WIND LOADS
    - \* BASIC WIND SPEED = 115 MPH (3 SECOND GUST)
    - \* FATIGUE IMPORTANCE FACTOR = 1.0 FOR NATURAL WIND GUSTS.
    - \* FATIGUE IMPORTANCE FACTOR = 1.0 FOR TRUCK-INDUCED GUSTS.
  5. PROVIDE STRUCTURAL STEEL, OTHER THAN PIPES, CONFORMING TO AASHTO M270, GRADE 50 (ASTM A709, GRADE 50) DESIGNATION EXCEPT WHEN NOTED OTHERWISE.
  6. PROVIDE WELDED OR SEAMLESS STEEL PIPE CONFORMING TO PUBLICATION 408, SECTION 948.2(g) 1.
  7. PROVIDE HIGH-STRENGTH BOLTS CONFORMING TO ASTM F3125, GRADE A325 AND ANCHOR BOLTS CONFORMING TO ASTM F1554, GRADE 55.
  8. USE CLASS AA CEMENT CONCRETE MODIFIED TO 5000 PSI IN THE CAST-IN-PLACE COLUMN TOP.
  9. USE 5000 PSI CEMENT CONCRETE FOR PRECAST COLUMN PER PUBLICATION 408, SECTION 714.
  10. ALL STRUCTURAL STEEL TO BE GALVANIZED AND PAINTED. PAINT COLOR TO BE DETERMINED.
  11. FORMLINER PATTERN ON THE COLUMN TO BE DETERMINED.



Mark	Description	By	Chk'd.	Recm'd.	Date
REVISIONS					



**FRONT ELEVATION**  
 2 0 2 4 6 FEET  
 SCALE: 3/16" = 1'-0"

DPM	JJ	MM	JJ
DESIGNED	CHECKED	DRAWN	CHECKED

MONOPIE SIGN STRUCTURES	BC-747M	08-04-2017
DESCRIPTION	DWG. NO.	REC'D DATE
SUPPLEMENTAL DRAWINGS		

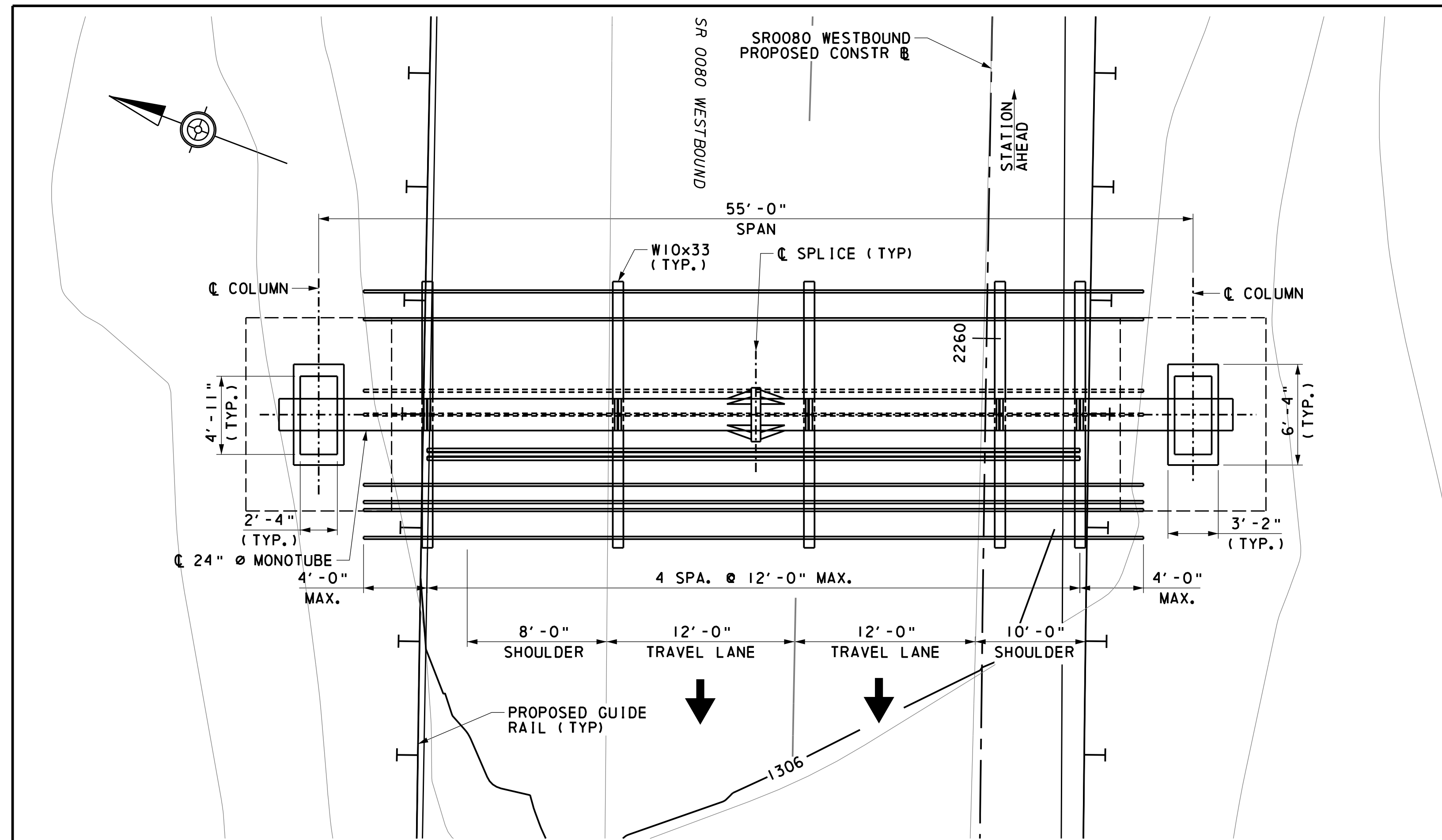
PREPARED BY:  
**wsp** WSP USA  
 2401 Walnut Street  
 Suite 501  
 Philadelphia, PA 19103

BMS 04-0080-XXXX-XXXX | MPMS XXXXX | BRKEY XXXXX

COMMONWEALTH OF PENNSYLVANIA  
 DEPARTMENT OF TRANSPORTATION

LUZERNE COUNTY  
 SR 0080 SECTION 08B  
 SR 0080 EASTBOUND STA 2060+02.79  
 SEGMENT XXXX OFFSET XXXX  
 EASTBOUND ORT GANTRY STRUCTURE  
 CONCEPTUAL TYPE, SIZE, AND LOCATION

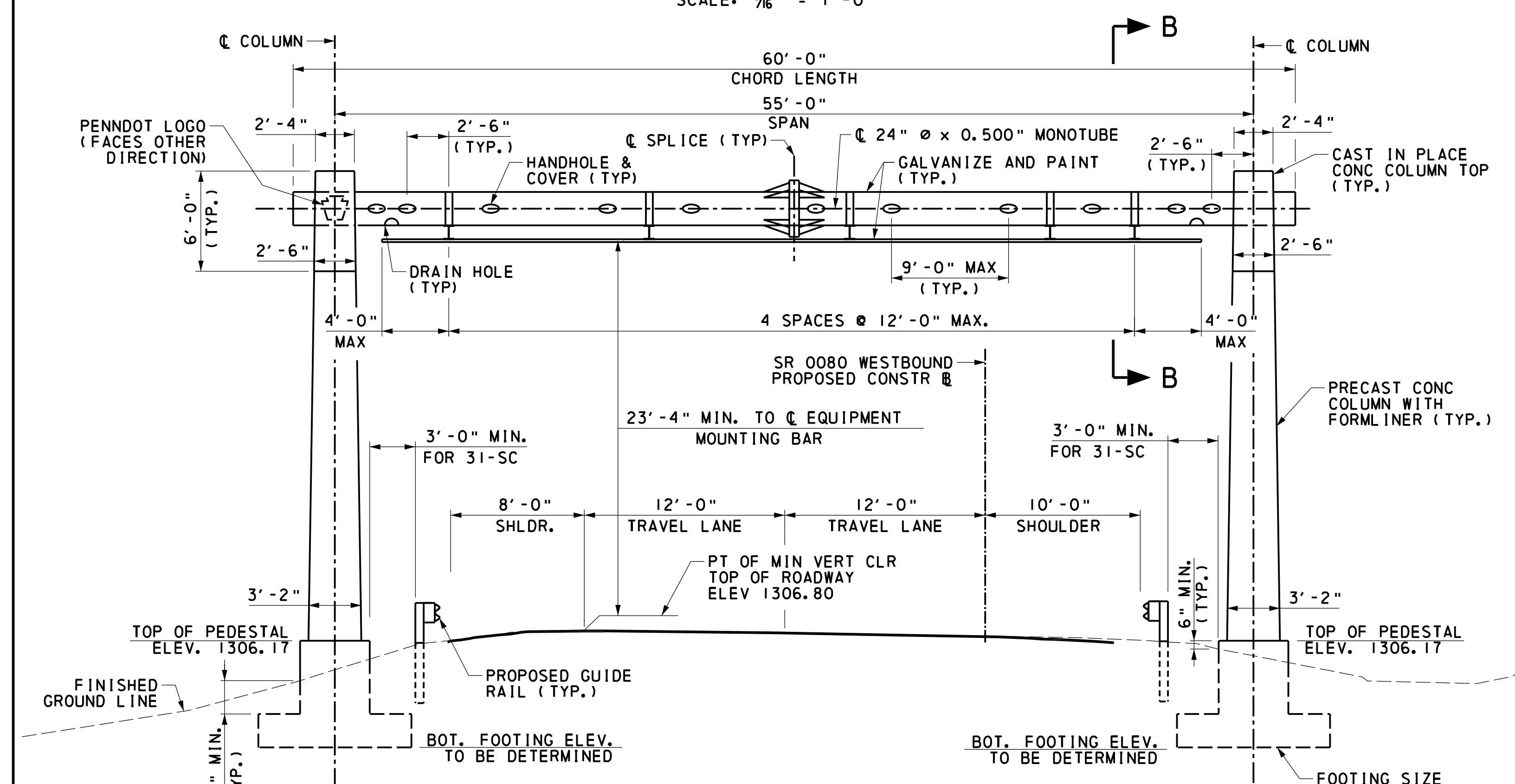
RECOMMENDED	SHEET 1 OF 3
S-XXXXX	



NOTE: EASTBOUND ORT GANTRY NOT SHOWN FOR CLARITY.

PLAN VIEW

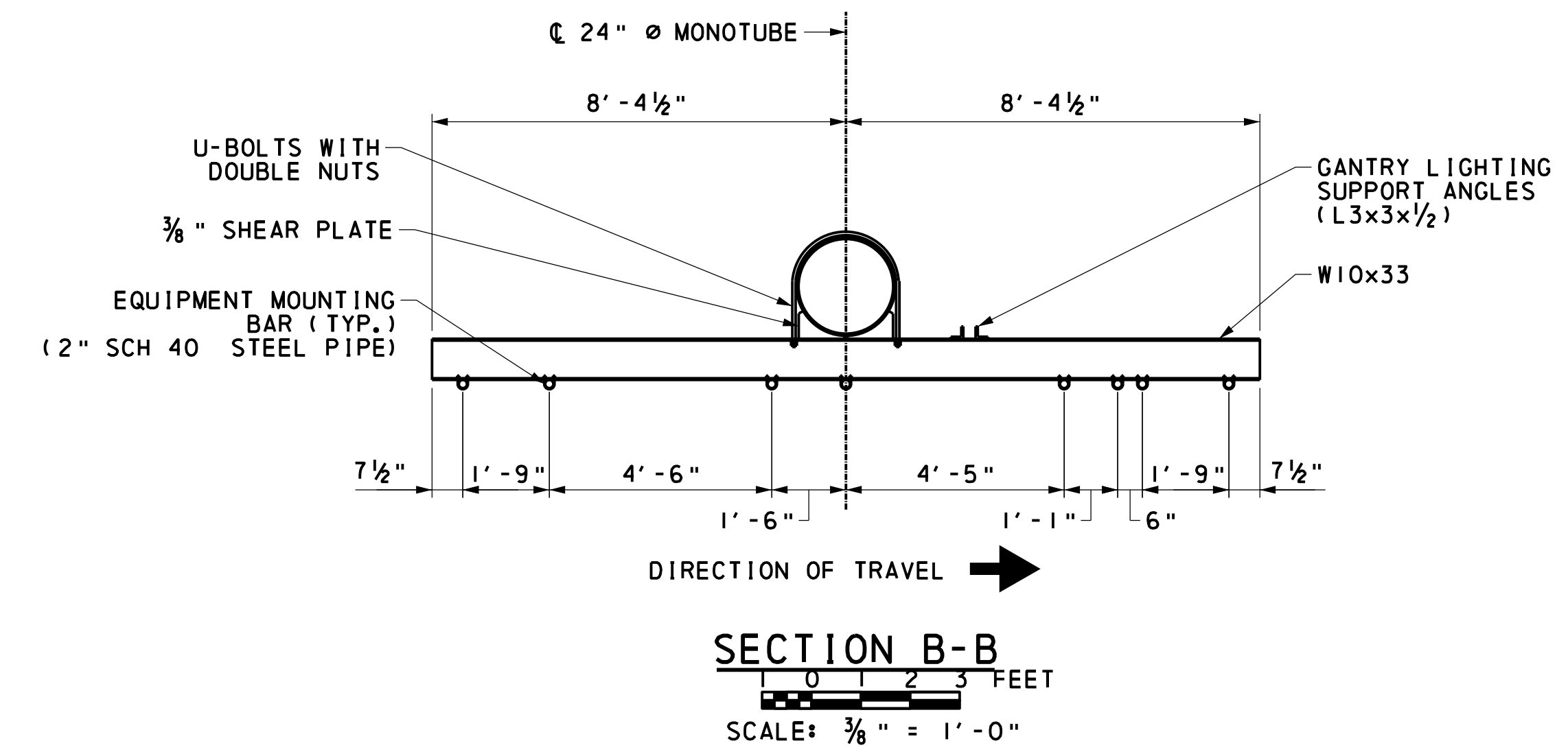
2 0 2 4 6 FEET  
SCALE: 3/8" = 1'-0"



FRONT ELEVATION

2 0 2 4 6 FEET  
SCALE: 3/8" = 1'-0"

DPM	JJ	MM	JJ
DESIGNED	CHECKED	DRAWN	CHECKED



SECTION B-B

0 1 2 3 FEET  
SCALE: 3/8" = 1'-0"

Mark	Description	By	Chk'd.	Recm'd.	Date
REVISIONS					

BMS 04-0080-XXXX-XXXX | MPMS XXXXX | BRKEY XXXXX

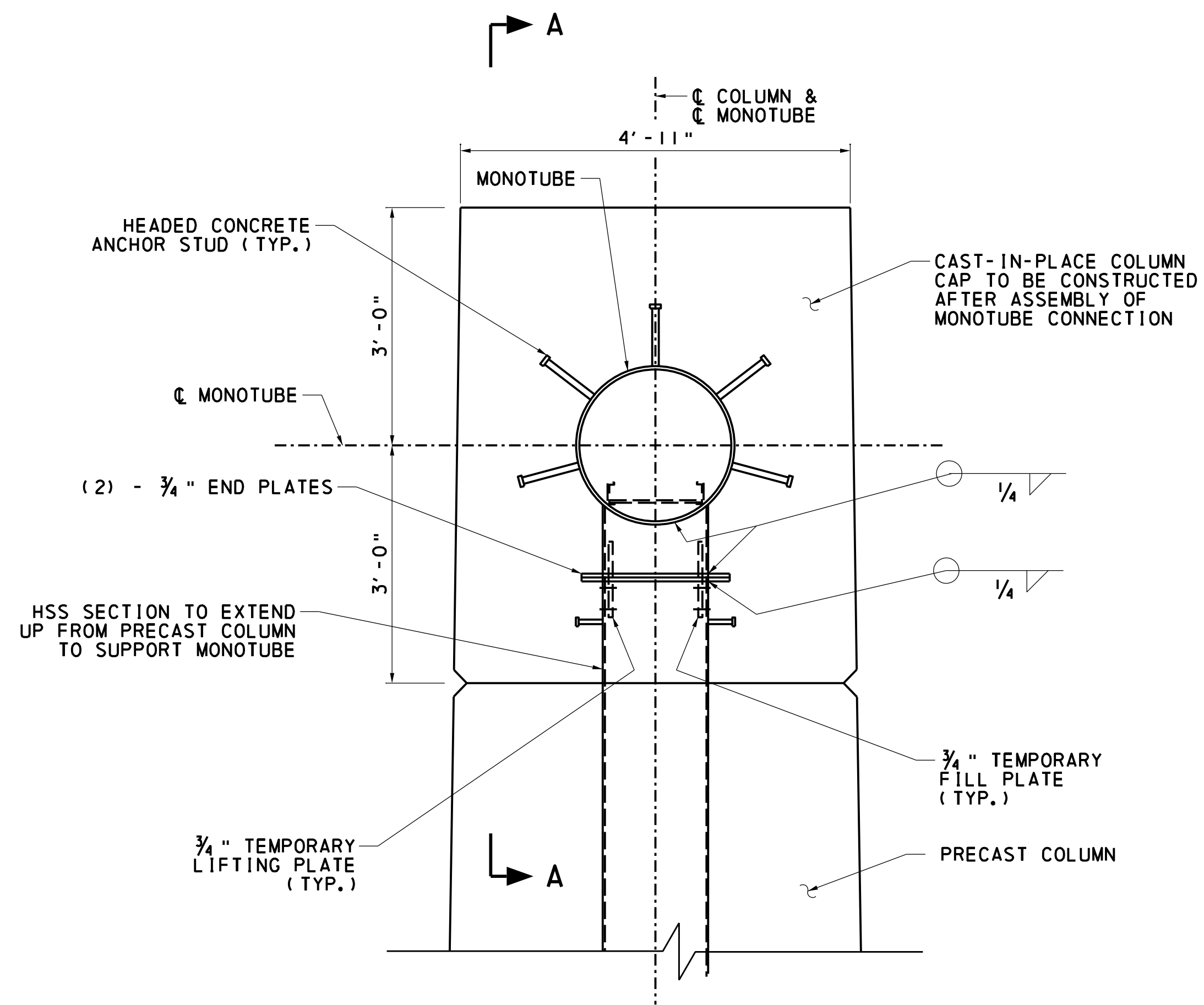
COMMONWEALTH OF PENNSYLVANIA  
DEPARTMENT OF TRANSPORTATION

LUZERNE COUNTY  
SR 0080 SECTION 08B  
SR 0080 WESTBOUND STA 2259+95.20  
SEGMENT XXXX OFFSET XXXX  
WESTBOUND ORT GANTRY STRUCTURE  
CONCEPTUAL TYPE, SIZE, AND LOCATION

PREPARED BY:

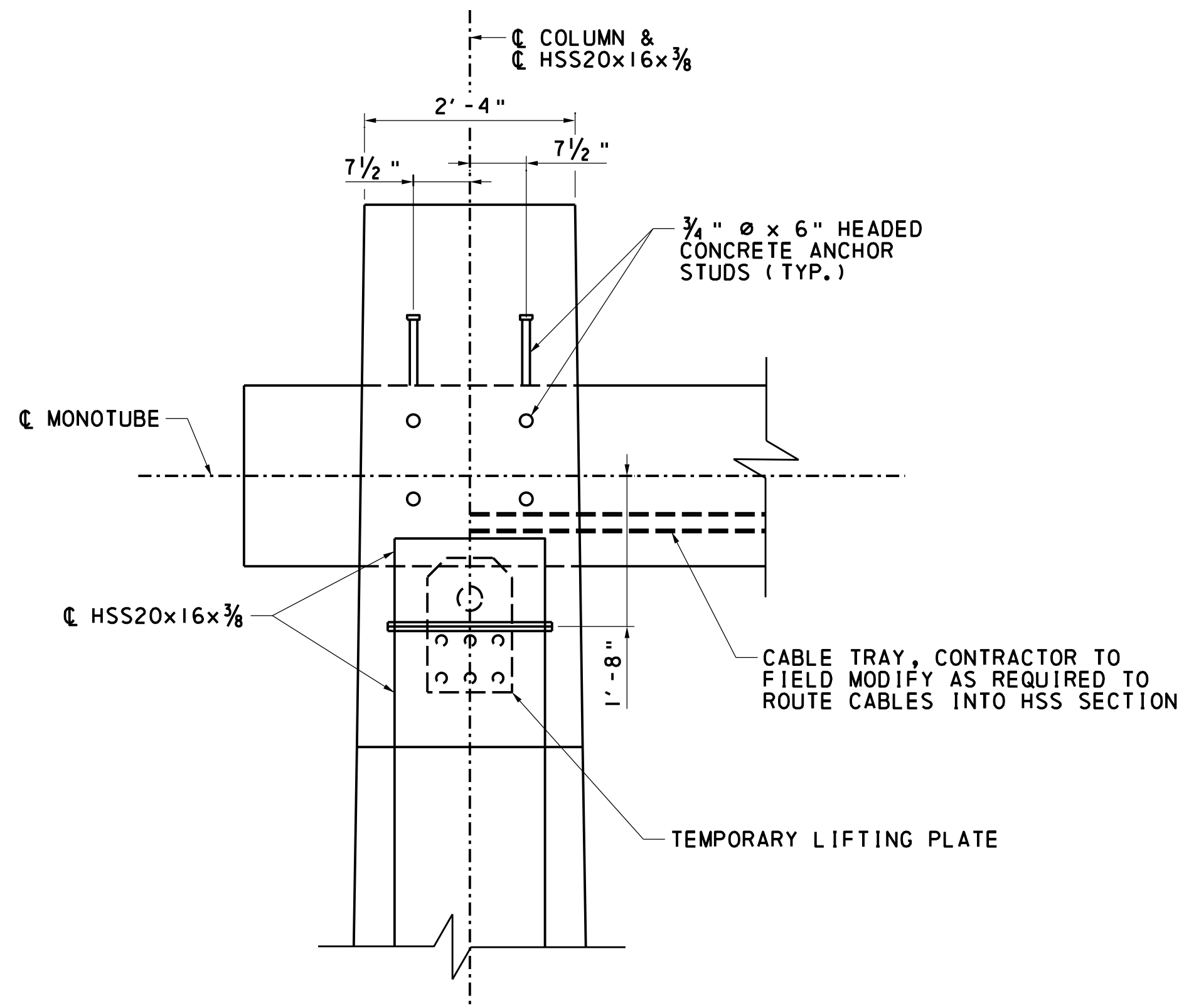
**wsp** WSP USA  
2401 Walnut Street  
Suite 501  
Philadelphia, PA 19103

RECOMMENDED	SHEET 2 OF 3
	S-XXXXX



**MONOTUBE CONNECTION ELEVATION**

12 0 12 INCHES  
SCALE: 3/4" = 1'-0"



**SECTION A-A**

12 0 12 INCHES  
SCALE: 3/4" = 1'-0"

Mark	Description	By	Chk'd.	Recm'd.	Date
REVISIONS					

BMS 04-0080-XXXX-XXXX | MPMS XXXXX | BRKEY XXXXX

**COMMONWEALTH OF PENNSYLVANIA**  
DEPARTMENT OF TRANSPORTATION

**LUZERNE COUNTY**  
SR 0080 SECTION 08B  
SR 0080 STA 2060+02.79  
SEGMENT XXXX OFFSET XXXX  
ORT GANTRY STRUCTURE  
CONCEPTUAL TYPE, SIZE, AND LOCATION

PREPARED BY:  
**wsp** WSP USA  
2401 Walnut Street  
Suite 501  
Philadelphia, PA 19103

RECOMMENDED \_\_\_\_\_ SHEET 3 OF 3  
S-XXXXX

DPM	JJ	MM	JJ
DESIGNED	CHECKED	DRAWN	CHECKED

4/28/2021  
PLOTTED:

# ELECTRICAL AND COMMUNICATIONS SERVICE

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
4-0	LUZERNE	0080	08B	1 OF 5
5-0	CARBON	0080	08B	

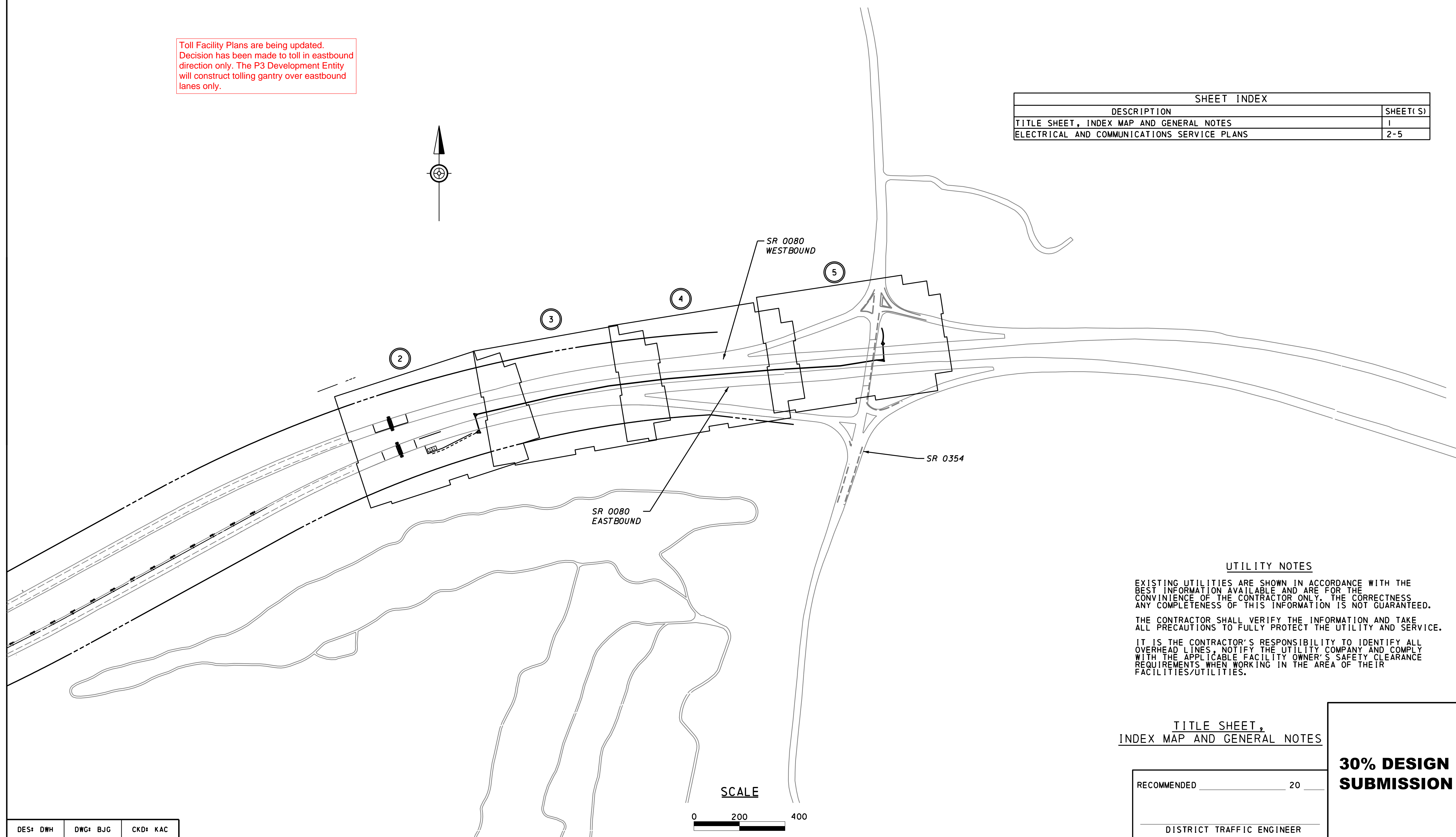
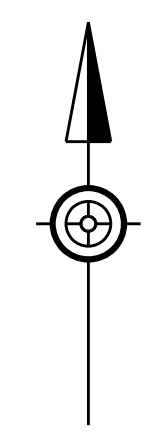
  

REVISION NUMBER	REVISIONS	DATE	BY

\*\* BOROUGH OF WHITE HAVEN  
BOROUGH OF EAST SIDE  
TOWNSHIP OF KIDDER

Toll Facility Plans are being updated.  
Decision has been made to toll in eastbound  
direction only. The P3 Development Entity  
will construct tolling gantry over eastbound  
lanes only.

SHEET INDEX	
DESCRIPTION	SHEET(S)
TITLE SHEET, INDEX MAP AND GENERAL NOTES	1
ELECTRICAL AND COMMUNICATIONS SERVICE PLANS	2-5



**UTILITY NOTES**

EXISTING UTILITIES ARE SHOWN IN ACCORDANCE WITH THE BEST INFORMATION AVAILABLE AND ARE FOR THE CONVENIENCE OF THE CONTRACTOR ONLY. THE CORRECTNESS ANY COMPLETENESS OF THIS INFORMATION IS NOT GUARANTEED.

THE CONTRACTOR SHALL VERIFY THE INFORMATION AND TAKE ALL PRECAUTIONS TO FULLY PROTECT THE UTILITY AND SERVICE.

IT IS THE CONTRACTOR'S RESPONSIBILITY TO IDENTIFY ALL OVERHEAD LINES, NOTIFY THE UTILITY COMPANY AND COMPLY WITH THE APPLICABLE FACILITY OWNER'S SAFETY CLEARANCE REQUIREMENTS WHEN WORKING IN THE AREA OF THEIR FACILITIES/UTILITIES.

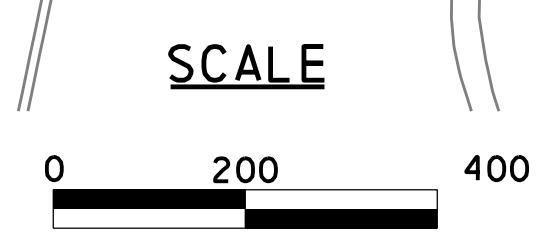
TITLE SHEET,  
INDEX MAP AND GENERAL NOTES

RECOMMENDED \_\_\_\_\_ 20 \_\_\_\_\_

\_\_\_\_\_

DISTRICT TRAFFIC ENGINEER

**30% DESIGN  
SUBMISSION**



4008008B.ELECT1.01

DES: DWH    DWG: BJB    CKD: KAC

# CONSTRUCTION NOTES

- 1 INSTALL NEW CONDUIT IN CONDUIT SLEEVE.
- 2 INSTALL CONDUIT SLEEVE UNDER ROADWAY USING BORING METHOD.
- 3 INSTALL ELECTRICAL AND COMMUNICATIONS CONDUIT IN UNPAVED AREA.
- 4 INSTALL ELECTRICAL SERVICE.
- 5 120/240 UTILITY SERVICE.
- 6 PROPOSED AT&T COMMUNICATION SERVICE.

# CONDUIT

- A 2" DIRECT BURIAL CONDUIT HDPE
- B 6" CONDUIT SLEEVE HDPE

# SYMBOL LEGEND

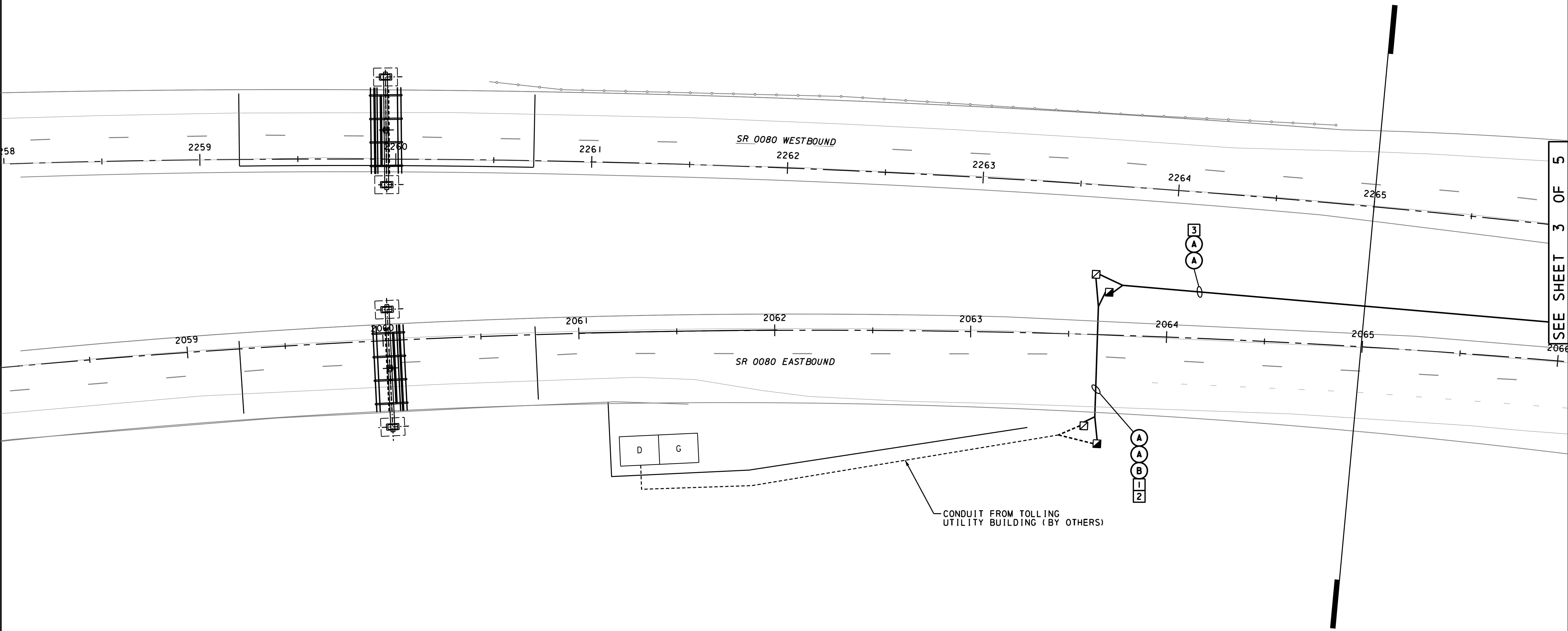
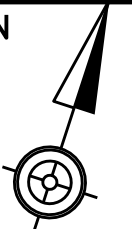
- ELECTRICAL/FIBER CONDUIT (PROPOSED)
- - - ELECTRICAL/FIBER CONDUIT (BY OTHERS)
- ☐ JB-1 (PROPOSED)
- JB-2 (PROPOSED)
- ⓪ POWER SUPPLY (PROPOSED)
- UTILITY POLE (PROPOSED)

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
4-0	LUZERNE	0080	08B	2 OF 5
5-0	CARBON	0080	08B	

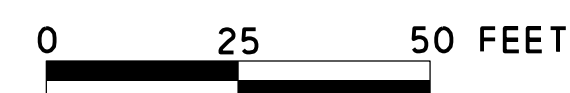
REVISION NUMBER	REVISIONS	DATE	BY

\*\* BOROUGH OF WHITE HAVEN  
BOROUGH OF EAST SIDE  
TOWNSHIP OF KIDDER



SEE SHEET 3 OF 5

### SCALE



ELECTRICAL AND COMMUNICATIONS  
SERVICE PLAN

**30% DESIGN  
SUBMISSION**

# CONSTRUCTION NOTES

- 1 INSTALL NEW CONDUIT IN CONDUIT SLEEVE.
- 2 INSTALL CONDUIT SLEEVE UNDER ROADWAY USING BORING METHOD.
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# SYMBOL LEGEND

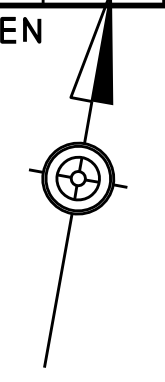
- ELECTRICAL/FIBER CONDUIT (PROPOSED)
- ELECTRICAL/FIBER CONDUIT (BY OTHERS)
- ☐ JB-1 (PROPOSED)
- JB-2 (PROPOSED)
- ⓪ POWER SUPPLY (PROPOSED)
- ⦿ UTILITY POLE (PROPOSED)

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
4-0	LUZERNE	0080	08B	3 OF 5
5-0	CARBON	0080	08B	

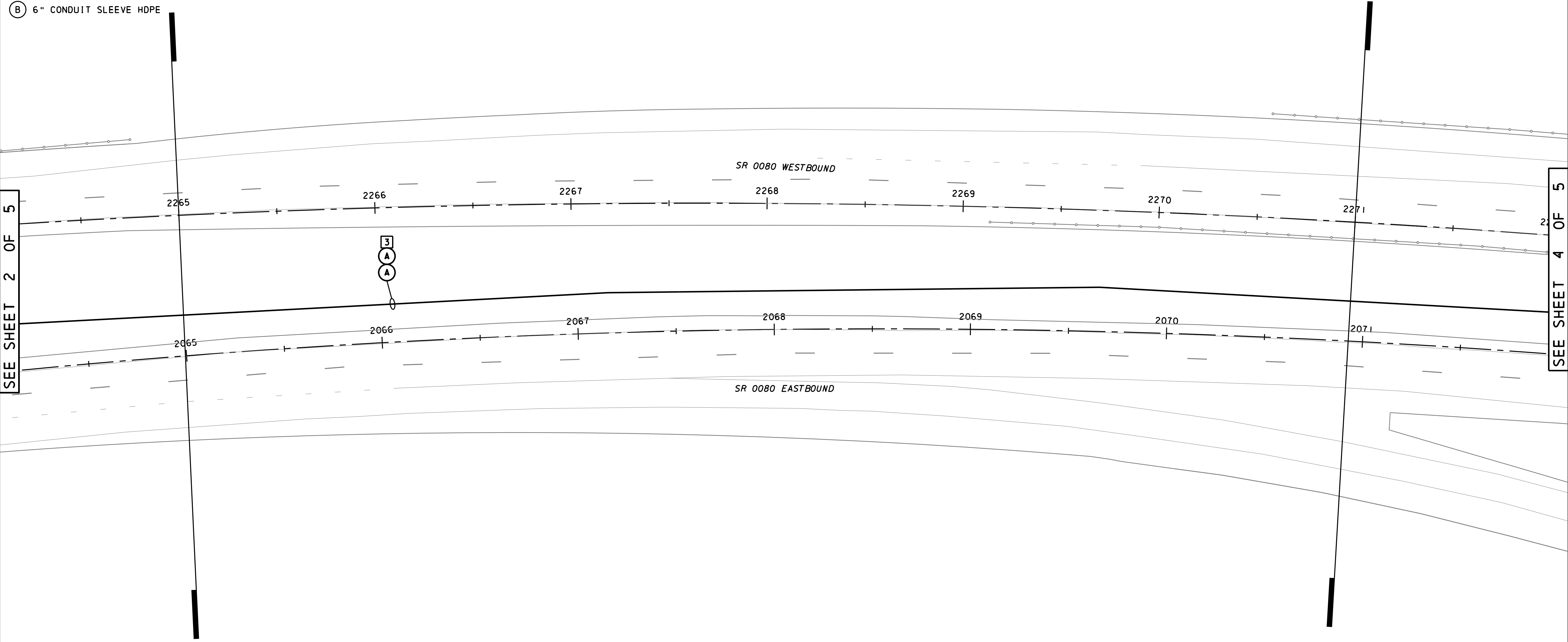
REVISION NUMBER	REVISIONS	DATE	BY

\*\* BOROUGH OF WHITE HAVEN  
BOROUGH OF EAST SIDE  
TOWNSHIP OF KIDDER



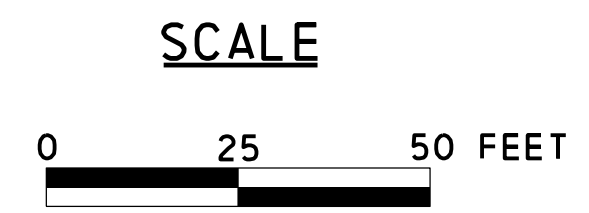
# CONDUIT

- A 2" DIRECT BURIAL CONDUIT HDPE
- B 6" CONDUIT SLEEVE HDPE



SEE SHEET 2 OF 5

SEE SHEET 4 OF 5



ELECTRICAL AND COMMUNICATIONS  
SERVICE PLAN

**30% DESIGN  
SUBMISSION**

DES: DWH	DWG: BJG	CKD: KAC
----------	----------	----------

4/28/2021

PLOTTED:

4008008B.ELECP1.02

# CONSTRUCTION NOTES

- 1 INSTALL NEW CONDUIT IN CONDUIT SLEEVE.
- 2 INSTALL CONDUIT SLEEVE UNDER ROADWAY USING BORING METHOD.
- 3 INSTALL ELECTRICAL AND COMMUNICATIONS CONDUIT IN UNPAVED AREA.
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# CONDUIT

- A 2" DIRECT BURIAL CONDUIT HDPE
- B 6" CONDUIT SLEEVE HDPE

# SYMBOL LEGEND

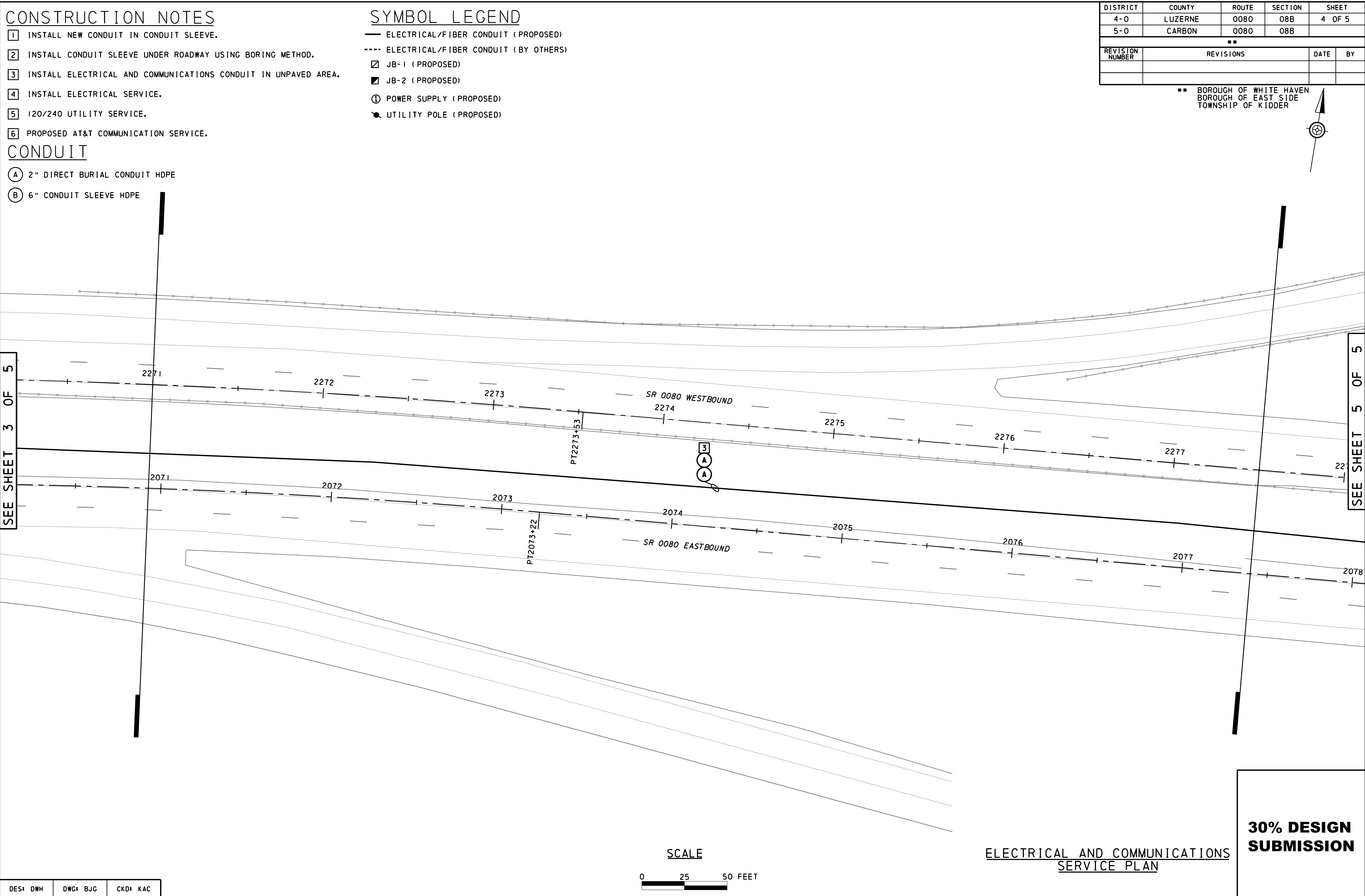
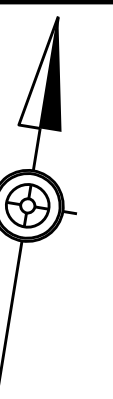
- ELECTRICAL/FIBER CONDUIT (PROPOSED)
- - - ELECTRICAL/FIBER CONDUIT (BY OTHERS)
- ☐ JB-1 (PROPOSED)
- ▣ JB-2 (PROPOSED)
- ⓐ POWER SUPPLY (PROPOSED)
- UTILITY POLE (PROPOSED)

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
4-0	LUZERNE	0080	08B	4 OF 5
5-0	CARBON	0080	08B	

REVISION NUMBER	REVISIONS	DATE	BY

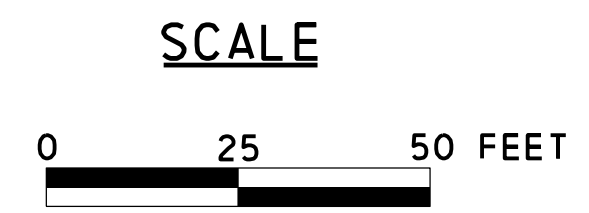
\*\* BOROUGH OF WHITE HAVEN  
BOROUGH OF EAST SIDE  
TOWNSHIP OF KIDDER



SEE SHEET 3 OF 5

SEE SHEET 5 OF 5

DES: DWH    DWG: BJG    CKD: KAC



ELECTRICAL AND COMMUNICATIONS  
SERVICE PLAN

**30% DESIGN  
SUBMISSION**

4/28/2021

PLOTTED:

4008008B.ELECP.1.03



# CONSTRUCTION NOTES

- 1 INSTALL NEW CONDUIT IN CONDUIT SLEEVE.
- 2 INSTALL CONDUIT SLEEVE UNDER ROADWAY USING BORING METHOD.
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- 6 PROPOSED AT&T COMMUNICATION SERVICE.

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- B 6" CONDUIT SLEEVE HDPE

# SYMBOL LEGEND

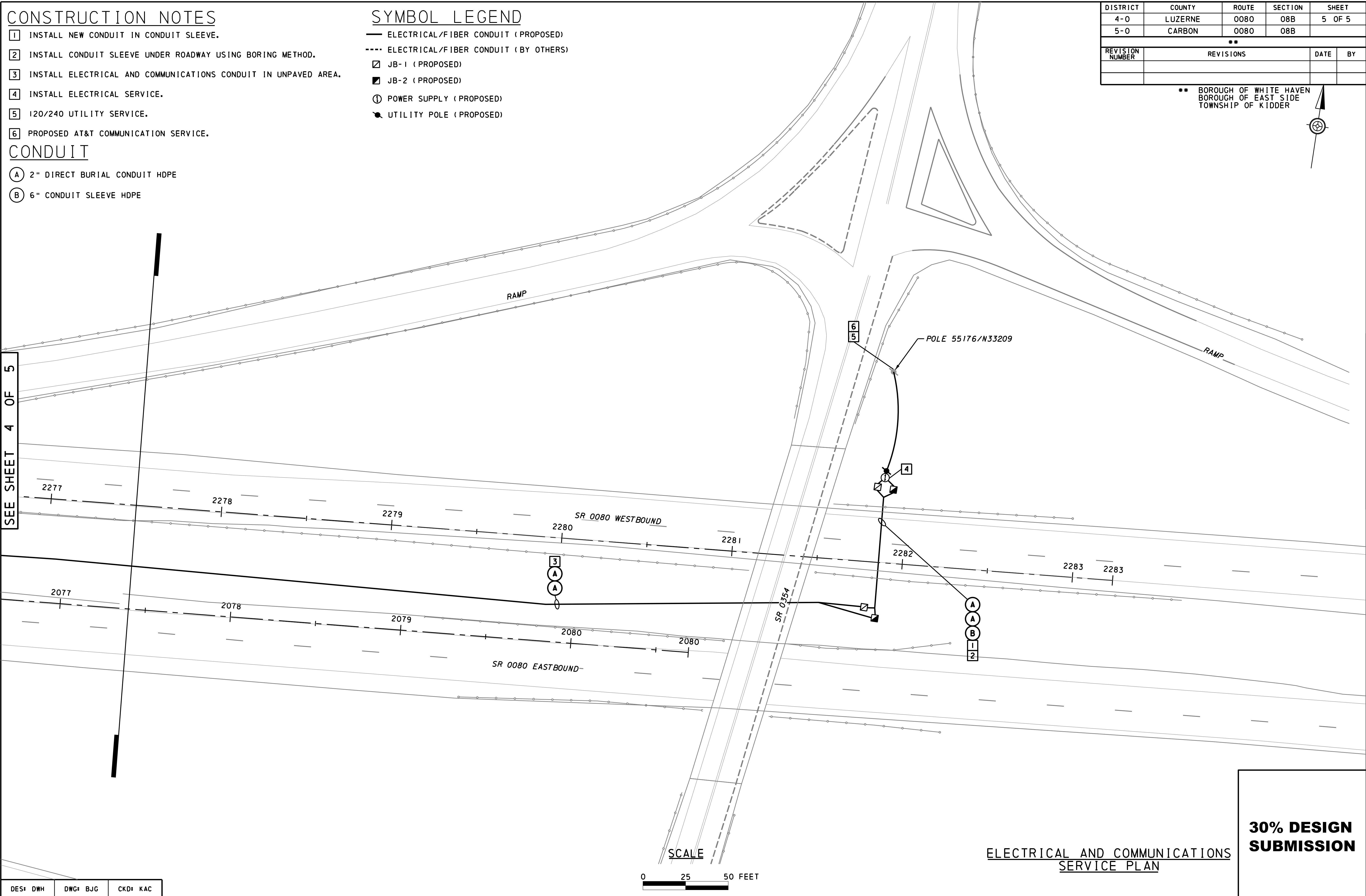
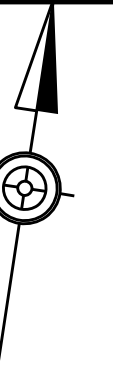
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- - - ELECTRICAL/FIBER CONDUIT (BY OTHERS)
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- JB-2 (PROPOSED)
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- UTILITY POLE (PROPOSED)

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
4-0	LUZERNE	0080	08B	5 OF 5
5-0	CARBON	0080	08B	

REVISION NUMBER	REVISIONS	DATE	BY

\*\* BOROUGH OF WHITE HAVEN  
BOROUGH OF EAST SIDE  
TOWNSHIP OF KIDDER



SEE SHEET 4 OF 5

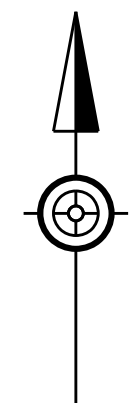


ELECTRICAL AND COMMUNICATIONS  
SERVICE PLAN

**30% DESIGN  
SUBMISSION**

4/28/2021  
PLOTTED:

# SIGNING AND PAVEMENT MARKING

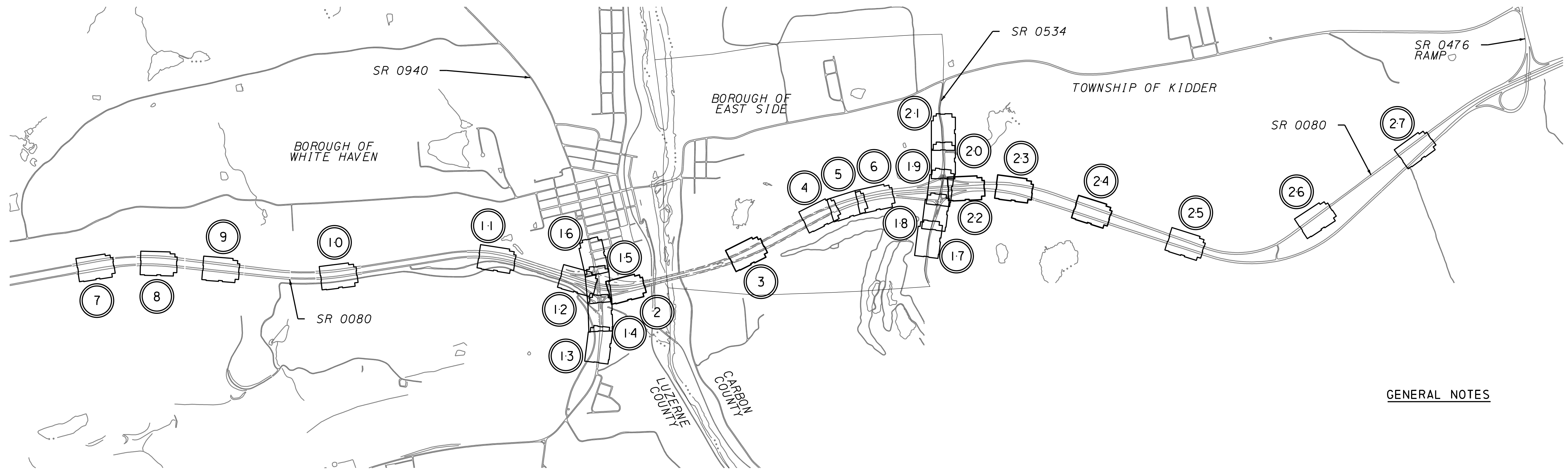


DISTRICT	COUNTY	ROUTE	SECTION	SHEET
4-0	LUZERNE	0080	08B	1 OF 27
5-0	CARBON	0080	08B	

REVISION NUMBER	REVISIONS	DATE	BY

\*\* BOROUGH OF WHITE HAVEN  
BOROUGH OF EAST SIDE  
TOWNSHIP OF KIDDER



Toll Facility Plans are being updated. Decision has been made to toll in eastbound direction only. The P3 Development Entity will construct tolling gantry over eastbound lanes only.

GENERAL NOTES

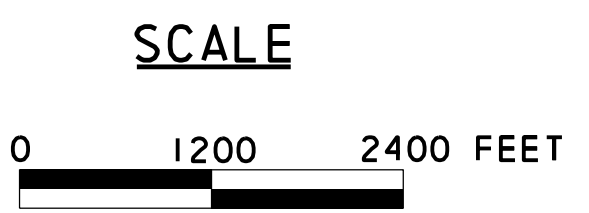
UTILITY NOTES

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SHEET INDEX	
DESCRIPTION	SHEET(S)
TITLE SHEET, INDEX MAP AND GENERAL NOTES	1
SIGNING AND PAVEMENT MARKING PLANS	2-6
SIGNING AND PAVEMENT MARKING PLANS ADVANCE SIGNING EASTBOUND	7-16
SIGNING AND PAVEMENT MARKING PLANS ADVANCE SIGNING WESTBOUND	17-27



TITLE SHEET,  
INDEX MAP AND GENERAL NOTES

RECOMMENDED \_\_\_\_\_ 20 \_\_\_\_\_

\_\_\_\_\_

DISTRICT TRAFFIC ENGINEER

**30% DESIGN SUBMISSION**

Signing and Pavement Marking Plan

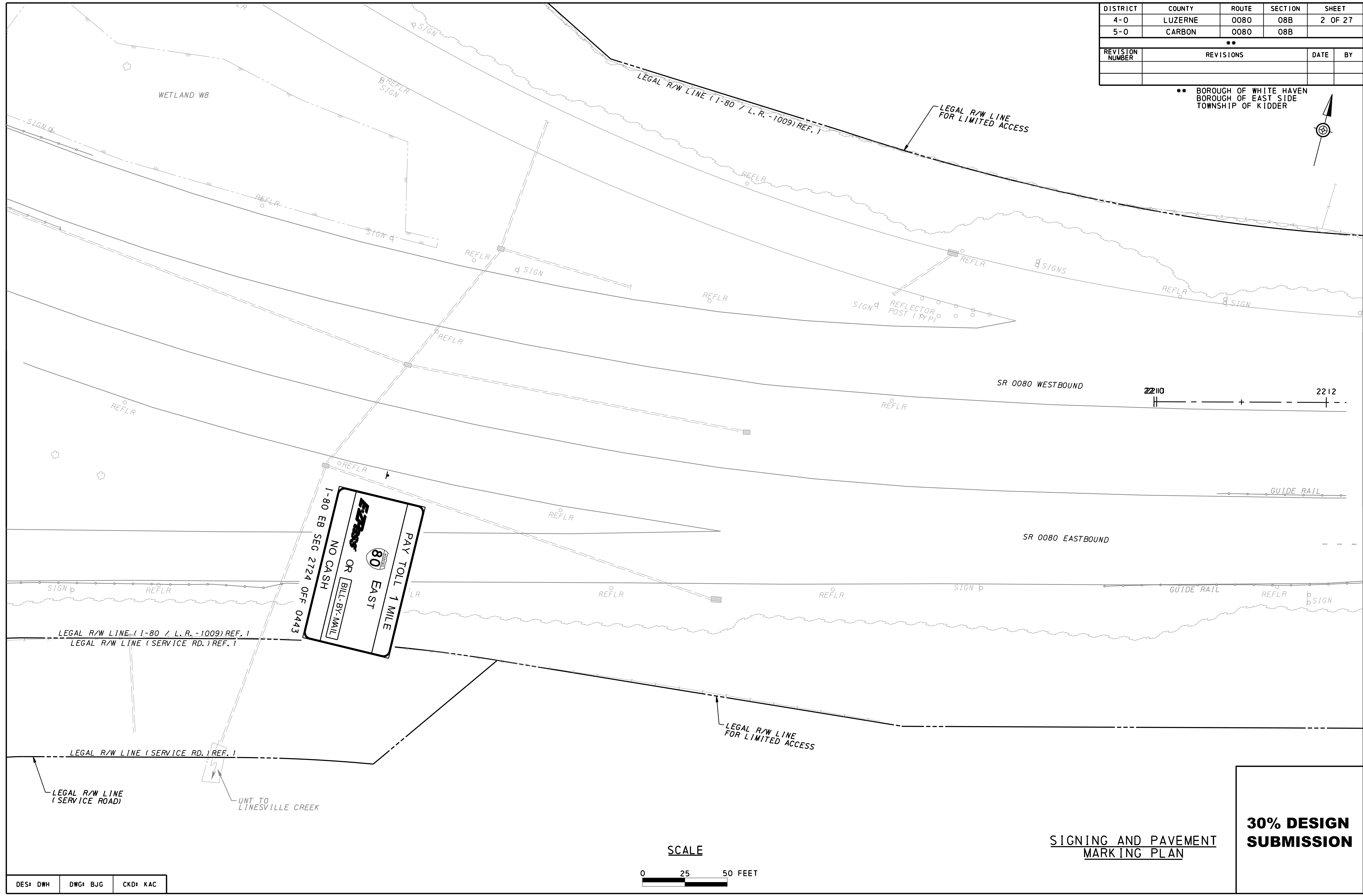
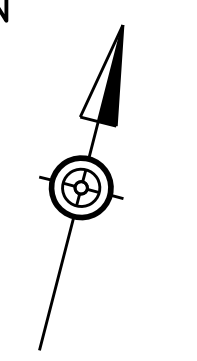
4/28/2021  
PLOTTED:

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
4-0	LUZERNE	0080	08B	2 OF 27
5-0	CARBON	0080	08B	

REVISION NUMBER	REVISIONS	DATE	BY

\*\* BOROUGH OF WHITE HAVEN  
BOROUGH OF EAST SIDE  
TOWNSHIP OF KIDDER



**SIGNING AND PAVEMENT  
MARKING PLAN**

**30% DESIGN  
SUBMISSION**

DES: DWH    DWG: BJG    CKD: KAC

Signing and Pavement Marking Plan

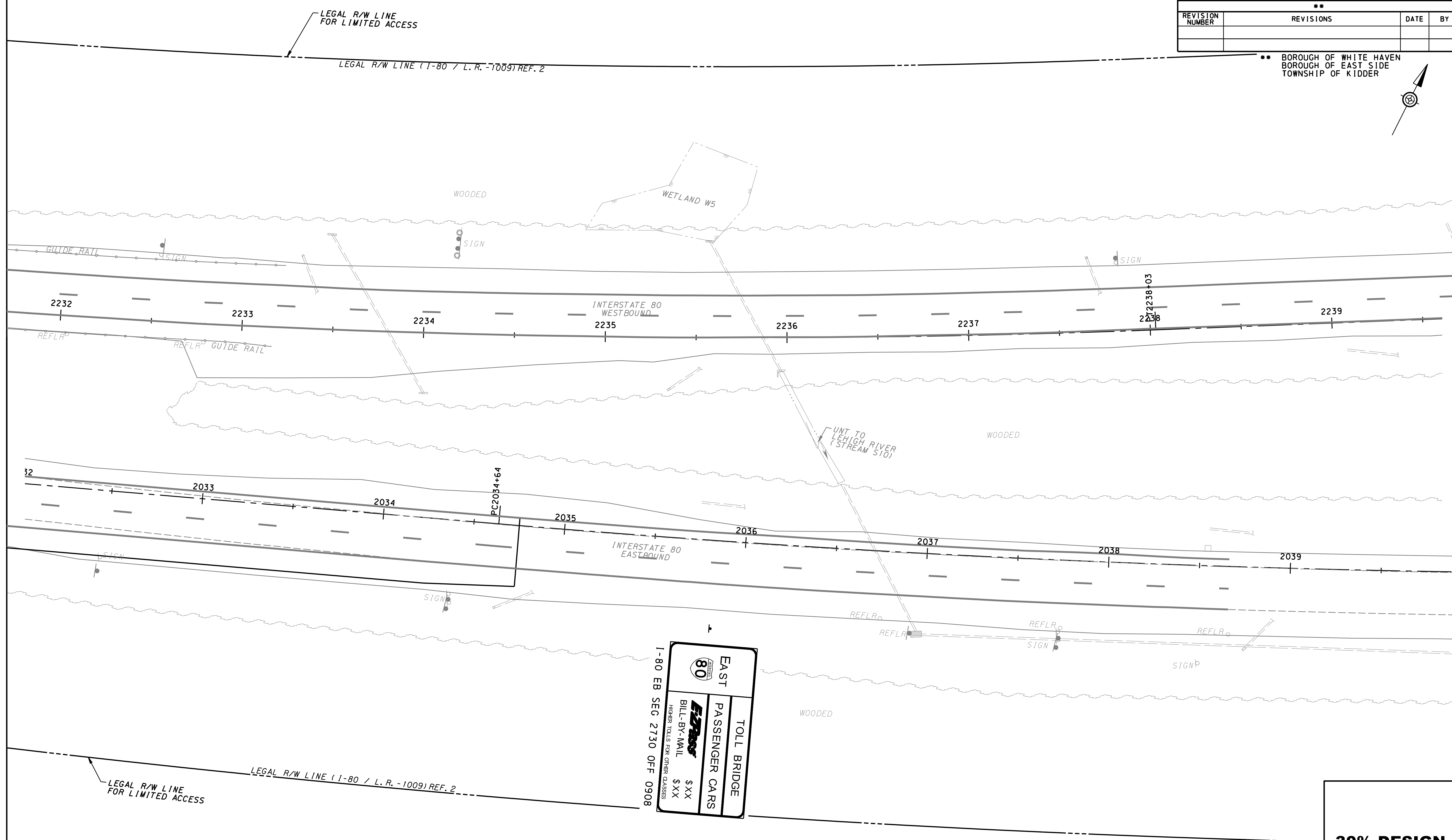
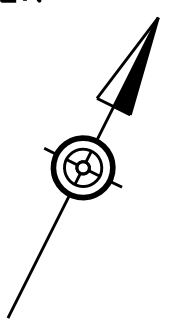
4/28/2021  
PLOTTED:

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
4-0	LUZERNE	0080	08B	3 OF 27
5-0	CARBON	0080	08B	

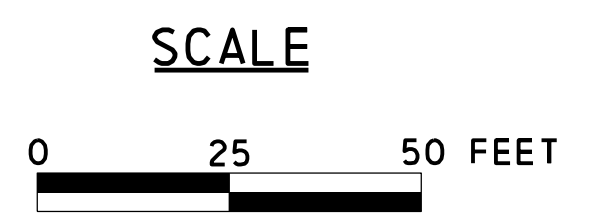
REVISION NUMBER	REVISIONS	DATE	BY

\*\* BOROUGH OF WHITE HAVEN  
BOROUGH OF EAST SIDE  
TOWNSHIP OF KIDDER



I-80 EB SEG 2730 OFF 0908

	EAST	TOLL BRIDGE
	PASSENGER CARS	
	\$XX	
BILL-BY-MAIL	\$XX	
HIGHER TOLLS FOR OTHER CLASSES		



**SIGNING AND PAVEMENT MARKING PLAN**

**30% DESIGN SUBMISSION**

Signing and Pavement Marking Plan

DES: DWH	DWG: BJC	CKD: KAC
----------	----------	----------

4/28/2021

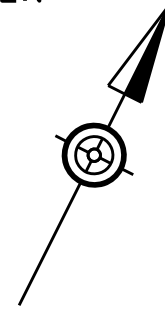
Signing and Pavement Marking Plan

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
4-0	LUZERNE	0080	08B	4 OF 27
5-0	CARBON	0080	08B	

REVISION NUMBER	REVISIONS	DATE	BY

\*\* BOROUGH OF WHITE HAVEN  
BOROUGH OF EAST SIDE  
TOWNSHIP OF KIDDER



**EXIT 273**

**White Haven  
Freeland**

**3/4 MILE**

**437**

**440**

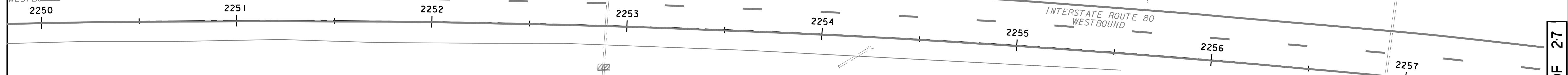
I-80 WB SEG 2731 OFF 2571  
RELOCATE EXISTING  
"1 MILE" SIGN  
FROM SEG 2735 OFF 0628  
ADD "3/4 MILE" PLAQUE  
TO EXISTING SIGN

LEGAL RIGHT-OF-WAY LINE

WOODED

STATE ROUTE 80  
WESTBOUND

INTERSTATE ROUTE 80  
WESTBOUND

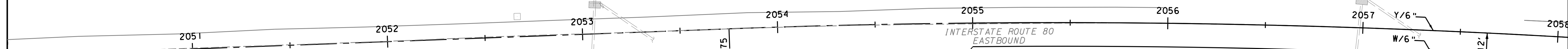


WOODED

WOODED

I-80 EB  
SEG 2734  
OFF 0160  
R4-9  
48"X60"

**STAY  
IN  
LANE**



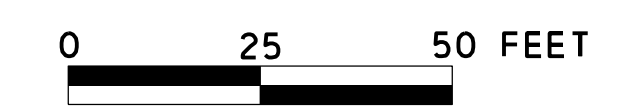
MATCH EXISTING PAV'T MARKINGS  
BEGIN W/6", Y/6"  
STA 2055+00.00  
SR 0080 EB

I-80 EB  
SEG 2734  
OFF 0160  
R4-9  
48"X60"

**STAY  
IN  
LANE**

LEGAL RIGHT-OF-WAY LINE

SCALE



**SIGNING AND PAVEMENT  
MARKING PLAN**

**30% DESIGN  
SUBMISSION**

DES: DWH	DWG: BJG	CKD: KAC
----------	----------	----------

SEE SHEET 5 OF 27

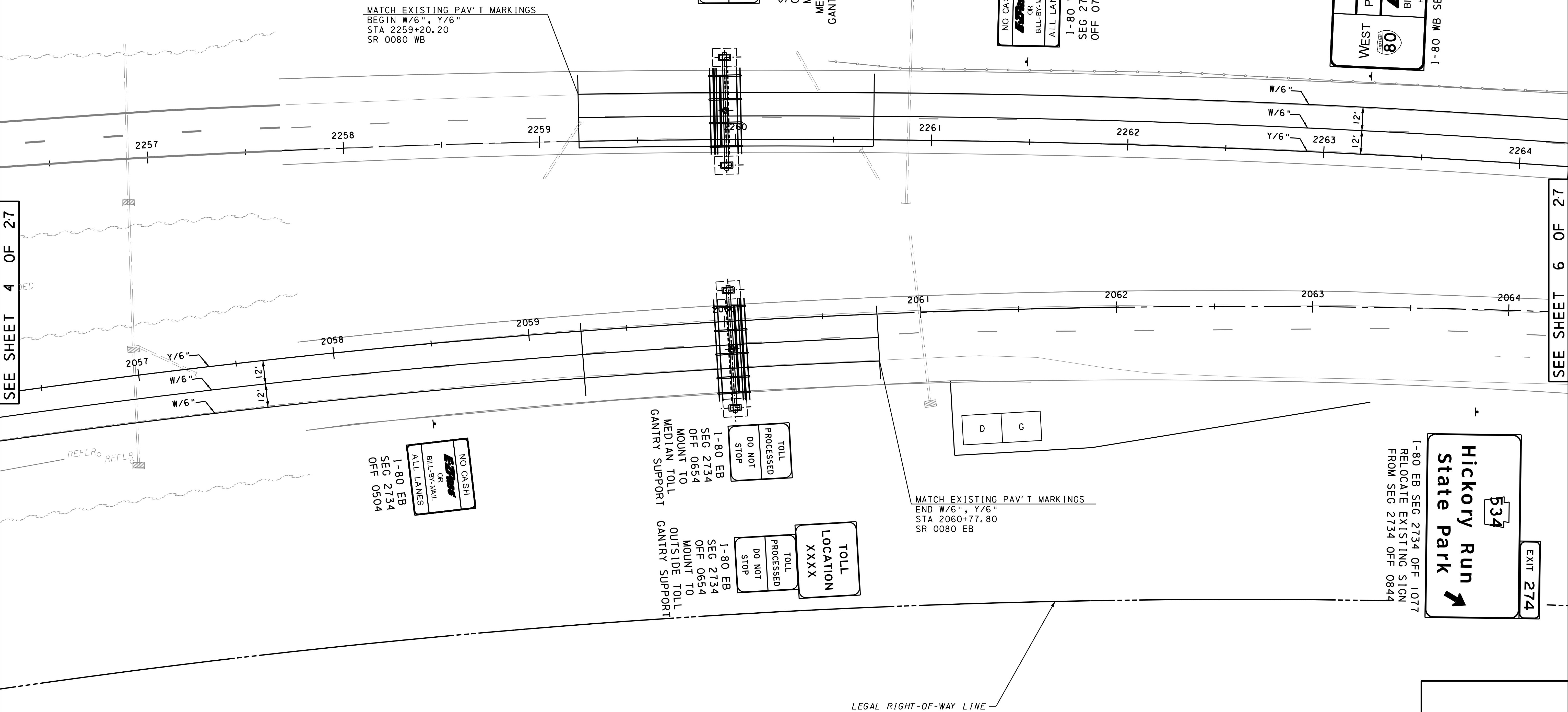
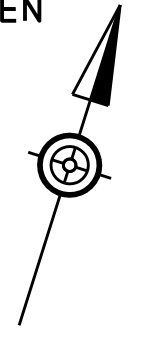
BOOK # 10  
REC. DATE: OCTOBER  
INST# \*\*\*  
AREA = 9.45 AC. (D)

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
4-0	LUZERNE	0080	08B	5 OF 27
5-0	CARBON	0080	08B	

REVISION NUMBER	REVISIONS	DATE	BY

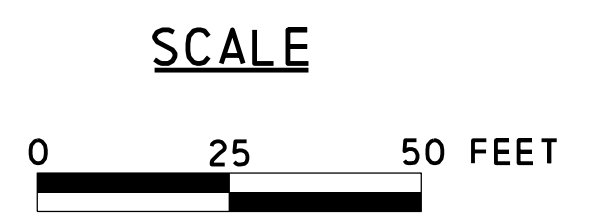
**TOLL BRIDGE**  
**PASSENGER**  
**WEST**  
**80**  
 BOROUGH OF WHITE HAVEN  
 BOROUGH OF EAST SIDE  
 TOWNSHIP OF KIDDER  
 BILL-BY-MAIL  
 HIGHER TOLLS FOR OTHER  
 1-80 WB SEC 2735 OFF



SEE SHEET 4 OF 27

SEE SHEET 6 OF 27

DES: DWH    DWG: BJC    CKD: KAC



**SIGNING AND PAVEMENT MARKING PLAN**

**EXIT 274**  
**534**  
**Hickory Run**  
**State Park**  
 1-80 EB SEC 2734 OFF 1077  
 RELOCATE EXISTING SIGN  
 FROM SEC 2734 OFF 0844

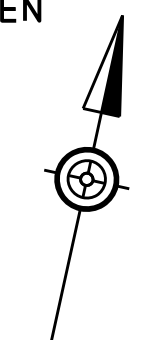
**30% DESIGN SUBMISSION**

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
4-0	LUZERNE	0080	08B	6 OF 27
5-0	CARBON	0080	08B	

REVISION NUMBER	REVISIONS	DATE	BY

\*\* BOROUGH OF WHITE HAVEN  
BOROUGH OF EAST SIDE  
TOWNSHIP OF KIDDER



TOLL BRIDGE  
PASSENGER CARS \$XX  
BILL-BY-MAIL \$XX  
HIGHER TOLLS FOR OTHER CLASSES

WEST  
80

1-80 WB SEG 2735 OFF 0935

STAY IN LANE

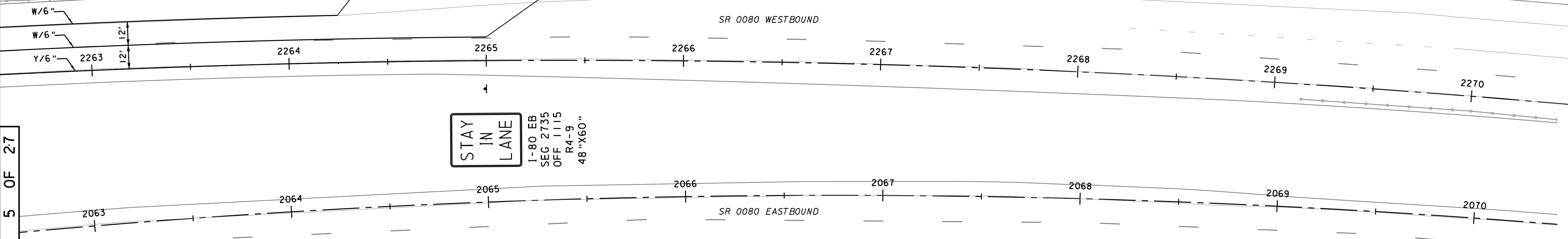
MATCH EXISTING PAV'T MARKINGS  
END W/6"  
STA 2264+25.00  
SR 0080 WB

1-80 EB  
SEG 2735  
OFF 1115  
R4-9  
48"X60"

MATCH EXISTING PAV'T MARKINGS  
END W/6", Y/6"  
STA 2265+00.00  
SR 0080 WB

STAY IN LANE

1-80 EB  
SEG 2735  
OFF 1115  
R4-9  
48"X60"

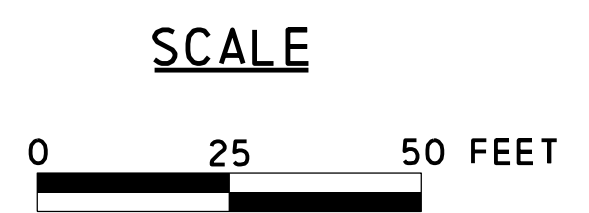


534  
EXIT 274

Hickory Run  
State Park

1-80 EB SEG 2734 OFF 1077  
RELOCATE EXISTING SIGN  
FROM SEG 2734 OFF 0844

DES: DWH	DWG: BJC	CKD: KAC
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SIGNING AND PAVEMENT  
MARKING PLAN

**30% DESIGN  
SUBMISSION**

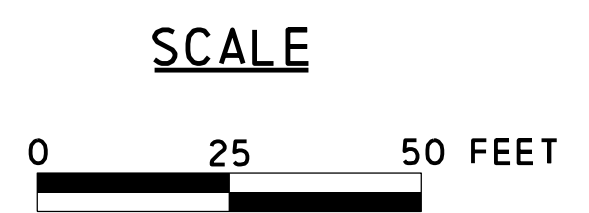
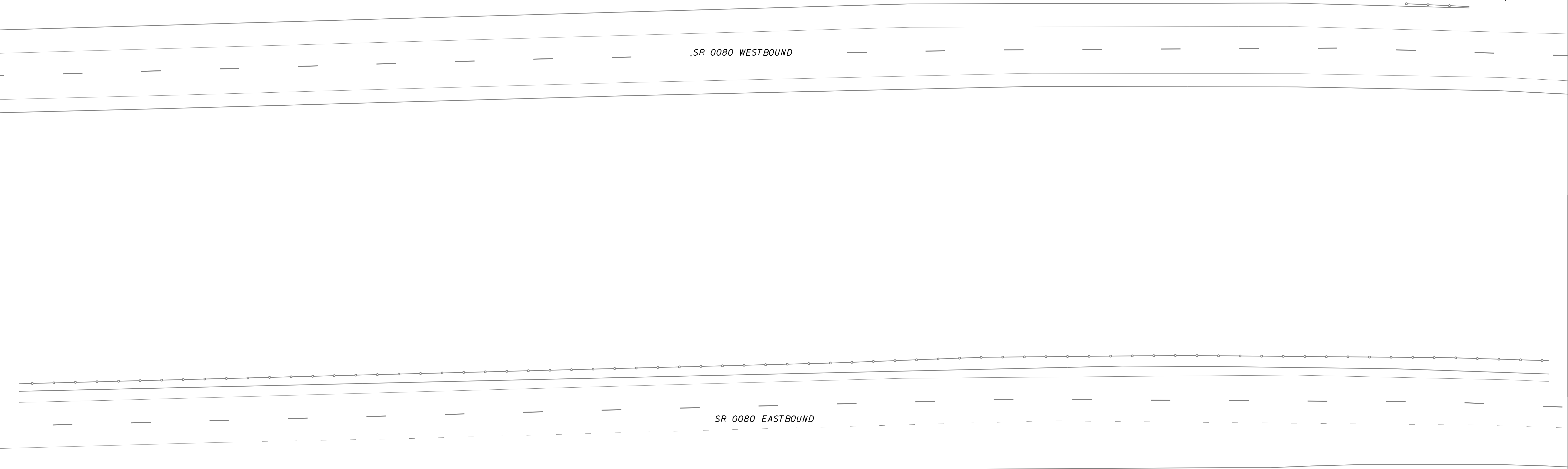
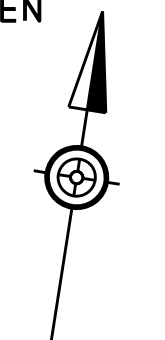
4/28/2021  
PLOTTED:

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
4-0	LUZERNE	0080	08B	7 OF 27
5-0	CARBON	0080	08B	

**		DATE	BY
REVISION NUMBER	REVISIONS		

\*\* BOROUGH OF WHITE HAVEN  
BOROUGH OF EAST SIDE  
TOWNSHIP OF KIDDER



SIGNING AND PAVEMENT  
MARKING PLAN  
ADVANCE SIGNING  
EASTBOUND

**30% DESIGN  
SUBMISSION**

DES: DWH    DWG: BJG    CKD: KAC

Signing and Pavement Marking Plan



4/28/2021  
PLOTTED:

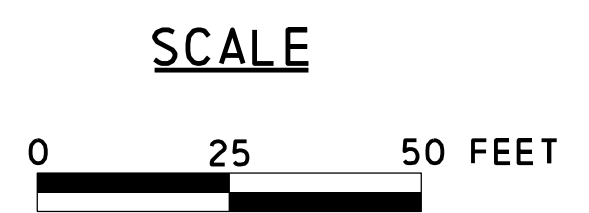
DISTRICT	COUNTY	ROUTE	SECTION	SHEET
4-0	LUZERNE	0080	08B	8 OF 27
5-0	CARBON	0080	08B	
**				
REVISION NUMBER	REVISIONS	DATE	BY	

\*\* BOROUGH OF WHITE HAVEN  
BOROUGH OF EAST SIDE  
TOWNSHIP OF KIDDER



1-80 EB SEG 2704 OFF 0506

E-Z-Road	OR	BILL-BY-MAIL
NO CASH		
PAY TOLL 3 MILES	80	EAST



SIGNING AND PAVEMENT  
MARKING PLAN  
ADVANCE SIGNING  
EASTBOUND

**30% DESIGN  
SUBMISSION**

DES: DWH    DWG: BJC    CKD: KAC

Signing and Pavement Marking Plan

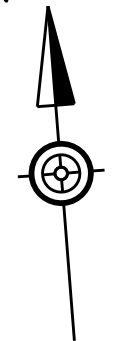
4/28/2021  
PLOTTED:

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
4-0	LUZERNE	0080	08B	9 OF 27
5-0	CARBON	0080	08B	

**				
REVISION NUMBER	REVISIONS	DATE	BY	

\*\* BOROUGH OF WHITE HAVEN  
BOROUGH OF EAST SIDE  
TOWNSHIP OF KIDDER



SR 0080 WESTBOUND

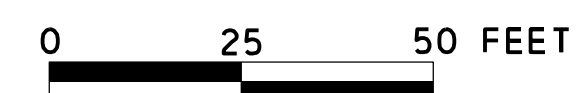
SR 0080 EASTBOUND

↑

<b>80</b> <small>FEET</small>	EAST PASSENGER CARS	TOLL BRIDGE
<b>E-ZPass</b>	BILL-BY-MAIL	\$ XX
<small>HIGHER TOLLS FOR OTHER CLASSES</small>	\$ XX	\$ XX

I-80 EB SEG 2704 OFF 1943

SCALE



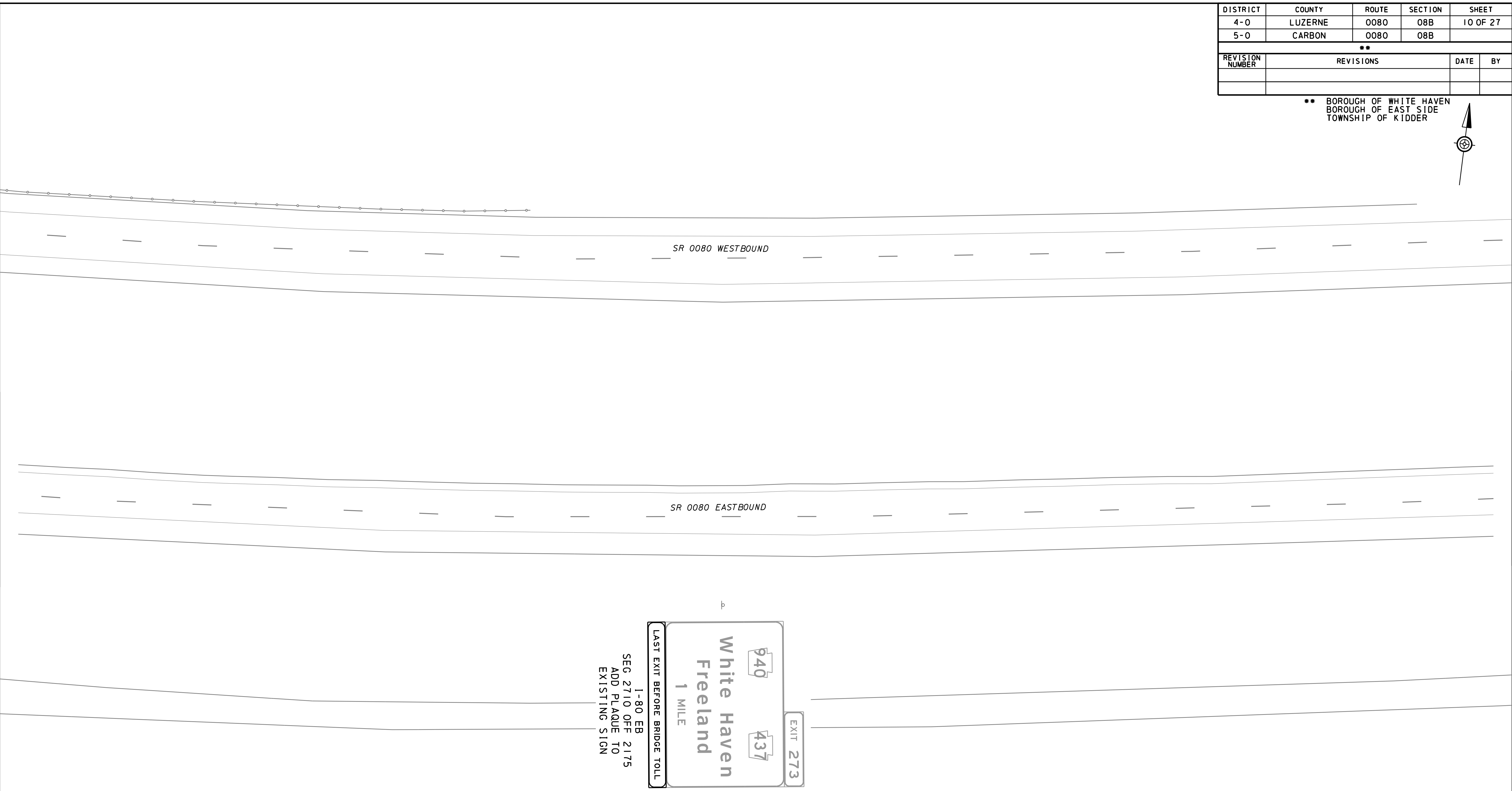
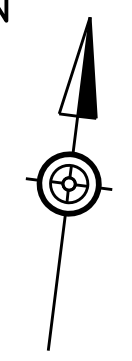
SIGNING AND PAVEMENT  
MARKING PLAN  
ADVANCE SIGNING  
EASTBOUND

**30% DESIGN  
SUBMISSION**

4/28/2021  
PLOTTED:

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
4-0	LUZERNE	0080	08B	10 OF 27
5-0	CARBON	0080	08B	
**				
REVISION NUMBER	REVISIONS	DATE	BY	

\*\* BOROUGH OF WHITE HAVEN  
BOROUGH OF EAST SIDE  
TOWNSHIP OF KIDDER



LAST EXIT BEFORE BRIDGE TOLL

1-80 EB  
SEG 2710 OFF 2175  
ADD PLAQUE TO  
EXISTING SIGN

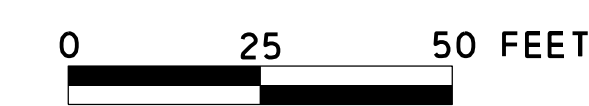
940

White Haven  
Freeland  
1 MILE

437

EXIT 273

SCALE



SIGNING AND PAVEMENT  
MARKING PLAN  
ADVANCE SIGNING  
EASTBOUND

**30% DESIGN  
SUBMISSION**

DES: DWH	DWG: BJG	CKD: KAC
----------	----------	----------

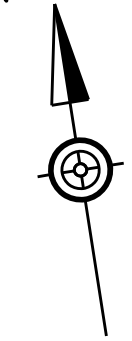
4/28/2021  
PLOTTED:

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
4-0	LUZERNE	0080	08B	11 OF 27
5-0	CARBON	0080	08B	

**				
REVISION NUMBER	REVISIONS	DATE	BY	

\*\* BOROUGH OF WHITE HAVEN  
BOROUGH OF EAST SIDE  
TOWNSHIP OF KIDDER



LEGAL RIGHT-OF-WAY LINE

SR 0080 WESTBOUND

SR 0080 EASTBOUND

LEGAL RIGHT-OF-WAY LINE

LEGAL RIGHT-OF-WAY LINE

REFLR

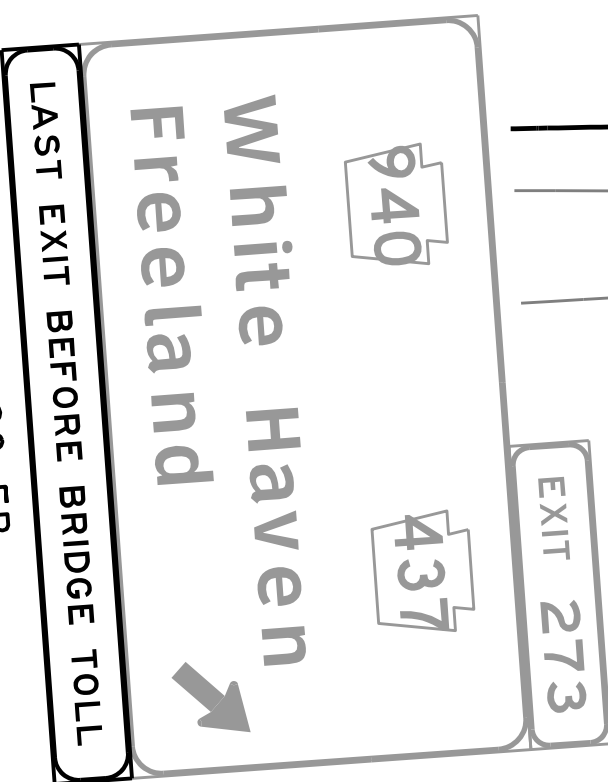
LEGAL RIGHT-OF-WAY LINE

LEGAL RIGHT-OF-WAY LINE

LEGAL RIGHT-OF-WAY LINE

LEGAL R/W LINE (SERVICE)

1-80 EB  
SEG 2720 OFF 0422  
ADD PLAQUE TO  
EXISTING SIGN



SCALE

0 25 50 FEET

SIGNING AND PAVEMENT  
MARKING PLAN  
ADVANCE SIGNING  
EASTBOUND

**30% DESIGN  
SUBMISSION**

DES: DWH    DWG: BJG    CKD: KAC

Signing and Pavement Marking Plan

4/28/2021 PLOTTED:

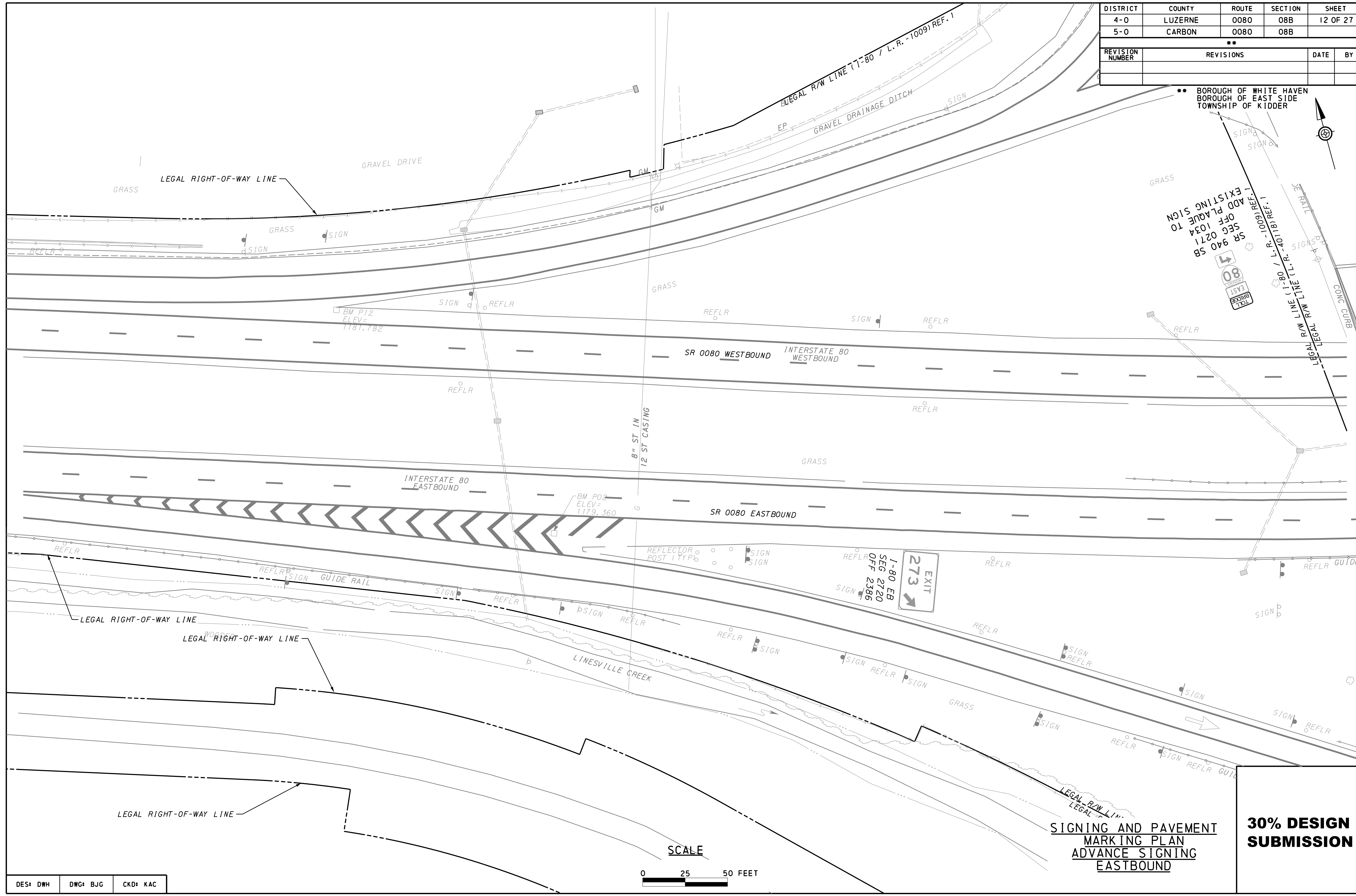
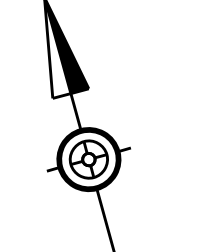
Signing and Pavement Marking Plan

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
4-0	LUZERNE	0080	08B	12 OF 27
5-0	CARBON	0080	08B	

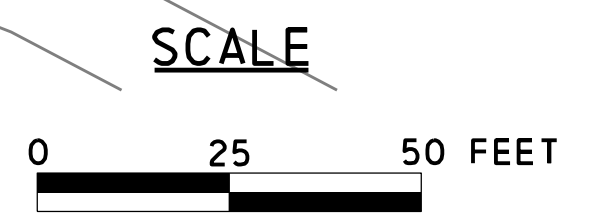
REVISION NUMBER	REVISIONS	DATE	BY
	**		

\*\* BOROUGH OF WHITE HAVEN  
BOROUGH OF EAST SIDE  
TOWNSHIP OF KIDDER



SR 940 SB  
SEG 0271  
OFF 1034  
ADD PLaque TO  
EXISTING SIGN

EXIT  
273  
I-80 EB  
SEG 2720  
OFF 2386



SIGNING AND PAVEMENT  
MARKING PLAN  
ADVANCE SIGNING  
EASTBOUND

**30% DESIGN  
SUBMISSION**

DES: DWH    DWG: BJG    CKD: KAC

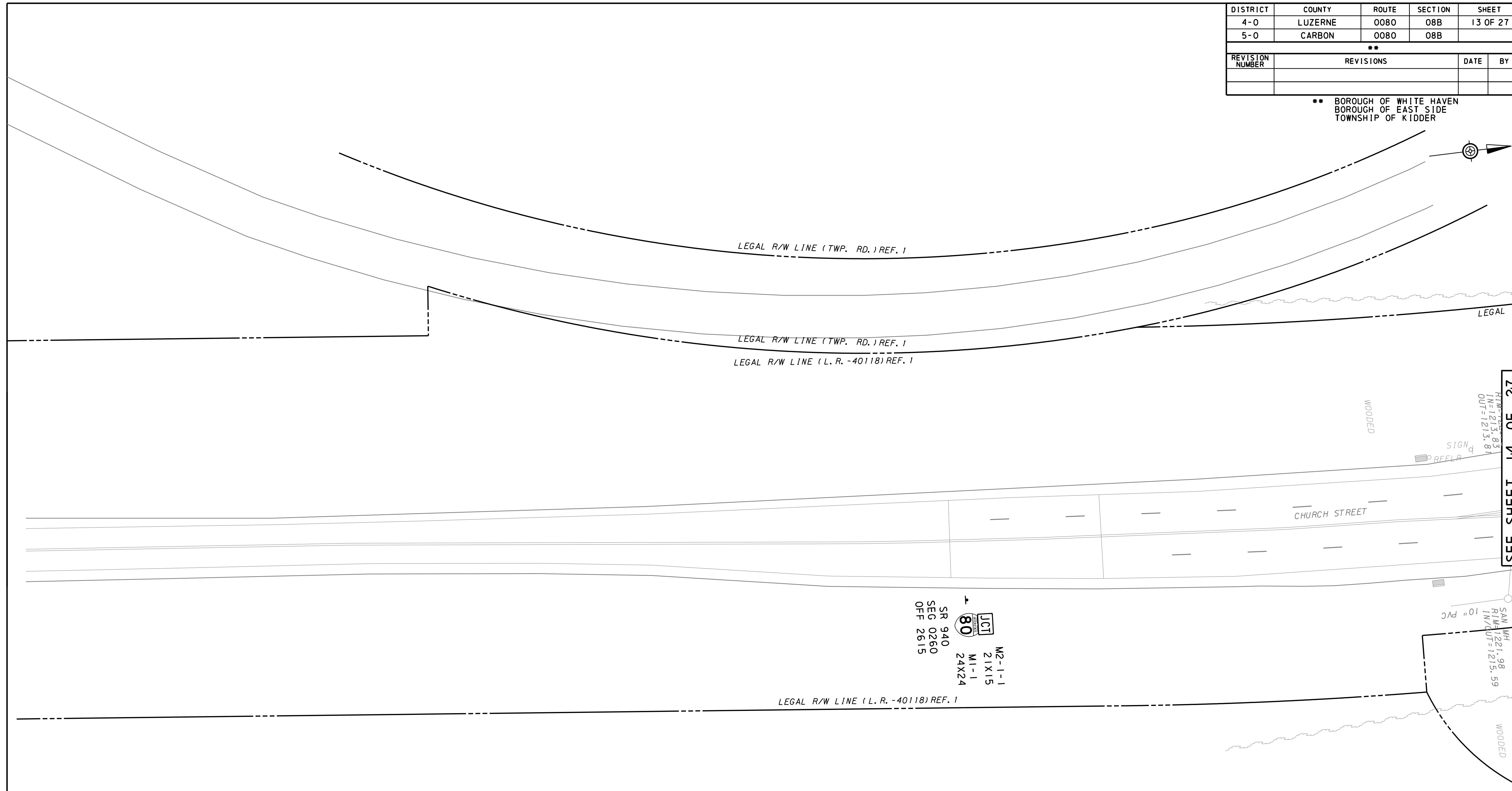
4/28/2021  
PLOTTED:

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
4-0	LUZERNE	0080	08B	13 OF 27
5-0	CARBON	0080	08B	

**				
REVISION NUMBER	REVISIONS	DATE	BY	

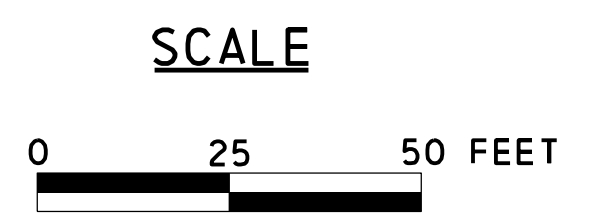
\*\* BOROUGH OF WHITE HAVEN  
BOROUGH OF EAST SIDE  
TOWNSHIP OF KIDDER



SEE SHEET 14 OF 27

Signing and Pavement Marking Plan

DES: DWH	DWG: BJG	CKD: KAC
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SIGNING AND PAVEMENT  
MARKING PLAN  
ADVANCE SIGNING  
EASTBOUND

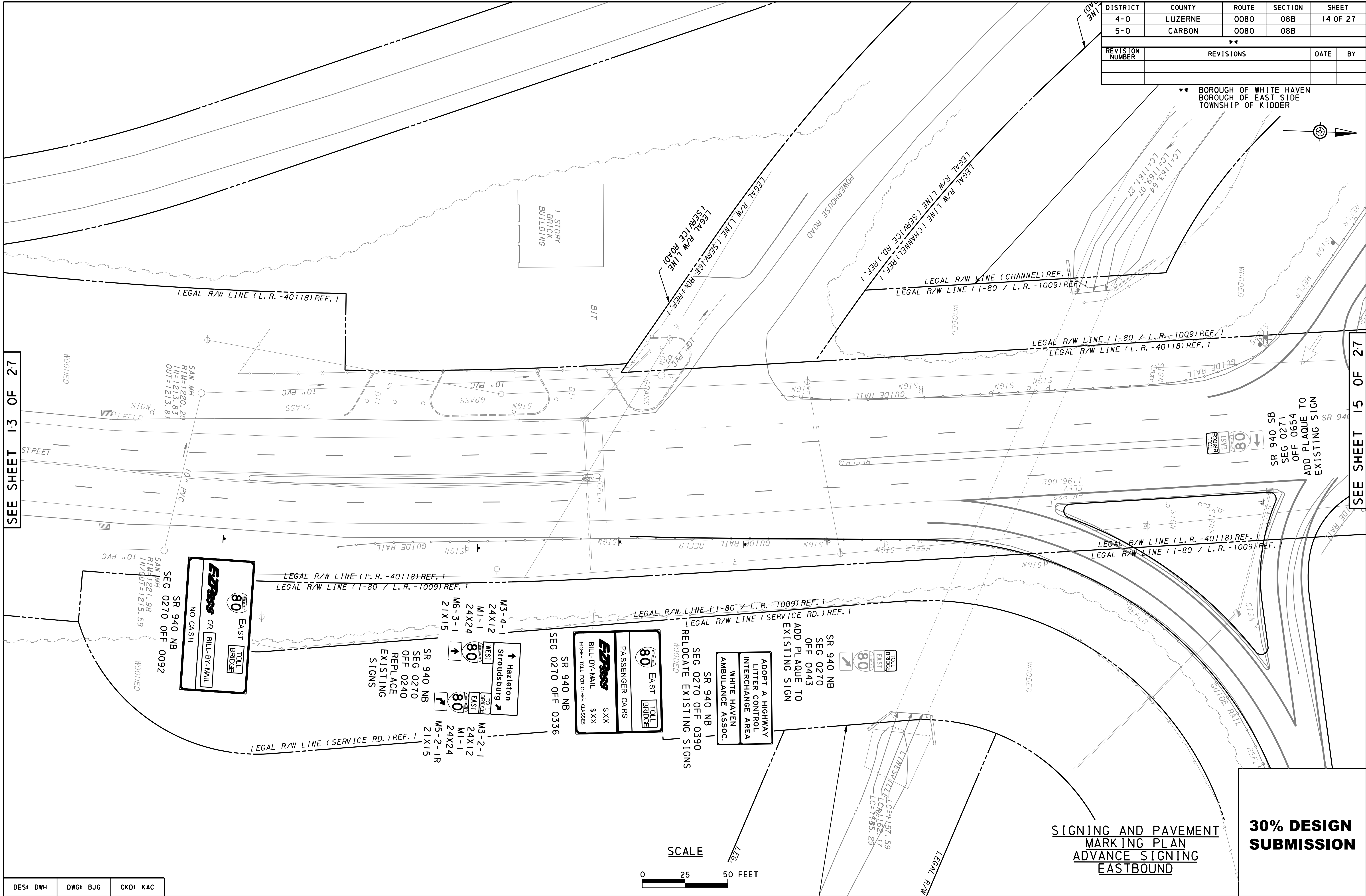
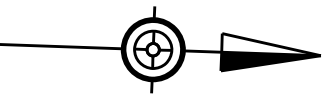
**30% DESIGN  
SUBMISSION**

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
4-0	LUZERNE	0080	08B	14 OF 27
5-0	CARBON	0080	08B	

REVISION NUMBER	REVISIONS	DATE	BY

\*\* BOROUGH OF WHITE HAVEN  
BOROUGH OF EAST SIDE  
TOWNSHIP OF KIDDER



SR 940 SB  
SEG 0271  
OFF 0654  
ADD PLAQUE TO  
EXISTING SIGN

SR 940 NB  
SEG 0270 OFF 0092

NO CASH

**80** EAST TOLL BRIDGE

**EZPass** OR BILL-BY-MAIL

SR 940 NB  
SEG 0270 OFF 0336

PASSENGER CARS  
BILL-BY-MAIL \$XX  
HIGHER TOLL FOR OTHER CLASSES

**80** EAST TOLL BRIDGE

SR 940 NB  
SEG 0270 OFF 0390

RELOCATE EXISTING SIGNS

ADOPT A HIGHWAY  
LITTER CONTROL  
INTERCHANGE AREA  
WHITE HAVEN  
AMBULANCE ASSOC

SR 940 NB  
SEG 0270 OFF 0240

REPLACE EXISTING SIGNS

M3-4-1  
24X12  
M1-1  
24X24  
M6-3-1  
21X15

WEST  
**80**  
WEST

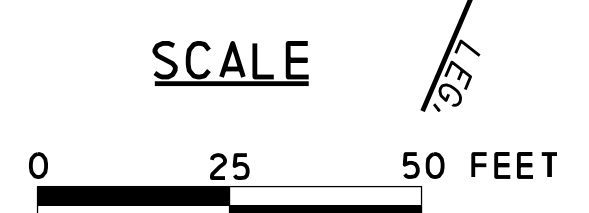
Hazleton  
Stroudsburg

M3-2-1  
24X12  
M1-1  
24X24  
M5-2-1R  
21X15

EAST  
**80**  
EAST

SIGNING AND PAVEMENT  
MARKING PLAN  
ADVANCE SIGNING  
EASTBOUND

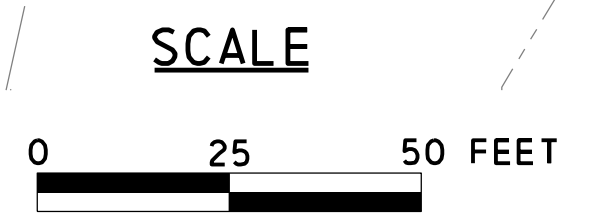
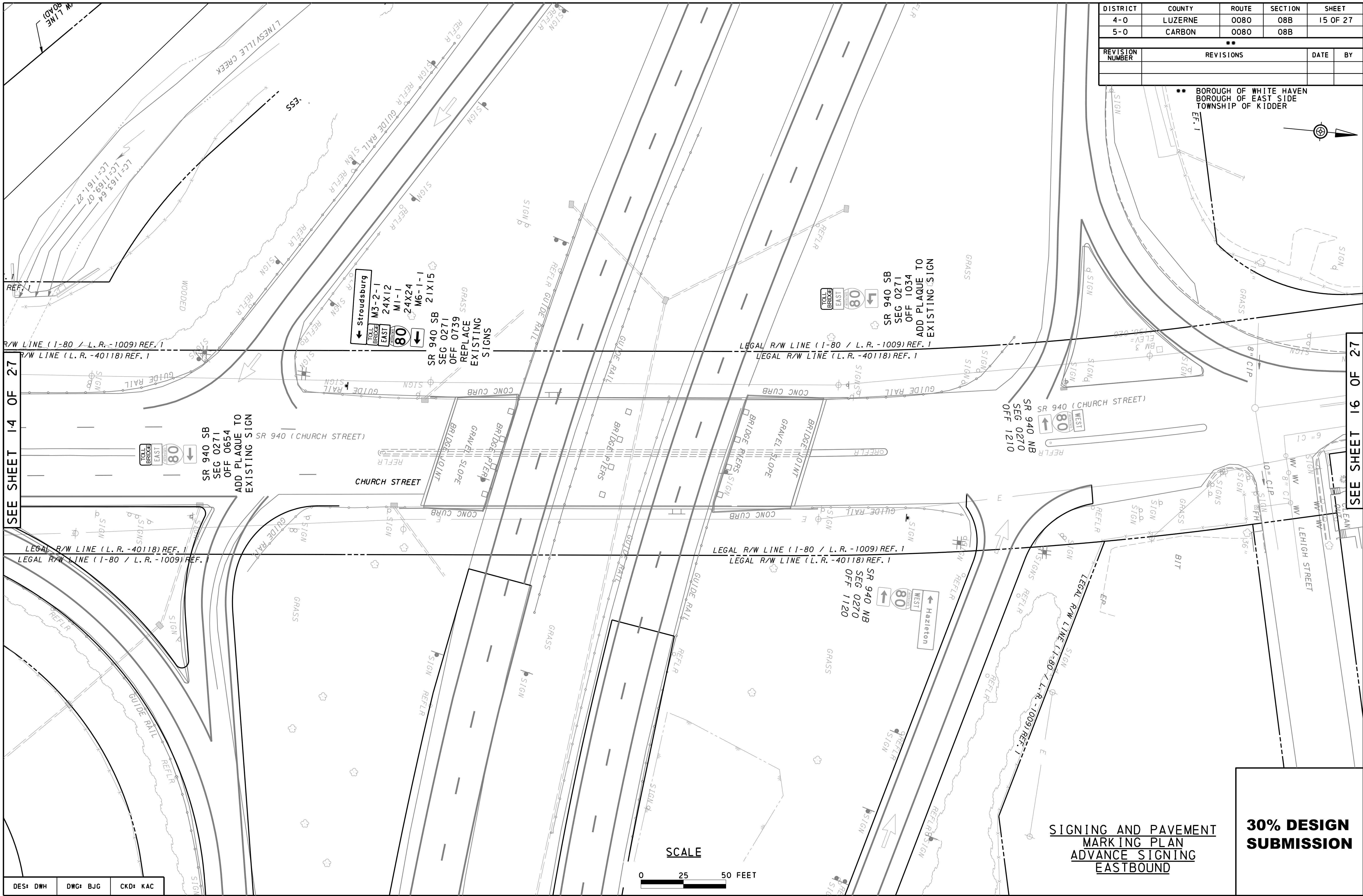
**30% DESIGN  
SUBMISSION**



DISTRICT	COUNTY	ROUTE	SECTION	SHEET
4-0	LUZERNE	0080	08B	15 OF 27
5-0	CARBON	0080	08B	

REVISION NUMBER	REVISIONS	DATE	BY
	** BOROUGH OF WHITE HAVEN BOROUGH OF EAST SIDE TOWNSHIP OF KIDDER		



**SIGNING AND PAVEMENT  
MARKING PLAN  
ADVANCE SIGNING  
EASTBOUND**

**30% DESIGN  
SUBMISSION**



SEE SHEET 15 OF 27

DES: DWH DWG: BJG CKD: KAC

LINE 11-80 / L.R.-10091 REF. 1

**EAST TOLL BRIDGE**  
**80**  
 PASSENGER CARS \$XX  
**EZPass** \$XX  
 BILL-BY-MAIL \$XX  
 HIGHER TOLL FOR OTHER CLASSES

SR 940 SEG 0290 OFF 0160

↑ Stroudsburg Hazleton

- M3-2-1 24X12 M1-1 24X24 M6-3-1 21X15
- M3-4-1 24X12 M1-1 24X24 M5-2-1 IR 21X15

SR 940 SEG 0290 OFF 0220 REPLACE EXISTING SIGNS

WEST 80 SR 940 SEG 0290 OFF 0105

ROW / INGRESS EGRESS

Ø1 101.00' 80.00' 36.148N 10

Ø1 R=163.00' L=118.35'

9) S87.1155°E 54.85' (FM) N87.1155°E (C)

JCT 80

SR 940 SEG 0290 OFF 0549

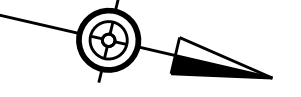
CHURCH STREET

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
4-0	LUZERNE	0080	08B	16 OF 27
5-0	CARBON	0080	08B	

REVISION NUMBER	REVISIONS	DATE	BY

\*\* BOROUGH OF WHITE HAVEN BOROUGH OF EAST SIDE TOWNSHIP OF KIDDER



SCALE



SIGNING AND PAVEMENT MARKING PLAN  
ADVANCE SIGNING EASTBOUND

**30% DESIGN SUBMISSION**

DES: DWH DWG: BJG CKD: KAC

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
4-0	LUZERNE	0080	08B	17 OF 27
5-0	CARBON	0080	08B	

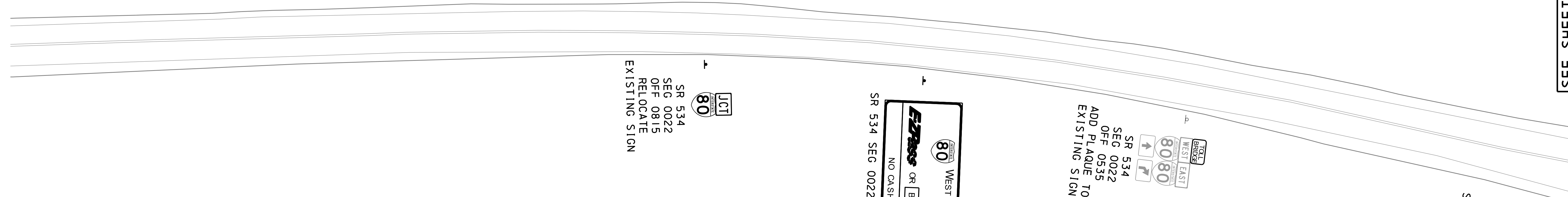
  

REVISION NUMBER	REVISIONS	DATE	BY

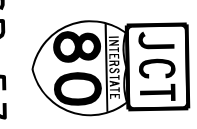
\*\* BOROUGH OF WHITE HAVEN  
BOROUGH OF EAST SIDE  
TOWNSHIP OF KIDDER



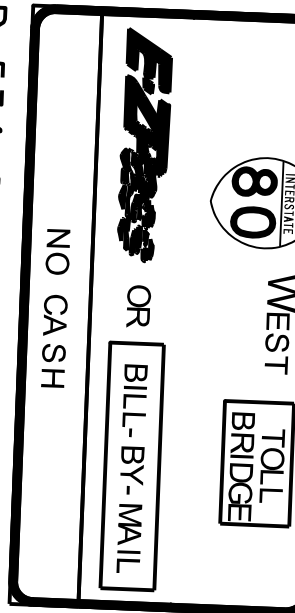
SEE SHEET 18 OF 27



SR 534  
SEG 0022  
OFF 0815  
RELOCATE  
EXISTING SIGN

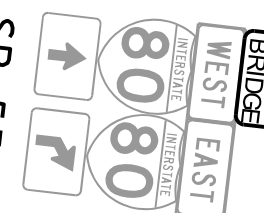


SR 534 SEG 0022 OFF 0695



NO CASH

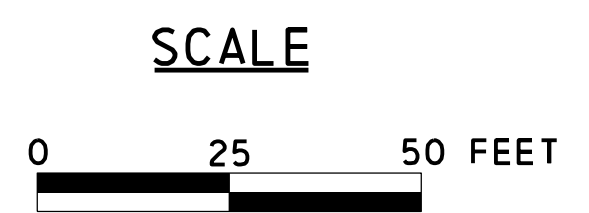
SR 534  
SEG 0022  
OFF 0535  
ADD PLAQUE TO  
EXISTING SIGN



SR 534 SEG 0022 OFF 0390



PASSENGER CARS \$XX  
BILL-BY-MAIL \$XX  
HIGHER TOLL FOR OTHER CLASSES



SIGNING AND PAVEMENT  
MARKING PLAN  
ADVANCE SIGNING  
WESTBOUND

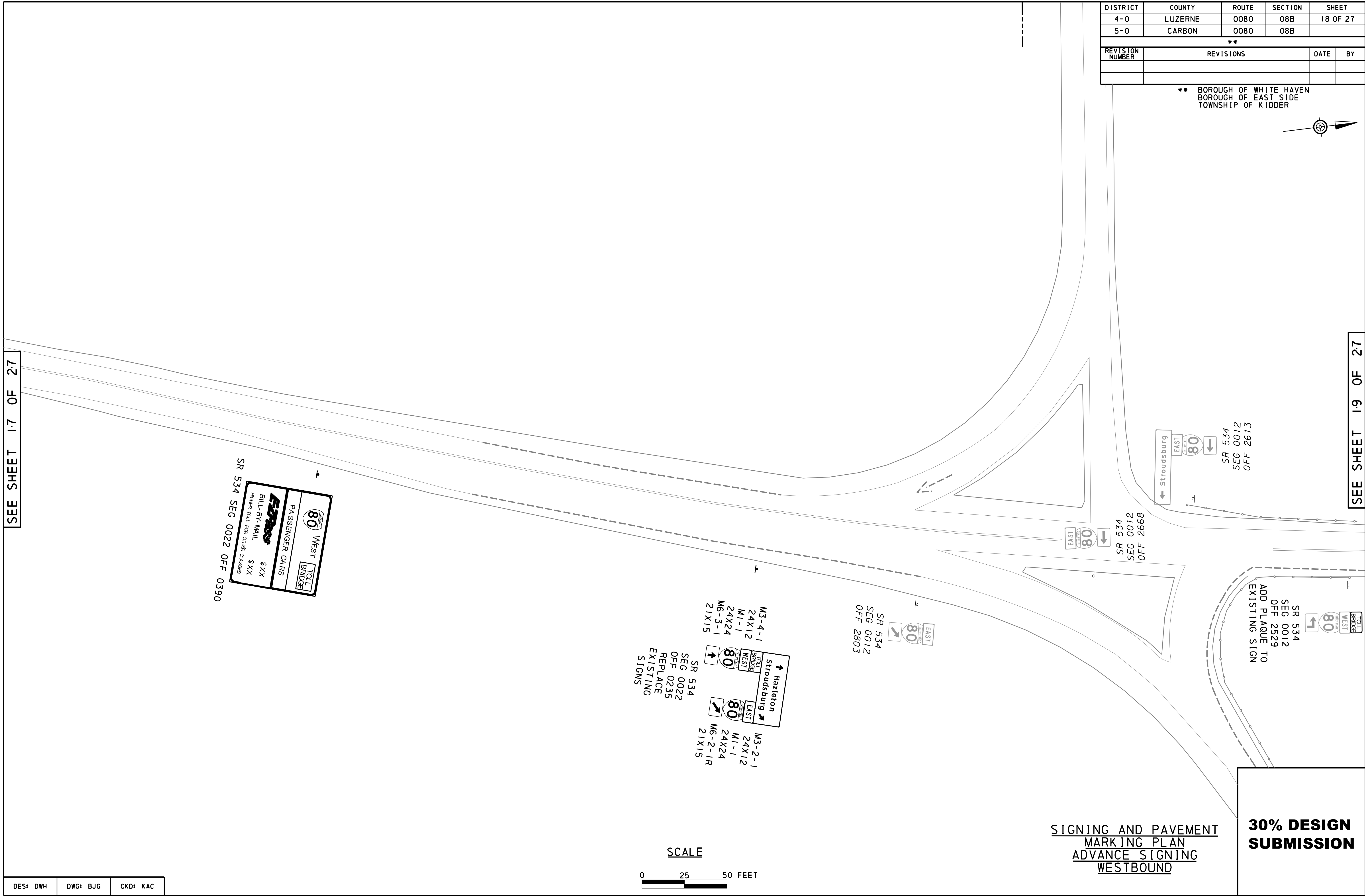
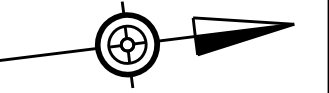
**30% DESIGN  
SUBMISSION**

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
4-0	LUZERNE	0080	08B	18 OF 27
5-0	CARBON	0080	08B	

REVISION NUMBER	REVISIONS	DATE	BY

\*\* BOROUGH OF WHITE HAVEN  
BOROUGH OF EAST SIDE  
TOWNSHIP OF KIDDER



SR 534 SEG 0022 OFF 0390

<b>80</b> WEST	TOLL BRIDGE
PASSENGER CARS	
<b>E-ZPass</b>	
BILL-BY-MAIL	\$XX
HIGHER TOLL FOR OTHER CLASSES	\$XX

M3-4-1  
24X12  
M1-1  
24X24  
M6-3-1  
21X15

SR 534  
SEG 0022  
OFF 0235  
REPLACE  
EXISTING  
SIGNS

↓ Hazleton	↑ Stroudsburg
↓	↑
<b>80</b> WEST	<b>80</b> EAST

M3-2-1  
24X12  
M1-1  
24X24  
M6-2-1R  
21X15

SR 534  
SEG 0012  
OFF 2803

<b>80</b> EAST
----------------

SR 534  
SEG 0012  
OFF 2668

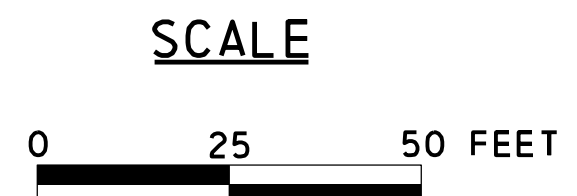
<b>80</b> EAST
----------------

SR 534  
SEG 0012  
OFF 2613

<b>80</b> EAST
----------------

SR 534  
SEG 0012  
OFF 2529  
ADD PLAQUE TO  
EXISTING SIGN

<b>80</b> WEST
----------------



SIGNING AND PAVEMENT  
MARKING PLAN  
ADVANCE SIGNING  
WESTBOUND

**30% DESIGN  
SUBMISSION**

SEE SHEET 18 OF 27

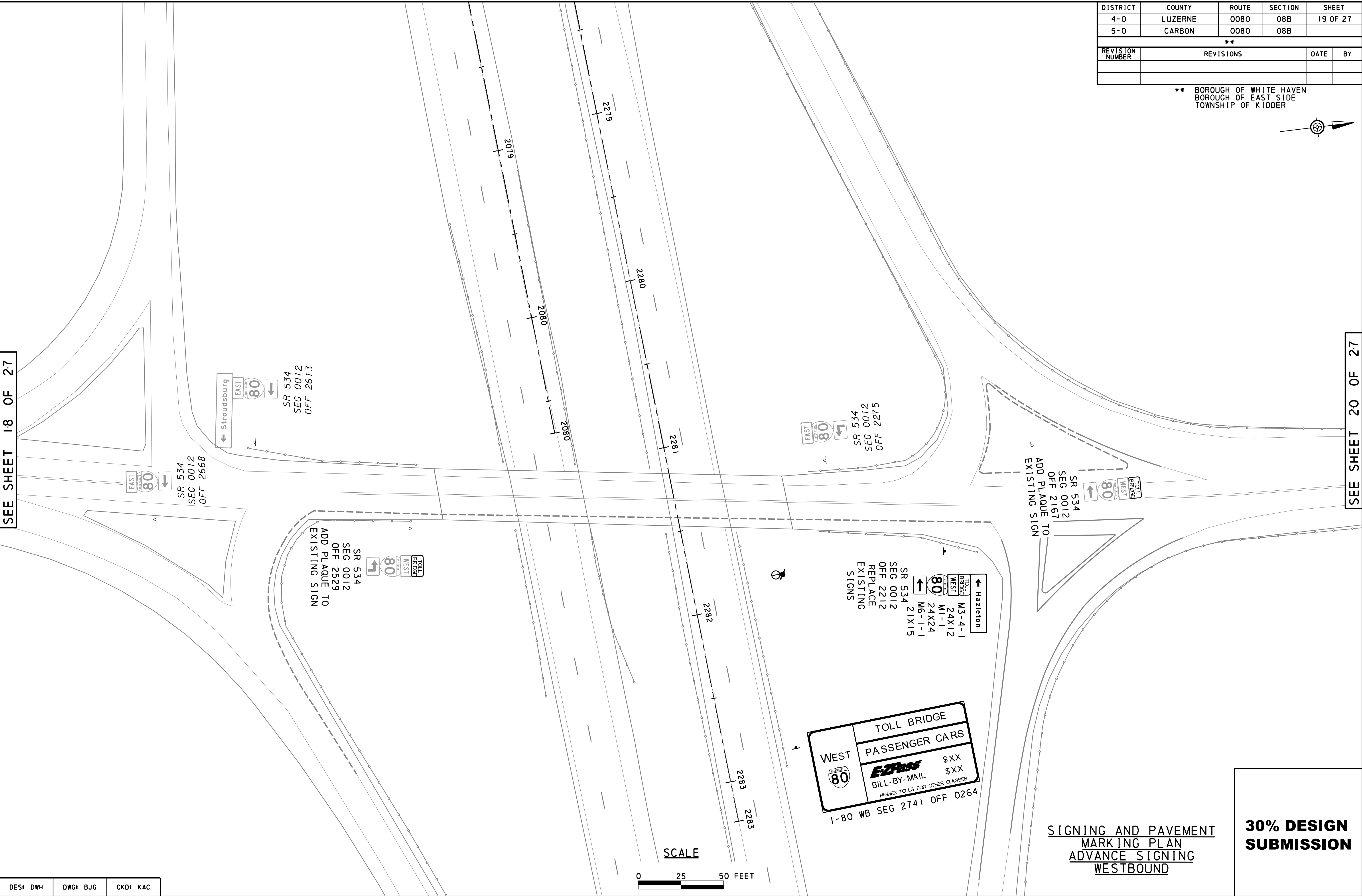
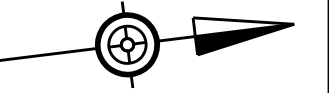
SEE SHEET 20 OF 27

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
4-0	LUZERNE	0080	08B	19 OF 27
5-0	CARBON	0080	08B	

REVISION NUMBER	REVISIONS	DATE	BY

\*\* BOROUGH OF WHITE HAVEN  
BOROUGH OF EAST SIDE  
TOWNSHIP OF KIDDER



DES: DWH	DWG: BJC	CKD: KAC
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SIGNING AND PAVEMENT  
MARKING PLAN  
ADVANCE SIGNING  
WESTBOUND

**30% DESIGN  
SUBMISSION**

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
4-0	LUZERNE	0080	08B	20 OF 27
5-0	CARBON	0080	08B	

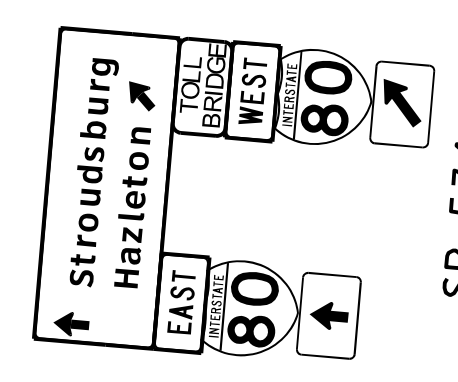
**				
REVISION NUMBER	REVISIONS	DATE	BY	

\*\* BOROUGH OF WHITE HAVEN  
BOROUGH OF EAST SIDE  
TOWNSHIP OF KIDDER



SEE SHEET 19 OF 27

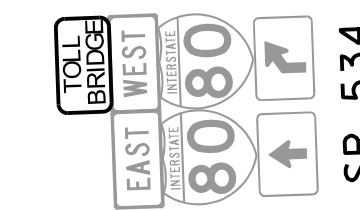
ADD PLAQUE TO EXISTING SIGN  
SR 534  
SEG 0012  
OFF 2167



SR 534  
SEG 0012  
OFF 1853  
RECONFIGURE  
EXISTING SIGNS  
ADD PLAQUE



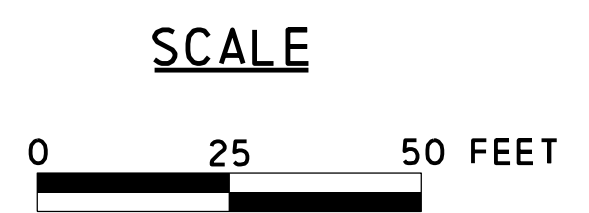
SR 534 SEG 0012 OFF 1665



SR 534  
SEG 0012  
OFF 1468  
ADD PLAQUE TO  
EXISTING SIGN

SEE SHEET 21 OF 27

DES: DWH    DWG: BJG    CKD: KAC



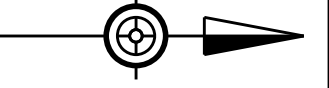
SIGNING AND PAVEMENT  
MARKING PLAN  
ADVANCE SIGNING  
WESTBOUND





**30% DESIGN  
SUBMISSION**

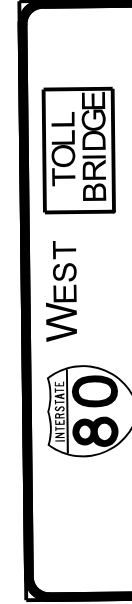
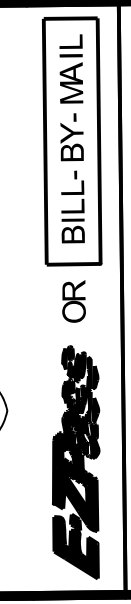
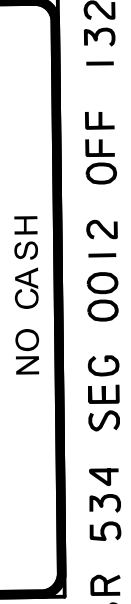
SEE SHEET 20 OF 27



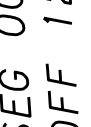
DISTRICT	COUNTY	ROUTE	SECTION	SHEET
4-0	LUZERNE	0080	08B	21 OF 27
5-0	CARBON	0080	08B	
**				
REVISION NUMBER	REVISIONS	DATE	BY	

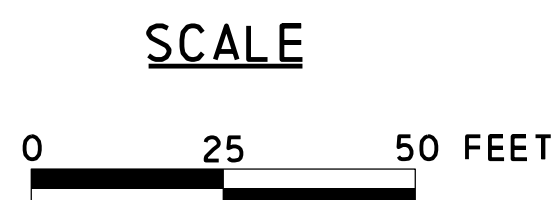
\*\* BOROUGH OF WHITE HAVEN  
BOROUGH OF EAST SIDE  
TOWNSHIP OF KIDDER



  
  
  
  
 SR 534  
 SEG 0012  
 OFF 1468  
 ADD PLAQUE TO  
 EXISTING SIGN

  
  
  
 WEST  
 TOLL BRIDGE  
 OR  
 BILL-BY-MAIL  
 NO CASH  
 SR 534 SEG 0012 OFF 1320

  
  
  
 JCT  
 SR 534  
 SEG 0012  
 OFF 1203



SIGNING AND PAVEMENT  
 MARKING PLAN  
 ADVANCE SIGNING  
 WESTBOUND

**30% DESIGN  
SUBMISSION**

4/28/2021

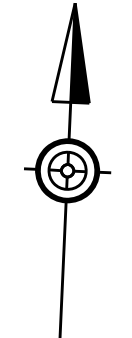
PLOTTED:

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
4-0	LUZERNE	0080	08B	22 OF 27
5-0	CARBON	0080	08B	

**				
REVISION NUMBER	REVISIONS	DATE	BY	

\*\* BOROUGH OF WHITE HAVEN  
 BOROUGH OF EAST SIDE  
 TOWNSHIP OF KIDDER

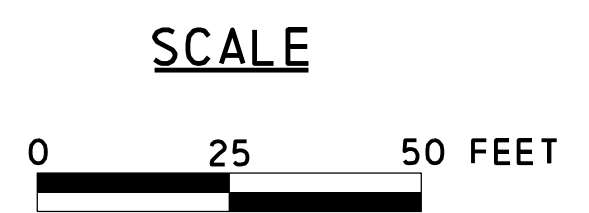


TOLL BRIDGE	
PASSENGER CARS	\$ XX
<b>EZPass</b>	\$ XX
BILL-BY-MAIL	\$ XX
HIGHER TOLLS FOR OTHER CLASSES	
WEST	
<b>80</b>	

I-80 WB SEG 2741 OFF 0264

EXIT	
<b>274</b>	
I-80 WB	
SEG 2741	
OFF 0644	

2283      2283



SIGNING AND PAVEMENT  
 MARKING PLAN  
 ADVANCE SIGNING  
 WESTBOUND

**30% DESIGN  
 SUBMISSION**

DES: DWH    DWG: BJG    CKD: KAC

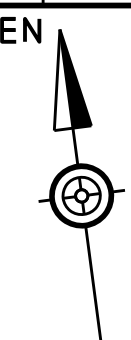
Signing and Pavement Marking Plan

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
4-0	LUZERNE	0080	08B	23 OF 27
5-0	CARBON	0080	08B	

**		DATE	BY
REVISION NUMBER	REVISIONS		

\*\* BOROUGH OF WHITE HAVEN  
BOROUGH OF EAST SIDE  
TOWNSHIP OF KIDDER



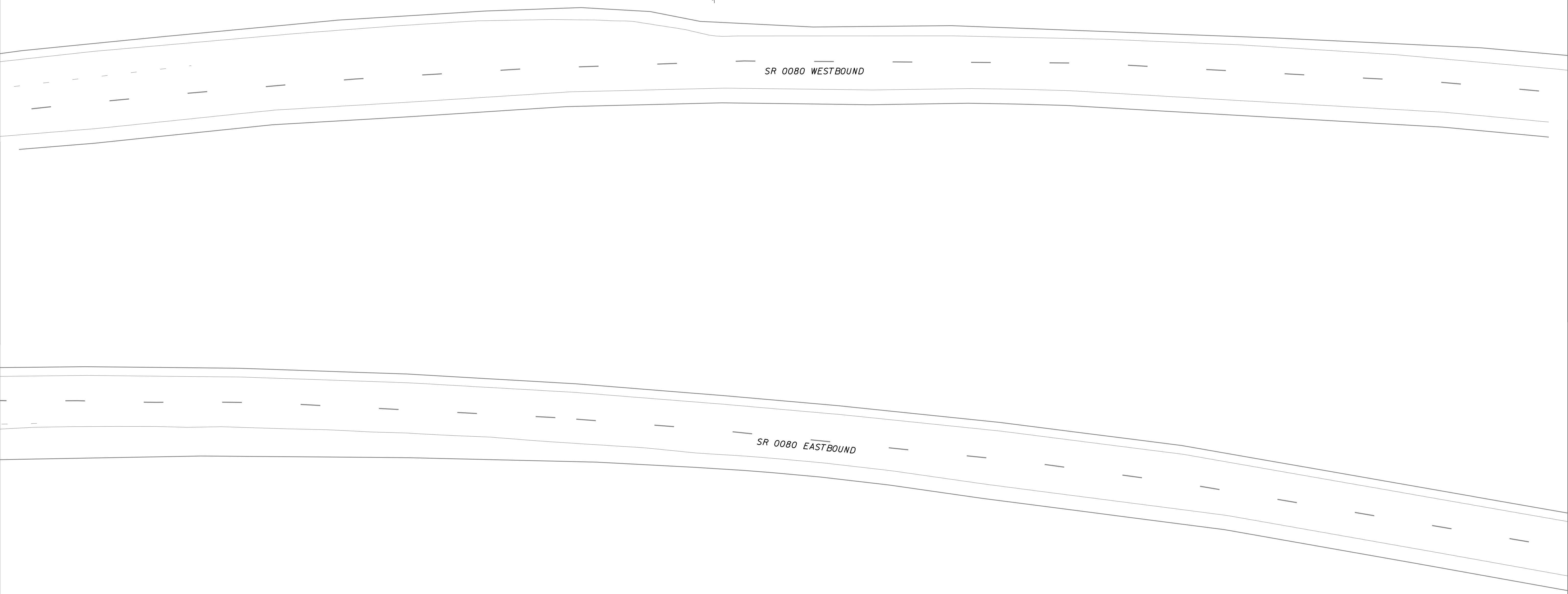
EXIT 274

534

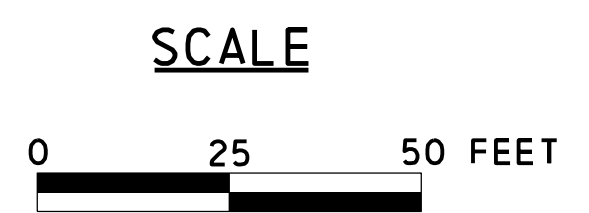
Hickory Run ↑  
State Park

LAST EXIT BEFORE BRIDGE TOLL

1-80 WB SEC 2741 OFF 1679  
ADD PLAQUE TO EXISTING SIGN



DES: DWH	DWG: BJG	CKD: KAC
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SIGNING AND PAVEMENT  
MARKING PLAN  
ADVANCE SIGNING  
WESTBOUND

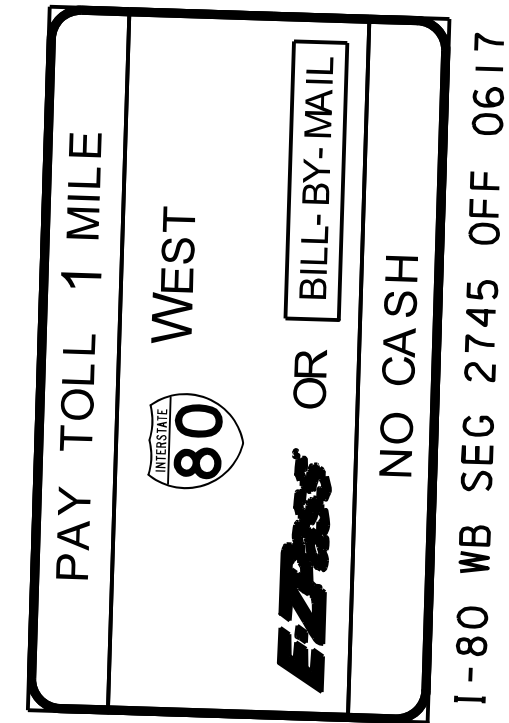
**30% DESIGN  
SUBMISSION**



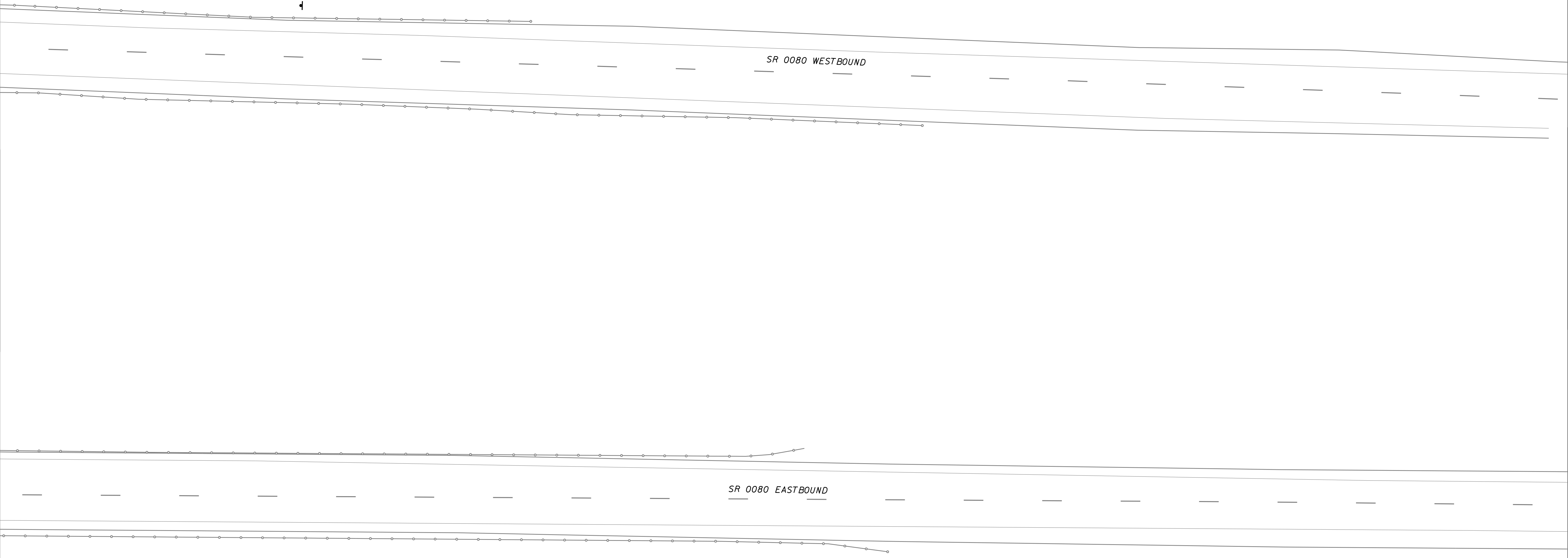
4/28/2021  
PLOTTED:

Signing and Pavement Marking Plan

DES: DWH    DWG: BJB    CKD: KAC



+



SCALE



SIGNING AND PAVEMENT  
MARKING PLAN  
ADVANCE SIGNING  
WESTBOUND

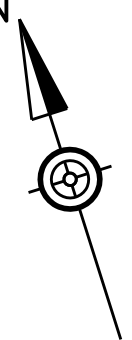
**30% DESIGN  
SUBMISSION**

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
4-0	LUZERNE	0080	08B	24 OF 27
5-0	CARBON	0080	08B	

**		DATE	BY
REVISION NUMBER	REVISIONS		

\*\* BOROUGH OF WHITE HAVEN  
BOROUGH OF EAST SIDE  
TOWNSHIP OF KIDDER

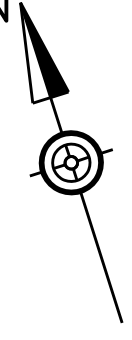


DISTRICT	COUNTY	ROUTE	SECTION	SHEET
4-0	LUZERNE	0080	08B	25 OF 27
5-0	CARBON	0080	08B	

**				
REVISION NUMBER	REVISIONS	DATE	BY	

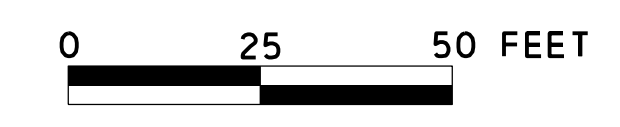
\*\* BOROUGH OF WHITE HAVEN  
BOROUGH OF EAST SIDE  
TOWNSHIP OF KIDDER



SR 0080 WESTBOUND

SR 0080 EASTBOUND

SCALE



SIGNING AND PAVEMENT  
MARKING PLAN  
ADVANCE SIGNING  
WESTBOUND

**30% DESIGN  
SUBMISSION**

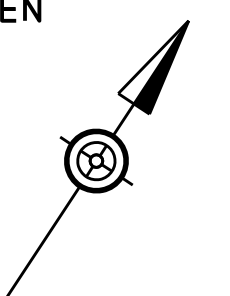
DES: DWH	DWG: BJB	CKD: KAC
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DISTRICT	COUNTY	ROUTE	SECTION	SHEET
4-0	LUZERNE	0080	08B	26 OF 27
5-0	CARBON	0080	08B	

**				
REVISION NUMBER	REVISIONS	DATE	BY	

\*\* BOROUGH OF WHITE HAVEN  
BOROUGH OF EAST SIDE  
TOWNSHIP OF KIDDER

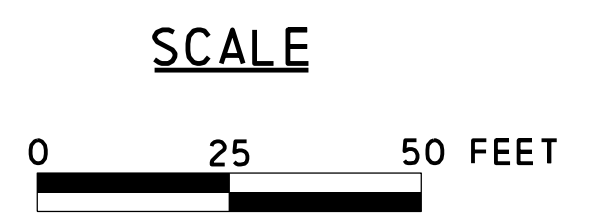
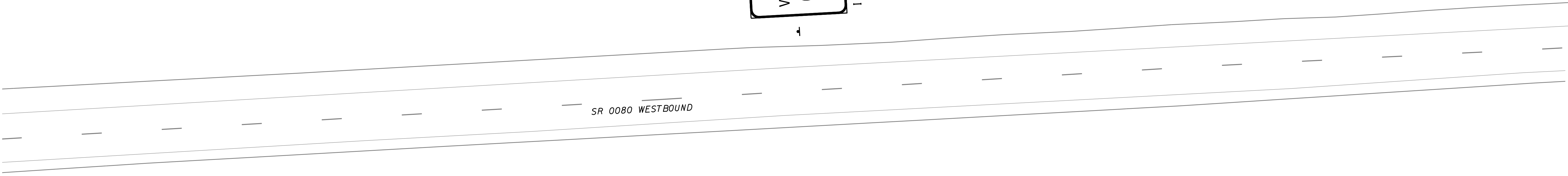


TOLL BRIDGE  
PASSENGER CARS

**EZPass**  
BILL-BY-MAIL \$XX  
HIGHER TOLLS FOR OTHER CLASSES \$XX

WEST  
**80**  
MILE PER HOUR

I-80 WB SEC 2755 OFF 0887



SIGNING AND PAVEMENT  
MARKING PLAN  
ADVANCE SIGNING  
WESTBOUND

**30% DESIGN  
SUBMISSION**

PAY TOLL 2 1/2 MILES  
WEST  
E-ZPass OR BILL-BY-MAIL  
NO CASH  
I-80 WB SEG 2761 OFF 0662

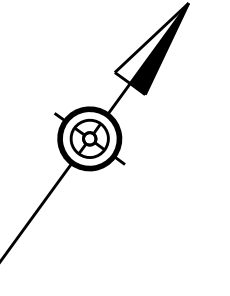
EXIT 274  
534  
Hickory Run  
State Park  
EXIT 2 MILES  
I-80 WB  
SEG 2761 OFF 0839

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
4-0	LUZERNE	0080	08B	27 OF 27
5-0	CARBON	0080	08B	

REVISION NUMBER	REVISIONS	DATE	BY

\*\* BOROUGH OF WHITE HAVEN  
BOROUGH OF EAST SIDE  
TOWNSHIP OF KIDDER



SR 0080 WESTBOUND

SR 0080 WESTBOUND

SCALE



SIGNING AND PAVEMENT  
MARKING PLAN  
ADVANCE SIGNING  
WESTBOUND

**30% DESIGN  
SUBMISSION**

**Appendix E**  
**Agency Coordination**

## Krommes, Kathy

---

**From:** Rossiter, Kelly <krossiter@pa.gov>  
**Sent:** Thursday, September 2, 2021 12:16 PM  
**To:** Markowitz, Katherine  
**Cc:** Krommes, Kathy; Baumgardner, John  
**Subject:** RE: [External] RE: I-80 White Haven Bridges - Scenic River Review

**CAUTION: [EXTERNAL]** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Thank you Katie,  
I'll file this with the other project materials. I appreciate the point by point feedback and responses. We typically like to see the concrete on bridges tinted because the standard stark white is a bit jarring in a scenic setting. However, if you're construction contract cannot be amended to include such a alteration to standard practice, that is still acceptable.

Regards,

**Kelly Rossiter, AICP** | Rivers Program Specialist  
Department of Conservation and Natural Resources  
Bureau of Recreation and Conservation  
400 Market St, 5<sup>th</sup> Floor | Harrisburg, PA 17101-2301  
Phone: 717.772.3319 | Fax: 717.787.9577  
<http://www.dcnr.state.pa.us/brc> | [www.ExplorePAtrails.com](http://www.ExplorePAtrails.com)

---

**From:** Markowitz, Katherine <Katherine.Markowitz@hdrinc.com>  
**Sent:** Thursday, August 5, 2021 11:27 AM  
**To:** Rossiter, Kelly <krossiter@pa.gov>  
**Cc:** Krommes, Kathy <Kathleen.Krommes@hdrinc.com>; Baumgardner, John <John.Baumgardner@hdrinc.com>  
**Subject:** [External] RE: I-80 White Haven Bridges - Scenic River Review

**ATTENTION:** This email message is from an external sender. Do not open links or attachments from unknown sources. To report suspicious email, forward the message as an attachment to [CWOPA\\_SPAM@pa.gov](mailto:CWOPA_SPAM@pa.gov).

Hello Kelly,

HDR, on behalf of PennDOT, has reviewed the DCNR Scenic Rivers Program review letter dated January 5, 2021, for the I-80 Section 08B Bridge Project. Please see the attached response letter dated August 3, 2021.

Thank you,

**Katie Markowitz**  
D 717-516-3155 M 201-258-0508

[hdrinc.com/follow-us](http://hdrinc.com/follow-us)

---

**From:** Rossiter, Kelly <krossiter@pa.gov>  
**Sent:** Tuesday, January 5, 2021 1:53 PM  
**To:** Markowitz, Katherine <Katherine.Markowitz@hdrinc.com>  
**Cc:** Krommes, Kathy <Kathleen.Krommes@hdrinc.com>

**Subject:** RE: I-80 White Haven Bridges - Scenic River Review

**Importance:** High

**CAUTION: [EXTERNAL]** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Katie,

Attached is the DCNR Scenic Rivers Program review letter related to the subject project. If you have any questions or concerns about the requested mitigations, please email me back or let me know a few times you can talk on the phone and we'll set up a call.

Take care. Best wishes for a healthy 2021.

**Kelly Rossiter, AICP** | Rivers Program Specialist  
Department of Conservation and Natural Resources  
Bureau of Recreation and Conservation  
400 Market St, 5<sup>th</sup> Floor | Harrisburg, PA 17101-2301  
Phone: 717.772.3319 | Fax: 717.787.9577  
<http://www.dcnr.state.pa.us/brc> | [www.ExplorePAtrails.com](http://www.ExplorePAtrails.com)

---

**From:** Markowitz, Katherine <[Katherine.Markowitz@hdrinc.com](mailto:Katherine.Markowitz@hdrinc.com)>

**Sent:** Friday, December 11, 2020 4:33 PM

**To:** Rossiter, Kelly <[krossiter@pa.gov](mailto:krossiter@pa.gov)>

**Cc:** Krommes, Kathy <[Kathleen.Krommes@hdrinc.com](mailto:Kathleen.Krommes@hdrinc.com)>

**Subject:** [External] I-80 White Haven Bridges - Scenic River Review

**ATTENTION:** This email message is from an external sender. Do not open links or attachments from unknown sources. To report suspicious email, forward the message as an attachment to [CWOPA\\_SPAM@pa.gov](mailto:CWOPA_SPAM@pa.gov).

Hello Kelly,

HDR, on behalf of PennDOT, is providing environmental services for the I-80 Bridge Replacement Project near White Haven Borough, Luzerne County, PA. We are pursuing a Scenic River review for the Lehigh River, which our project area traverses. Please find attached a letter requesting review by the DCNR along the project description, location, and accompanying attachments.

Thank you,

**Katie Markowitz**  
Environmental Scientist

**HDR**  
4900 Ritter Road, Suite 101  
Mechanicsburg, PA 17055  
D 717-516-3155 M 201-258-0508  
[katherine.markowitz@hdrinc.com](mailto:katherine.markowitz@hdrinc.com)  
[hdrinc.com/follow-us](http://hdrinc.com/follow-us)

BUREAU OF RECREATION AND CONSERVATION

January 5, 2021

Katherine Markowitz  
HDR Engineering Inc  
4900 Ritter Road, Suite 101  
Mechanicsburg, PA 17055

**RE: Scenic River Reviews – 2021**

**Project Name:** I-80 Section 08B Bridge

**Description:** Bridge Replacement Project

**Location:** White Haven Borough, Luzerne County and East Side Borough, Carbon County

**Applicant Code:** 946

**Request Date:** December 11, 2020

Ms. Markowitz:

Information submitted regarding the subject application has been reviewed. The project lies within the **Lehigh River Pennsylvania Scenic Rivers Corridor System**. Because of Scenic River involvement compliance with Sec 2. of the “Pennsylvania Scenic Rivers Act” of 1972, P.L. 1277, No. 283 as amended May 7, 1982, P.L. 379, No. 110, is necessary:

*“Many of the rivers of Pennsylvania or sections thereof and related adjacent land areas, possess outstanding aesthetic and recreational values of present and potential benefit to the citizens of Pennsylvania. It shall be the policy of the Commonwealth to protect these values and to practice sound conservation policies and practices within this scenic rivers system.”*

The project information provided addressed several typical concerns with bridge projects such as utilizing robust E&S plans and developing an approved Aids to Navigation plan to accommodate recreational use of the waterway during construction. In addition to the plans already presented, DCNR requests that the following actions be integrated into the project to further protect river values that elevate this waterway to inclusion in the scenic rivers program.

- The construction and demolition staging areas should be screened by a vegetative buffer and set back as far as possible from the river’s edge.
- Bridge materials and design should reflect the character of the surrounding environment as well as any local architectural style, normally creating a “natural” or “rustic” appearance. DCNR would encourage clean, simple, lines that complement rather than dominate the landscape. Additionally, since the river segment is a water trail, the design needs to allow safe recreational use / watercraft passage.



- Consider using darker sand and aggregate native to the area or add a color/tint to any concrete mix to soften the appearance of any new concrete. Concrete structures could also be masonry faced with stone native to the area or the concrete surface could be rough faced/textured and tinted a “natural” color (green, brown, tan, grey, etc).
- Exposed metal structures should be painted a “natural” color. Painting procedures should have minimal impact on the creek and its environs and a containment system that prevents paint and paint chips from entering the creek must be used if any part of the bridge is being painted.
- Native or local stone should be used in areas where riprap or stone is needed.
- Native vegetation should be incorporated in the disturbed riverbank areas to maintain the natural character and the scenic qualities of the waterway.
- The Scenic Rivers Program recommends that two types of signs be placed on or in close proximity to the bridge. The first type of sign is to be placed at each end of the bridge to let the public know that they are crossing a designated state scenic river. A copy of this sign design is included as Appendix A.

The second type of sign will aid water trail users and safety responders in navigation. It is to be placed on the upstream side of the bridge and centered on the outside face of the parapet. The sign should identify the bridge name so that users can identify their location along the Lehigh River. The design for this is included as Appendix B.

- DCNR is a proponent of increased waterway access along designated water trails such as the Lehigh River Water Trail. Therefore, this bridge project should include basic accommodations for a non-motorized boat launch as part of the project design, if at all possible.
- Additionally, DCNR encourages that bridge replacements include pedestrian/bike access along the bridge to contribute to a more complete transportation system.

**If you concur with implementation of these actions, please sign off on the line provided below** and return a copy to me (contact information below). Once signed, you may include this letter with your required permit documentation submission to DEP to verify that the subject plans are consistent with the requirements of the Pennsylvania Scenic Rivers Act.

**See response letter attached.**

---

(Name)

(Position)

Katherine Markowitz  
I-80 Section 08B Bridge Replacement  
January 5, 2021  
Page #3

Should you have any questions regarding our review, process or recommendations for this or future applications, please contact me.

Sincerely,

A handwritten signature in blue ink that reads "Kelly Rossiter". The signature is written in a cursive style with a large, looping initial "K".

Kelly Rossiter, AICP  
Rivers Program Specialist  
Partnerships Division

## APPENDIX A

### I-80 BRIDGE\* SIGN SPECIFICATIONS

#### **Description:**

(2) 48-inch x 24-inch .080 gauge aluminum signs, highway blue background with white border and 4 lines of white lettering. The upper left-hand corner contains a 12-inch x 8-inch logo designed to identify the Pennsylvania Scenic Rivers System.

#### **Printing:**

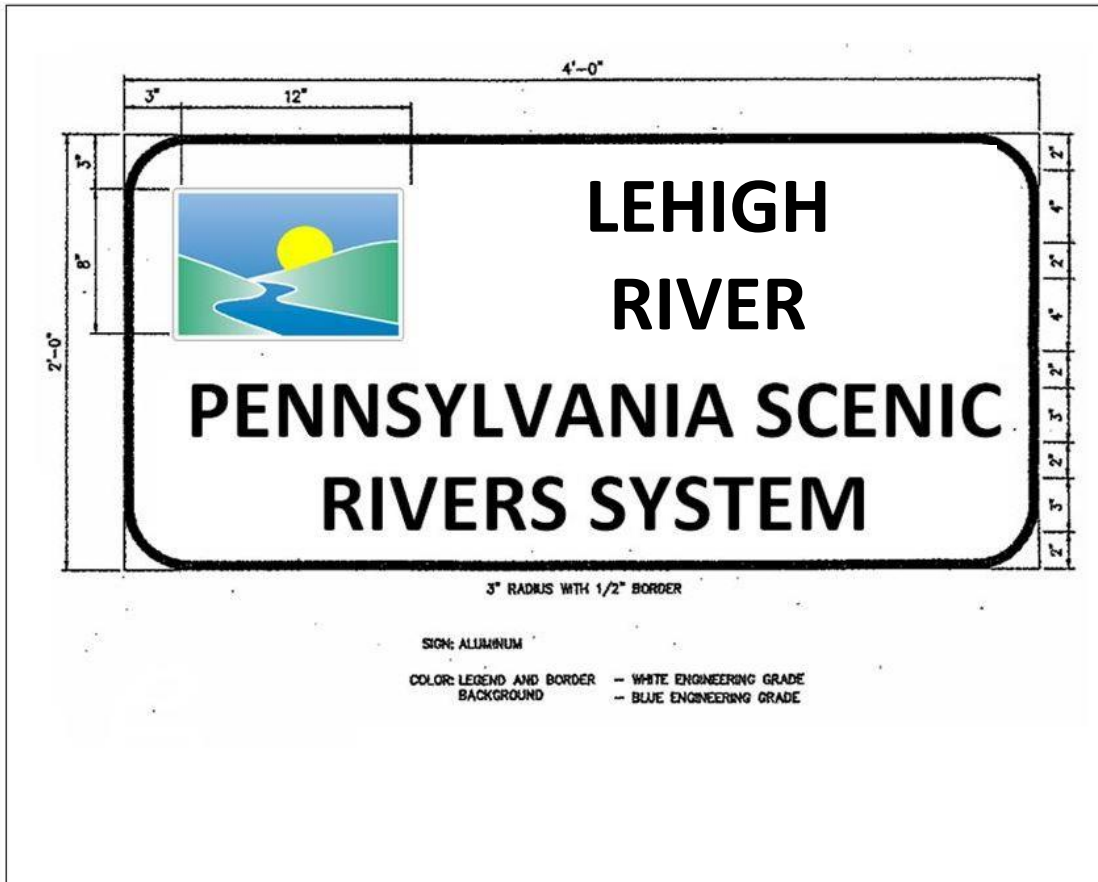
The first line will consist of the Scenic Rivers logo and the name **LEHIGH**. The name on the second line will be RIVER. The logo is to be placed on the left half of the sign area. The name **LEHIGH RIVER** is to be placed on the right half of the sign area and will have lettering 4 inches high. The third and fourth lines will have lettering 3 inches high and will be put on blanks as follows:

PENNSYLVANIA SCENIC  
RIVER SYSTEM

#### **Specific Instructions:**

Signs can be placed upon completion and as directed by the Engineer. The two signs that read **LEHIGH RIVER PENNSYLVANIA SCENIC RIVERS SYSTEM** are to be placed at each end of the bridge.

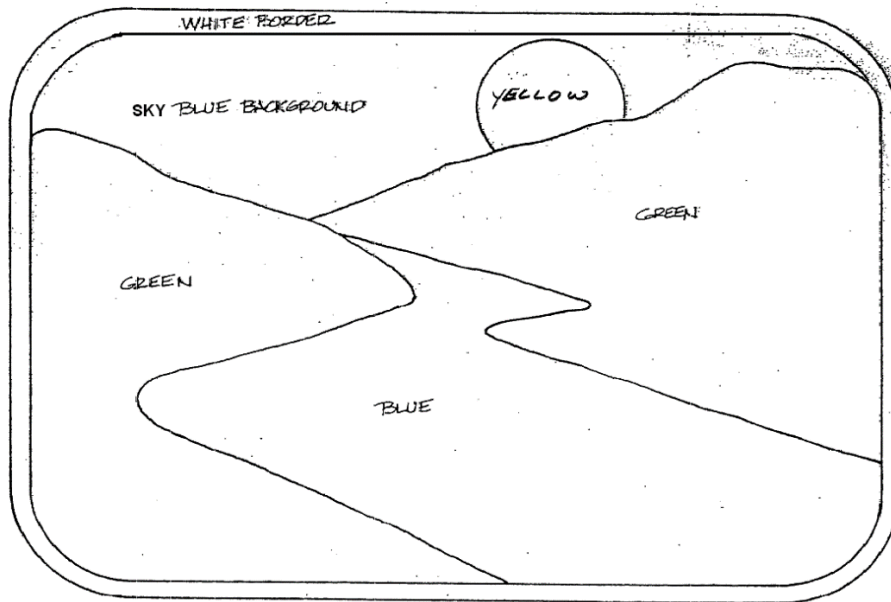
*\*I-80 Bridge is a placeholder: bridge name can be changed, based on how the bridge is known locally, appears on maps, is referenced for emergency management purposes, etc.*



### LOGO SPECIFICATIONS

**Color:** River - Peacock Blue – Ad-vance  
Land - Emerald Green – Colonial  
Sun - Chrome yellow – Colonial  
Background – Sky-blue  
Border - White

**Size:** Size of logo should be in proportion to size of sign with lettering (*see attached sample sign*)



## APPENDIX B

### I-80 BRIDGE SIGN SPECIFICATIONS

**Description:**

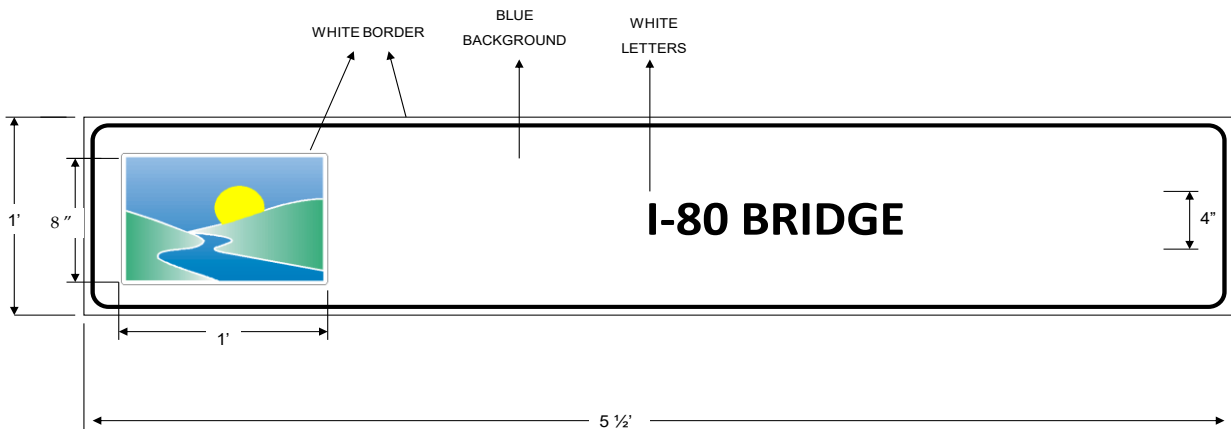
(1) 66-inch x 12 inch .080 gauge aluminum sign, highway blue background with white border and one line of white lettering. The left hand side contains a 12-inch x 8-inch logo designed to identify the Pennsylvania Scenic Rivers System.

**Printing:**

The sign will consist of the Scenic Rivers logo and the name **I-80 BRIDGE**. The logo is to be placed on the left hand side of the sign area. The name is to be placed to the right of the logo and will have lettering 4 inches high.

**Specific Instructions:**

The sign is to be placed on the outside of the parapet, as directed by the Engineer.



## **Appendix C**

### **Way Finding Signs on Bridges over Water Trails**

Water trails are recreational waterways on lakes, rivers, or streams, suitable for canoes, kayaks and small motorized watercraft. Like conventional trails, water trails are recreational corridors between specific locations, containing access points and day-use and/or camping sites for the boating public. In Pennsylvania, water trails are officially designated by the PA Fish and Boat Commission (PFBC).

For all bridge replacement/rehabilitation projects identification of PFBC designated water trails should be made during the scoping field view. A list of designated water trails can be found on the PFBC website at <http://fishandboat.com/watertrails/trailindex.htm>.

Projects involving these water trails are to include the installation of way finding signs that identify the route/road name of the structure crossing the water trail. The purpose of the sign is to provide aid to boaters in identifying their location on the water trail. The sign(s) should be placed on both the upstream and downstream sides of the bridge and be visible to boaters but should not detract from boaters' experience in the natural setting. If the waterway is 150 feet wide or more, multiple signs should be placed no more than 75 feet apart. On waters less than 150 feet, one sign, placed near mid waterway, is sufficient as long as terrain allows it to be visible from either shoreline.

The project manager should include the sign installation in the Environmental Commitment Mitigation Tracking System.

The Overhead Roadway Identification Sign (I18-1) will be used for this purpose. A non-reflective blue/white sign will be used. A manual SAP order should be placed for an Overhead Roadway Identification (I18-1), blue/ white non-reflective sign and include the specific Route/Road Name needed. The sign standard can be found in [Pub 236, Handbook of Approved Signs](#).

The Route number should be listed first followed by the Road Name. Examples follow:

I-79 Raymond P. Shafer Highway  
US 22 William Penn Highway  
PA 51 Morgantown Street  
PA 2 Lackawanna Trail  
PA 501 Lititz Pike  
SR 0504 Appalachian Thruway



August 3, 2021

Kelly Rossiter, AICP  
Rivers Program Specialist  
Partnerships Division

Re: **Scenic River Reviews – 2021**  
**Project Name:** I-80 Section 08B Bridges  
**Location:** White Haven Borough, Luzerne County and East Side Borough, Carbon County  
**Application Code:** 946

Thank you for providing Scenic River Review for the subject project. In response to the letter from DCNR dated January 5, 2021, the PennDOT bridge design team provides the following response to comments.

DCNR Comment: The project information provided addressed several typical concerns with bridge projects such as utilizing robust E&S plans and developing an approved Aids to Navigation plan to accommodate recreational use of the waterway during construction. In addition to the plans already presented, DCNR requests that the following actions be integrated into the project to further protect river values that elevate this waterway to inclusion in the scenic rivers program.

PennDOT Response: The project will include Erosion and Sedimentation Control Plans, in addition to a NPDES permit. The project Aids to Navigation Plan will be reviewed by PAFBC.

DCNR Comment: The construction and demolition staging areas should be screened by a vegetative buffer and set back as far as possible from the river's edge.

PennDOT Response: Vegetative buffers will be provided to extent practicable.

DCNR Comment: Bridge materials and design should reflect the character of the surrounding environment as well as any local architectural style, normally creating a "natural" or "rustic" appearance. DCNR would encourage clean, simple, lines that complement rather than dominate the landscape. Additionally, since the river segment is a water trail, the design needs to allow safe recreational use / watercraft passage.

PennDOT Response: The anticipated bridge type is steel or concrete multi-girder, supported by concrete piers and abutments. The piers as shown in the Conceptual Type, Size, and Location are "Tulip" shaped. The pier shape may be changed in Final Design by the Development Entity if approved by PennDOT. Piers will not be allowed within the middle portion of the Lehigh River, defined as Station 2017+10 to Station 2018+20 for the Eastbound bridge and Station 2217+25 to Station 2218+25 for the Westbound bridge (see attached).



DCNR Comment: Consider using darker sand and aggregate native to the area or add a color/tint to any concrete mix to soften the appearance of any new concrete. Concrete structures could also be masonry faced with stone native to the area or the concrete surface could be rough faced/textured and tinted a “natural” color (green, brown, tan, grey, etc).

PennDOT Response: PennDOT will utilize its standard concrete mix specifications, without requirements of specific sand or aggregate color. The concrete will not be tinted, stone faced or textured.

DCNR Comment: Exposed metal structures should be painted a “natural” color. Painting procedures should have minimal impact on the creek and its environs and a containment system that prevents paint and paint chips from entering the creek must be used if any part of the bridge is being painted.

PennDOT Response: The existing steel bridges are being removed. Steel girders, if used on the new bridge, will be weathering steel or painted a “natural” color to blend with the setting.

DCNR Comment: Native or local stone should be used in areas where riprap or stone is needed.

PennDOT Response: The riprap will be required to meet PennDOT’s material specifications, and the contract will not require native or local stone. Where possible, the contract will require riprap to be covered with soil and vegetation.

DCNR Comment: Native vegetation should be incorporated in the disturbed riverbank areas to maintain the natural character and the scenic qualities of the waterway.

PennDOT Response: The contract will require native vegetation to be used in the disturbed riverbank areas. PennDOT will continue to coordinate with DCNR regarding the project plantings.

DCNR Comment: The Scenic Rivers Program recommends that two types of signs be placed on or in close proximity to the bridge. The first type of sign is to be placed at each end of the bridge to let the public know that they are crossing a designated state scenic river. A copy of this sign design is included as Appendix A.

The second type of sign will aid water trail users and safety responders in navigation. It is to be placed on the upstream side of the bridge and centered on the outside face of the parapet. The sign should identify the bridge name so that users can identify their location along the Lehigh River. The design for this is included as Appendix B.

PennDOT Response: The contract will require the scenic river signs at the leading ends of each bridge. The contract will require signage on the bridge for water trail users. The Development Entity will coordinate with PennDOT during final design regarding the location of the sign(s), as the bridge is approximately 100 feet above the river. The sign(s) may be placed on piers in the vicinity of the river.

DCNR Comment: DCNR is a proponent of increased waterway access along designated water trails such as the Lehigh River Water Trail. Therefore, this bridge project should include basic accommodations for a non-motorized boat launch as part of the project design, if at all possible.

PennDOT Response: DCNR is building a new park entrance just south of the bridge, with construction anticipated to be completed in 2021. The new park facilities will include four new boat ramps. One of these boat ramps will be disturbed by the bridge project and will be reconstructed after the bridge project is complete. The other three boat ramps will not be disturbed by the bridge project and will remain open during construction. Coordination with DNCR regarding impacts to the park facilities is ongoing.

DCNR Comment: Additionally, DCNR encourages that bridge replacements include pedestrian/bike access along the bridge to contribute to a more complete transportation system.

PennDOT Response: The interstate bridges will not accommodate pedestrian/bike traffic. Temporary impacts to the Lehigh Gorge Trail under the bridge are being coordinated with DCNR.

Please contact me if you should have any questions or require further information.

Sincerely,

A handwritten signature in black ink, appearing to read 'JB', with a long horizontal flourish extending to the right.

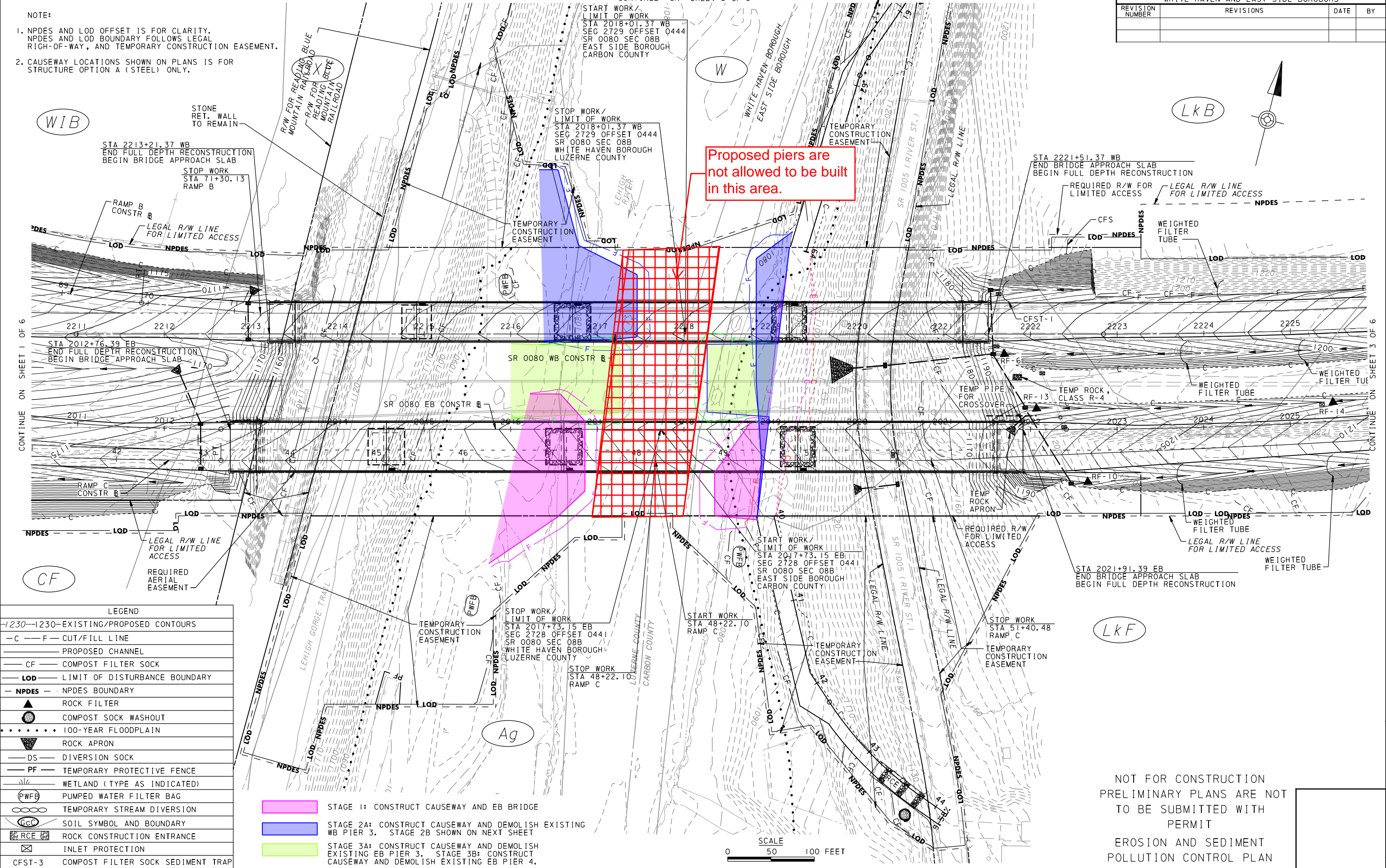
John Baumgardner, PE  
*Project Manager*

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
5-0	*	0080	08B	2 OF 6
WHITE HAVEN AND EAST SIDE BOROUGHS				
REVISION NUMBER	REVISIONS	DATE	BY	

- NOTE:
- NPDES AND LOD OFFSET IS FOR CLARITY. NPDES AND LOD BOUNDARY FOLLOWS LEGAL RIGHT-OF-WAY, AND TEMPORARY CONSTRUCTION EASEMENT.
  - CAUSEWAY LOCATIONS SHOWN ON PLANS IS FOR STRUCTURE OPTION A (STEEL) ONLY.

Proposed piers are not allowed to be built in this area.

USER:LSALQUM PLOT DRIVER:RpmDOT\_PDF\_Color.plt;g PLOT DATE:05-07-2021 3:25:08 PM  
 PATH:ct:\pwworking\ins\1011\0742841\FILE:00808B-ES-PLAN1.dgn MODEL:road\_plan 2



LEGEND	
-1230--1230-	EXISTING/PROPOSED CONTOURS
-C -F-	CUT/FILL LINE
- - -	PROPOSED CHANNEL
CF	COMPOST FILTER SOCK
LOD	LIMIT OF DISTURBANCE BOUNDARY
NPDES	NPDES BOUNDARY
▲	ROCK FILTER
●	COMPOST SOCK WASHOUT
.....	100-YEAR FLOODPLAIN
▲	ROCK APRON
DS	DIVERSION SOCK
PF	TEMPORARY PROTECTIVE FENCE
W	WETLAND (TYPE AS INDICATED)
PWFB	PUMPED WATER FILTER BAG
○	TEMPORARY STREAM DIVERSION
Co	SOIL SYMBOL AND BOUNDARY
RCE	ROCK CONSTRUCTION ENTRANCE
⊠	INLET PROTECTION
CFST-3	COMPOST FILTER SOCK SEDIMENT TRAP

- STAGE 1: CONSTRUCT CAUSEWAY AND EB BRIDGE
- STAGE 2A: CONSTRUCT CAUSEWAY AND DEMOLISH EXISTING WB PIER 3. STAGE 2B SHOWN ON NEXT SHEET
- STAGE 3A: CONSTRUCT CAUSEWAY AND DEMOLISH EXISTING EB PIER 3. STAGE 3B: CONSTRUCT CAUSEWAY AND DEMOLISH EXISTING EB PIER 4.

SCALE  
0 50 100 FEET

NOT FOR CONSTRUCTION  
PRELIMINARY PLANS ARE NOT  
TO BE SUBMITTED WITH  
PERMIT  
EROSION AND SEDIMENT  
POLLUTION CONTROL PLAN

## Krommes, Kathy

---

**From:** Krommes, Kathy  
**Sent:** Monday, December 6, 2021 10:10 AM  
**To:** Krommes, Kathy  
**Subject:** FW: [External] PA Outstanding Geologic Features  
**Attachments:** Lehigh Gorge.docx; I-80\_OutstandingGeologicFeaturesLetter\_20210119.pdf

---

**From:** Shaulis, James <jshaulis@pa.gov>  
**Sent:** Thursday, April 15, 2021 8:34 AM  
**To:** Markowitz, Katherine <Katherine.Markowitz@hdrinc.com>  
**Cc:** Reese, Stuart <streese@pa.gov>; Hand, Kristen <khand@pa.gov>; Ebersole, Craig <craebersol@pa.gov>; Schmid, Katherine <kschmid@pa.gov>  
**Subject:** RE: [External] PA Outstanding Geologic Features

**CAUTION: [EXTERNAL]** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Katie,

I thought I sent this but I didn't. Sorry for the oversight and delay. The bridge replacement project involving the I-80 Section 08B near White Haven will not have any impact on the Leigh Gorge geoheritage site you were alerted to in your PNDI search. Thank you for contacting us and providing us the opportunity to comment on this project.

Sincerely,  
Jim Shaulis

---

**From:** Markowitz, Katherine [mailto:Katherine.Markowitz@hdrinc.com]  
**Sent:** Wednesday, April 14, 2021 11:15 AM  
**To:** Shaulis, James <jshaulis@pa.gov>  
**Subject:** RE: [External] PA Outstanding Geologic Features

Hello Jim,

I wanted to follow up on this request for the I-80 bridge replacement project. Were you able to review the project information and attachments?

Thank you,

**Katie Markowitz**  
D 717-516-3155 M 201-258-0508

[hdrinc.com/follow-us](https://hdrinc.com/follow-us)

---

**From:** Markowitz, Katherine  
**Sent:** Tuesday, January 19, 2021 3:40 PM  
**To:** Shaulis, James <[jshaulis@pa.gov](mailto:jshaulis@pa.gov)>  
**Cc:** Krommes, Kathy <[Kathleen.Krommes@hdrinc.com](mailto:Kathleen.Krommes@hdrinc.com)>  
**Subject:** RE: [External] PA Outstanding Geologic Features

Hello Jim,

Please find attached a letter requesting review by the DCNR for the I-80 bridge replacement project. The letter contains project information and attachments including the project description, location, photos, and the PNDI receipt.

Thank you,

**Katie Markowitz**

**D** 717-516-3155 **M** 201-258-0508

[hdrinc.com/follow-us](http://hdrinc.com/follow-us)

---

**From:** Shaulis, James <[jshaulis@pa.gov](mailto:jshaulis@pa.gov)>  
**Sent:** Thursday, January 7, 2021 1:20 PM  
**To:** Markowitz, Katherine <[Katherine.Markowitz@hdrinc.com](mailto:Katherine.Markowitz@hdrinc.com)>  
**Subject:** RE: [External] PA Outstanding Geologic Features

**CAUTION: [EXTERNAL]** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Katie,

If could you please send me a pdf with the PNDI receipt information, location, and outline of the project I can get back to you with our comments.

Thanks,

jim

---

**From:** Markowitz, Katherine [<mailto:Katherine.Markowitz@hdrinc.com>]  
**Sent:** Thursday, January 7, 2021 12:31 PM  
**To:** Shaulis, James <[jshaulis@pa.gov](mailto:jshaulis@pa.gov)>  
**Cc:** Krommes, Kathy <[Kathleen.Krommes@hdrinc.com](mailto:Kathleen.Krommes@hdrinc.com)>  
**Subject:** [External] PA Outstanding Geologic Features

**ATTENTION:** This email message is from an external sender. Do not open links or attachments from unknown sources. To report suspicious email, forward the message as an attachment to [CWOPA\\_SPAM@pa.gov](mailto:CWOPA_SPAM@pa.gov).

Good afternoon Mr. Shaulis,

I received your name and contact information from Kelly Rossiter to request environmental review in regards to PA Outstanding Geologic Features. HDR, on behalf of PennDOT, is providing environmental services for the I-80 bridge replacement project near White Haven Borough, Luzerne County, PA, where the project traverses over the Lehigh River, Lehigh Gorge Trail, and Lehigh Gorge State Park. Please let me know what project information is needed for this request of environmental review for the project NEPA document. In this time of the pandemic, may I assume an emailed request would be appropriate?

Thank you,

**Katie Markowitz**

*Environmental Scientist*

**HDR**

4900 Ritter Road, Suite 101

Mechanicsburg, PA 17055

**D** 717-516-3155 **M** 201-258-0508

[katherine.markowitz@hdrinc.com](mailto:katherine.markowitz@hdrinc.com)

[hdrinc.com/follow-us](http://hdrinc.com/follow-us)





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BUREAU OF RECREATION AND CONSERVATION

February 22, 2022

Nicole L. Auker  
Environmental Planning Supervisor  
PA Department of Transportation  
400 North Street, 7<sup>th</sup> Floor  
Harrisburg, PA 17120

RE: I-80 WHITE HAVEN BRIDGE REPLACEMENT & GABION WALL REPLACEMENT  
SR 0080 & SR 0940 River Street  
Luzerne County, Carbon County  
LWCF 42-00033, 42-00081, Lehigh Gorge State Park

Dear Ms. Auker:

This letter is being written based on PennDOT's need to proceed with the environmental process for the above referenced bridge replacement project. The Federal Highway Administration requires acknowledgement of coordination between agencies to allow the project to continue through the environmental process, to begin the Right-Of-Way process, engage with the landowner for potential acquisition, and complete the National Park Service (NPS) Compliance and Stewardship Form (C&S Form).

The replacement of the I-80 bridge and Gabion wall along SR 0940 in East Side Borough will require permanent/TCE of 6.2 acres of impact to the Land and Water Conservation Fund protected Lehigh Gorge State Park. The impact area breakdown is as follows; 0.1 acre of Required Right-of-way, 0.4 acres of proposed Slope Easement, 0.5 acres of proposed Aerial Easement, 4.9 acres of TCE within LGSP, and 0.3 acres of TCE within DCNR Easement.

The proposed replacement property acreage is approximately 6.45 acres, Parcel ID (65A-51-C402) in Penn Forest Township. DCNR received NPS concurrence on the proposed conversion and replacement property on 11/22/2021. DCNR Bureau of State Parks, DCNR Bureau of Recreation and Conservation, and PennDOT agree on the proposed replacement property to satisfy the value, location and recreational usefulness criteria under the Land and Water Conservation Fund Act as mitigation for both the I-80 bridge replacement and the SR 0940 gabion wall replacement.

DCNR recognizes PennDOT's need to proceed through the project planning and design phases and agrees with the proposed conversion footprint and replacement property while we await NPS concurrence. Please understand NPS has ultimate approval authority of the conversion package including replacement property.

If you have questions or concerns, please contact me at (717) 783-0318 or Ashley Rebert at (717) 772-3322.

Sincerely,

*Emma Harrison*

Emma Harrison  
Land Conversion Specialist  
Bureau of Recreation and Conservation

cc.: Ashley Rebert



## Krommes, Kathy

---

**From:** Krommes, Kathy  
**Sent:** Monday, March 28, 2022 3:06 PM  
**To:** Krommes, Kathy  
**Subject:** FW: [External] I-80 Bridges conversion/Replacement properties - NPS concurrence

---

**From:** Morrison, Mary (Missy) <[Mary\\_Morrison@nps.gov](mailto:Mary_Morrison@nps.gov)>  
**Sent:** Monday, November 22, 2021 3:26 PM  
**To:** Rebert, Ashley <[arebert@pa.gov](mailto:arebert@pa.gov)>  
**Cc:** Imgrund, Lauren <[limgrund@pa.gov](mailto:limgrund@pa.gov)>  
**Subject:** [External] I-80 Bridges conversion/Replacement properties - NPS concurrence

**ATTENTION:** This email message is from an external sender. Do not open links or attachments from unknown sources. To report suspicious email, forward the message as an attachment to [CWOPA\\_SPAM@pa.gov](mailto:CWOPA_SPAM@pa.gov).

Good Afternoon Ashley-

The NPS, LWCF program has reviewed the proposed conversion footprint and replacement property for the SR0080 Section 550 conversion at LWCF 42-00033 Walter Dick Memorial Park and the I-80 White Haven Bridge, conversion replacement property materials (Impacts Lehigh Gorge State Park, Luzerne County).

### Walter Dick Mem Park

The NPS, LWCF can concur that the proposed replacement property adjacent to Walter Dick Memorial Park satisfies the LWCF Act and Post Completion Compliance Regulations (36 CFR 59.3) for appropriate replacement property.

However, please confirm the final conversion footprint acreage: I had to go back and check through Jack's emails and a February email with attachment from you indicates the conversion footprint impact to this park is 7.6 acres; within the PennDot letter of August 31st 2021, PennDOT indicates a 5.5 acre footprint and approximately 6 acres replacement property.

An email from you with the updated/explanation of what the actual conversion footprint is meant to be will suffice. Also keep in mind that when the conversion package is submitted we will need maps indicating the correct acreage.

### Lehigh Gorge SP conversion - I-80 White Haven Bridge, Impacts Lehigh Gorge State Park, Luzerne County

The NPS LWCF also concurs with the proposed replacement property at Unionville Road for the replacement of 7 acres of land converted from Recreation use at Lehigh Gorge SP meets the LWCF Act and regulations.

I sincerely apologize for the delay in reviewing the documentation for the above projects; thank you for your patience!

If you have any additional questions, please let me know.

Missy

**Missy Morrison**

State and Local Assistance Programs / Compliance Team Lead

National Park Service

1849 C Street NW

Washington, DC 20240

Mobile: (202) 641-6557

U.S. Department of  
Homeland Security

United States  
Coast Guard



Commander  
United States Coast Guard  
Fifth Coast Guard District

431 Crawford Street  
Portsmouth, VA 23704-5004  
Staff Symbol: dpb  
Phone: (757) 398-6422  
Fax: (757) 398-6334  
Email: [Crystal.k.tucker@uscg.mil](mailto:Crystal.k.tucker@uscg.mil)

16591  
18 NOV 2021

Ms. Brandy Rotz  
Pennsylvania Department of Transportation  
1002 Hamilton Street  
Allentown, PA 18101

Dear Ms. Rotz:

Coast Guard review of your proposed project as provided in your email dated August 18, 2021, is complete.

Based on the documentation provided and our research, it is determined that a Coast Guard bridge permit will not be required for the proposed highway fixed bridges – Lehigh River Bridges over Lehigh River, mile 5.7, 41.055423, -75.770451 between Luzerne and Carbon County, PA.

The project will be placed in our Coast Guard Authorization Act of 1982 exemption category for the location and structure described above and **is valid for five years from the date of this letter**. The Coast Guard Authorization Act of 1982 exempts bridge projects from Coast Guard Bridge permits when the bridge project crosses non-tidal waters which are not used, susceptible to use in their natural condition, or susceptible to use by reasonable improvement as a means to transport interstate commerce. The following conditions apply to this determination:

- a. If the construction project on the above bridge does not commence within this time, you must contact this office for reaffirmation of this determination.
- b. Future bridge projects along the above waterway will have to be independently evaluated before they may be considered for placement in the Coast Guard Authorization Act of 1982 exemption category. This includes modification, replacement and removal of the above bridge, following its initial construction.

In addition, the requirement to display navigational lighting at the aforementioned bridge is hereby waived, as per Title 33 Code of Federal Regulations, Part 118.40(b). This waiver may be rescinded at any time in the future should nighttime navigation through the proposed bridge be increased to a level determined by the District Commander to warrant lighting.

The fact that a Coast Guard bridge permit is not required does not relieve you of the responsibility for compliance with the requirements of any other Federal, State, or local agency who may have jurisdiction over any aspect of the project.

16591  
18 NOV 2021

If you have any further questions, please contact Ms. Crystal K. Tucker at the above listed address or telephone number.

Sincerely,



HAL R. PITTS  
Bridge Program Manager  
By direction

Copy: Mr. John Baumgardner, HDR Inc.  
CG Sector Delaware Bay, Waterways Management  
U. S. Army Corps of Engineers, Philadelphia District  
Federal Highways Administration, Harrisburg, PA

**Appendix F**  
**Threatened and Endangered Species**



## Pennsylvania Fish & Boat Commission

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**Division of Environmental Services**  
Watershed Analysis Section  
595 E Rolling Ridge Dr.  
Bellefonte, PA 16823

October 15, 2021

**IN REPLY REFER TO**  
SIR# 55187

HDR  
Kathleen Krommes  
4900 Ritter Road  
Suite 101  
Mechanicsburg, Pennsylvania 17055

**RE: Species Impact Review (SIR) – Rare, Candidate, Threatened and Endangered Species  
PNDI Search No.  
I-80 Bridge Replacement over Lehigh River  
CARBON County: East Side Borough, Kidder Township - LUZERNE County: Foster  
Township, White Haven Borough**

Dear Kathleen Krommes:

This responds to your inquiry about a Pennsylvania Natural Diversity Inventory (PNDI) Internet Database search “potential conflict” or a threatened and endangered species impact review. These projects are screened for potential conflicts with rare, candidate, threatened or endangered species under Pennsylvania Fish & Boat Commission jurisdiction (fish, reptiles, amphibians, aquatic invertebrates only) using the Pennsylvania Natural Diversity Inventory (PNDI) database and our own files. These species of special concern are listed under the Endangered Species Act of 1973, the Wild Resource Conservation Act, and the Pennsylvania Fish & Boat Code (Chapter 75), or the Wildlife Code.

An element occurrence of a rare, candidate, threatened, or endangered species under our jurisdiction is known from the vicinity of the proposed project. However, given the nature of the proposed project, the immediate location, or the current status of the nearby element occurrence(s), no adverse impacts are expected to the species of special concern.

This response represents the most up-to-date summary of the PNDI data and our files and is valid for two (2) years from the date of this letter. An absence of recorded species information does not necessarily imply species absence. Our data files and the PNDI system are continuously being updated with species occurrence information. Should project plans change or additional information on listed or proposed species become available, this determination may be reconsidered, and consultation shall be re-initiated.

### **Our Mission:**

[www.fish.state.pa.us](http://www.fish.state.pa.us)

*To protect, conserve and enhance the Commonwealth's aquatic resources and provide fishing and boating opportunities.*

**If you have any questions regarding this review, please contact Joshua Wisor at 814-359-5135 and refer to the SIR # 55187.** Thank you for your cooperation and attention to this important matter of species conservation and habitat protection.

Sincerely,

A handwritten signature in blue ink that reads "Joshua M. Wisor". The signature is written in a cursive style with a large initial 'J' and 'W'.

Joshua Wisor, Fisheries Biologist  
Watershed Analysis Section

HAS/JMW/dn

## 1. PROJECT INFORMATION

Project Name: **PennDOT - I-80 Lehigh River Bridges; Pathways Program**

Date of Review: **10/13/2021 12:43:51 PM**

Project Category: **Transportation, Structures and Bridges, Bridge Replacement adjacent to existing alignment (within 100 feet up/down stream)**

Project Area: **136.70 acres**

County(s): **Carbon; Luzerne**

Township/Municipality(s): **EAST SIDE; FOSTER TOWNSHIP; KIDDER TOWNSHIP; WHITE HAVEN**

ZIP Code:

Quadrangle Name(s): **HICKORY RUN; WHITE HAVEN**

Watersheds HUC 8: **Lehigh**

Watersheds HUC 12: **Black Creek-East Side of Lehigh River; Stony Creek-Lehigh River; Wright Creek-Lehigh River**

Decimal Degrees: **41.057305, -75.764172**

Degrees Minutes Seconds: **41° 3' 26.2964" N, 75° 45' 51.190" W**

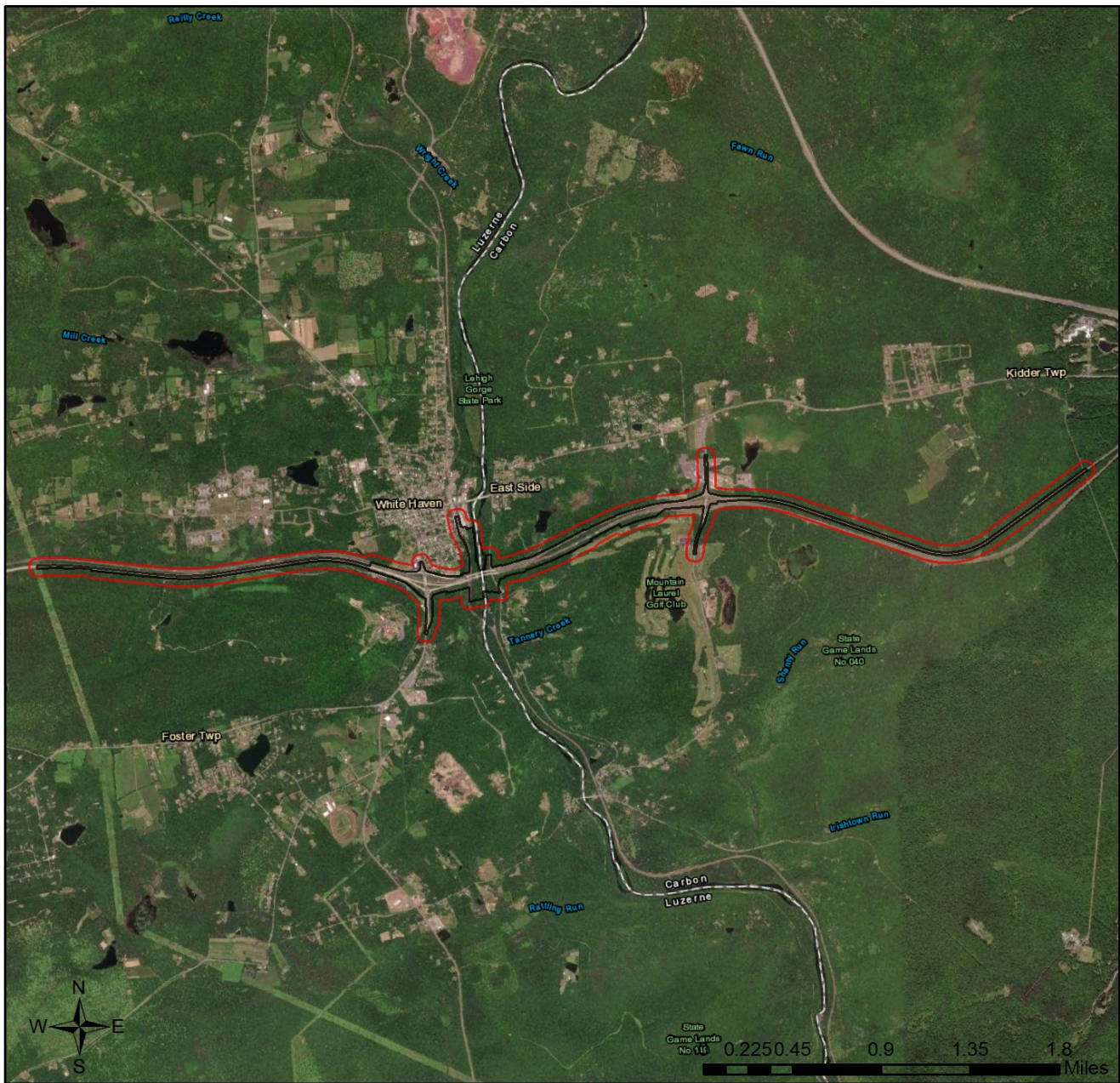
## 2. SEARCH RESULTS



Agency	Results	Response
PA Game Commission	No Known Impact	No Further Review Required
PA Department of Conservation and Natural Resources	No Known Impact	No Further Review Required
PA Fish and Boat Commission	<b>Potential Impact</b>	<b>FURTHER REVIEW IS REQUIRED, See Agency Response</b>
U.S. Fish and Wildlife Service	No Known Impact	No Further Review Required

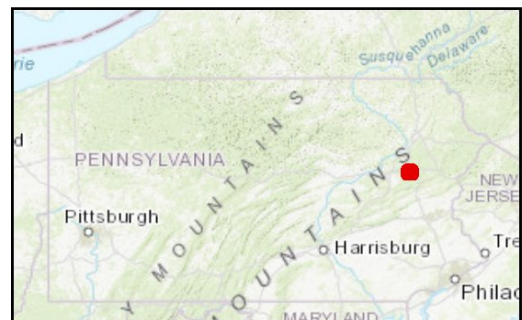
As summarized above, Pennsylvania Natural Diversity Inventory (PNDI) records indicate there may be potential impacts to threatened and endangered and/or special concern species and resources within the project area. If the response above indicates "No Further Review Required" no additional communication with the respective agency is required. If the response is "Further Review Required" or "See Agency Response," refer to the appropriate agency comments below. Please see the DEP Information Section of this receipt if a PA Department of Environmental Protection Permit is required.



## PennDOT - I-80 Lehigh River Bridges; PATHways Program

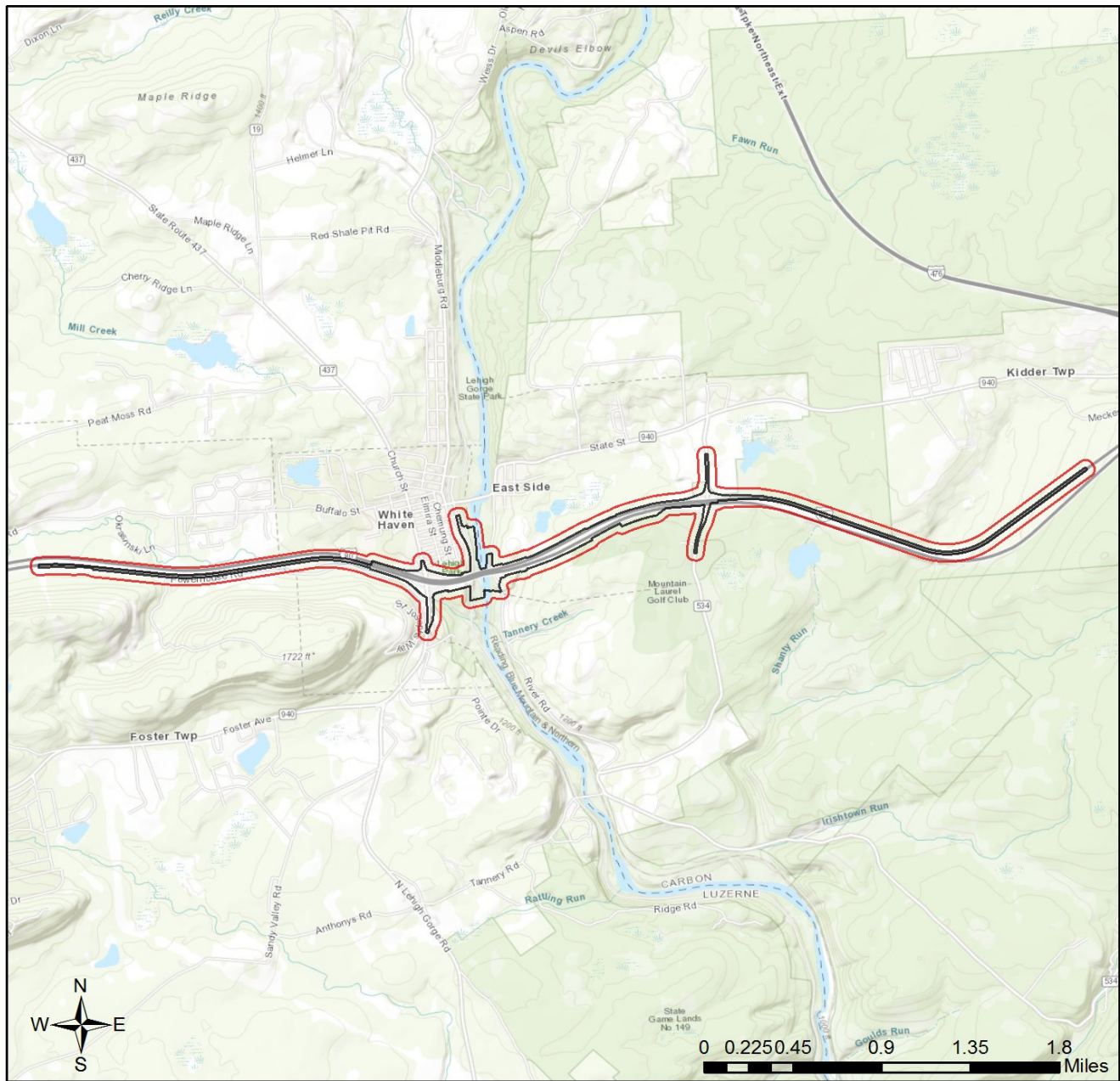


-  Project Boundary
-  Buffered Project Boundary



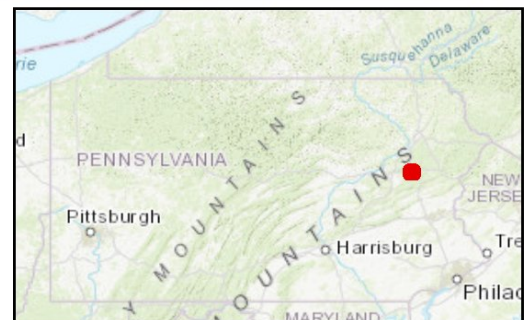
Service Layer Credits: Source: Esri, Maxar, GeoEye, Earthstar Geographics, CNES/Airbus DS, USDA, USGS, AeroGRID, IGN, and the GIS User Community  
Esri, HERE, Garmin, (c) OpenStreetMap contributors, and the GIS user community  
Sources: Esri, HERE, Garmin, Intermap, increment P Corp., GEBCO, USGS, FAO, NPS, NRCAN, GeoBase, IGN, Kadaster NL, Ordnance Survey, Esri Japan, METI, Esri China

## PennDOT - I-80 Lehigh River Bridges; PATHways Program



- Project Boundary
- Buffered Project Boundary

Service Layer Credits: Sources: Esri, HERE, Garmin, Intemap, increment P Corp., GEBCO, USGS, FAO, NPS, NRCAN, GeoBase, IGN, Kadaster NL, Ordnance Survey, Esri Japan, METI, Esri China (Hong Kong), (c) OpenStreetMap contributors, and the GIS User Community



### 3. AGENCY COMMENTS

Regardless of whether a DEP permit is necessary for this proposed project, any potential impacts to threatened and endangered species and/or special concern species and resources must be resolved with the appropriate jurisdictional agency. In some cases, a permit or authorization from the jurisdictional agency may be needed if adverse impacts to these species and habitats cannot be avoided.

These agency determinations and responses are **valid for two years** (from the date of the review), and are based on the project information that was provided, including the exact project location; the project type, description, and features; and any responses to questions that were generated during this search. If any of the following change: 1) project location, 2) project size or configuration, 3) project type, or 4) responses to the questions that were asked during the online review, the results of this review are not valid, and the review must be searched again via the PNDI Environmental Review Tool and resubmitted to the jurisdictional agencies. The PNDI tool is a primary screening tool, and a desktop review may reveal more or fewer impacts than what is listed on this PNDI receipt. The jurisdictional agencies **strongly advise against** conducting surveys for the species listed on the receipt prior to consultation with the agencies.

#### PA Game Commission

##### RESPONSE:

No Impact is anticipated to threatened and endangered species and/or special concern species and resources.

#### PA Department of Conservation and Natural Resources

##### RESPONSE:

No Impact is anticipated to threatened and endangered species and/or special concern species and resources.

#### PA Fish and Boat Commission

##### RESPONSE:

Further review of this project is necessary to resolve the potential impact(s). Please send project information to this agency for review (see WHAT TO SEND).

**PFBC Species:** (Note: The Pennsylvania Conservation Explorer tool is a primary screening tool, and a desktop review may reveal more or fewer species than what is listed below.)

Scientific Name	Common Name	Current Status
Sensitive Species**		Endangered

#### U.S. Fish and Wildlife Service

##### RESPONSE:

No impacts to **federally** listed or proposed species are anticipated. Therefore, no further consultation/coordination under the Endangered Species Act (87 Stat. 884, as amended; 16 U.S.C. 1531 et seq. is required. Because no take of federally listed species is anticipated, none is authorized. This response does not reflect potential Fish and Wildlife Service concerns under the Fish and Wildlife Coordination Act or other authorities.

\* Special Concern Species or Resource - Plant or animal species classified as rare, tentatively undetermined or candidate as well as other taxa of conservation concern, significant natural communities, special concern populations (plants or animals) and unique geologic features.

\*\* Sensitive Species - Species identified by the jurisdictional agency as collectible, having economic value, or being susceptible to decline as a result of visitation.

## WHAT TO SEND TO JURISDICTIONAL AGENCIES

If project information was requested by one or more of the agencies above, upload\* or email the following information to the agency(s) (see AGENCY CONTACT INFORMATION). Instructions for uploading project materials can be found [here](#). This option provides the applicant with the convenience of sending project materials to a single location accessible to all three state agencies (but not USFWS).

\*If information was requested by USFWS, applicants must email, or mail, project information to [IR1\\_ESPenn@fws.gov](mailto:IR1_ESPenn@fws.gov) to initiate a review. USFWS will not accept uploaded project materials.

### Check-list of Minimum Materials to be submitted:

\_\_\_ Project narrative with a description of the overall project, the work to be performed, current physical characteristics of the site and acreage to be impacted.

\_\_\_ A map with the project boundary and/or a basic site plan (particularly showing the relationship of the project to the physical features such as wetlands, streams, ponds, rock outcrops, etc.)

**In addition to the materials listed above, USFWS REQUIRES the following**

\_\_\_ **SIGNED** copy of a Final Project Environmental Review Receipt

### The inclusion of the following information may expedite the review process.

\_\_\_ Color photos keyed to the basic site plan (i.e. showing on the site plan where and in what direction each photo was taken and the date of the photos)

\_\_\_ Information about the presence and location of wetlands in the project area, and how this was determined (e.g., by a qualified wetlands biologist), if wetlands are present in the project area, provide project plans showing the location of all project features, as well as wetlands and streams.

## 4. DEP INFORMATION

The Pa Department of Environmental Protection (DEP) requires that a signed copy of this receipt, along with any required documentation from jurisdictional agencies concerning resolution of potential impacts, be submitted with applications for permits requiring PNDI review. Two review options are available to permit applicants for handling PNDI coordination in conjunction with DEP's permit review process involving either T&E Species or species of special concern. Under sequential review, the permit applicant performs a PNDI screening and completes all coordination with the appropriate jurisdictional agencies prior to submitting the permit application. The applicant will include with its application, both a PNDI receipt and/or a clearance letter from the jurisdictional agency if the PNDI Receipt shows a Potential Impact to a species or the applicant chooses to obtain letters directly from the jurisdictional agencies. Under concurrent review, DEP, where feasible, will allow technical review of the permit to occur concurrently with the T&E species consultation with the jurisdictional agency. The applicant must still supply a copy of the PNDI Receipt with its permit application. The PNDI Receipt should also be submitted to the appropriate agency according to directions on the PNDI Receipt. The applicant and the jurisdictional agency will work together to resolve the potential impact(s). See the DEP PNDI policy at <https://conservationexplorer.dcnr.pa.gov/content/resources>.

### 5. ADDITIONAL INFORMATION

The PNDI environmental review website is a preliminary screening tool. There are often delays in updating species status classifications. Because the proposed status represents the best available information regarding the conservation status of the species, state jurisdictional agency staff give the proposed statuses at least the same consideration as the current legal status. If surveys or further information reveal that a threatened and endangered and/or special concern species and resources exist in your project area, contact the appropriate jurisdictional agency/agencies immediately to identify and resolve any impacts.

For a list of species known to occur in the county where your project is located, please see the species lists by county found on the PA Natural Heritage Program (PNHP) home page ([www.naturalheritage.state.pa.us](http://www.naturalheritage.state.pa.us)). Also note that the PNDI Environmental Review Tool only contains information about species occurrences that have actually been reported to the PNHP.

### 6. AGENCY CONTACT INFORMATION

**PA Department of Conservation and Natural Resources**  
Bureau of Forestry, Ecological Services Section  
400 Market Street, PO Box 8552  
Harrisburg, PA 17105-8552  
Email: [RA-HeritageReview@pa.gov](mailto:RA-HeritageReview@pa.gov)

**U.S. Fish and Wildlife Service**  
Pennsylvania Field Office  
Endangered Species Section  
110 Radnor Rd; Suite 101  
State College, PA 16801  
Email: [IR1\\_ESPenn@fws.gov](mailto:IR1_ESPenn@fws.gov)  
NO Faxes Please

**PA Fish and Boat Commission**  
Division of Environmental Services  
595 E. Rolling Ridge Dr., Bellefonte, PA 16823  
Email: [RA-FBPACENOTIFY@pa.gov](mailto:RA-FBPACENOTIFY@pa.gov)

**PA Game Commission**  
Bureau of Wildlife Habitat Management  
Division of Environmental Planning and Habitat Protection  
2001 Elmerton Avenue, Harrisburg, PA 17110-9797  
Email: [RA-PGC\\_PNDI@pa.gov](mailto:RA-PGC_PNDI@pa.gov)  
NO Faxes Please

### 7. PROJECT CONTACT INFORMATION

Name: Kathleen I. Krommes  
Company/Business Name: HDR  
Address: 4900 Bitter Road, Suite 101  
City, State, Zip: Mechanicsburg, PA 17055  
Phone: (717) 516-3158 Fax: (717) 516-3145  
Email: kathy.krommes@hdrinc.com

### 8. CERTIFICATION

I certify that ALL of the project information contained in this receipt (including project location, project size/configuration, project type, answers to questions) is true, accurate and complete. In addition, if the project type, location, size or configuration changes, or if the answers to any questions that were asked during this online review change, I agree to re-do the online environmental review.

Kath I. Krommes  
applicant/project proponent signature

3/22/2022  
date

## 1. PROJECT INFORMATION

Project Name: **PennDOT Pathways - I-80 Lehigh River, SR 940 Gabion Wall Replacement**

Date of Review: **3/3/2022 01:21:52 PM**

Project Category: **Transportation, Roads, Other**

Project Area: **0.38 acres**

County(s): **Carbon**

Township/Municipality(s): **EAST SIDE**

ZIP Code:

Quadrangle Name(s): **WHITE HAVEN**

Watersheds HUC 8: **Lehigh**

Watersheds HUC 12: **Wright Creek-Lehigh River**

Decimal Degrees: **41.063014, -75.769088**

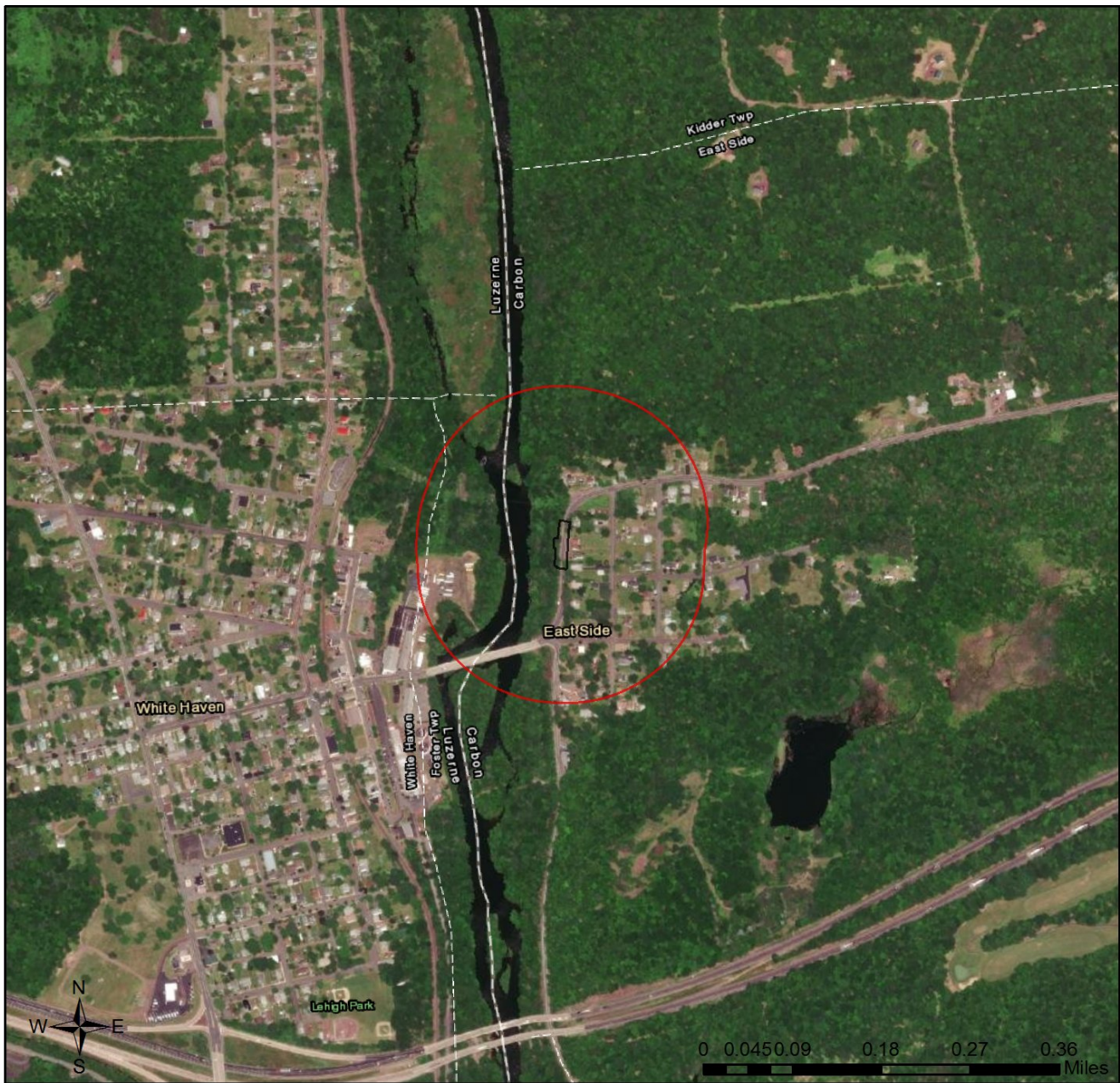
Degrees Minutes Seconds: **41° 3' 46.8505" N, 75° 46' 8.7153" W**

## 2. SEARCH RESULTS

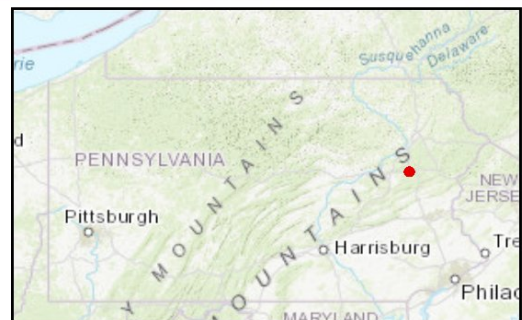
Agency	Results	Response
PA Game Commission	No Known Impact	No Further Review Required
PA Department of Conservation and Natural Resources	No Known Impact	No Further Review Required
PA Fish and Boat Commission	No Known Impact	No Further Review Required
U.S. Fish and Wildlife Service	No Known Impact	No Further Review Required

As summarized above, Pennsylvania Natural Diversity Inventory (PNDI) records indicate no known impacts to threatened and endangered species and/or special concern species and resources within the project area. Therefore, based on the information you provided, no further coordination is required with the jurisdictional agencies. This response does not reflect potential agency concerns regarding impacts to other ecological resources, such as wetlands.

# PennDOT Pathways - I-80 Lehigh River, SR 940 Gabion Wall Replacement

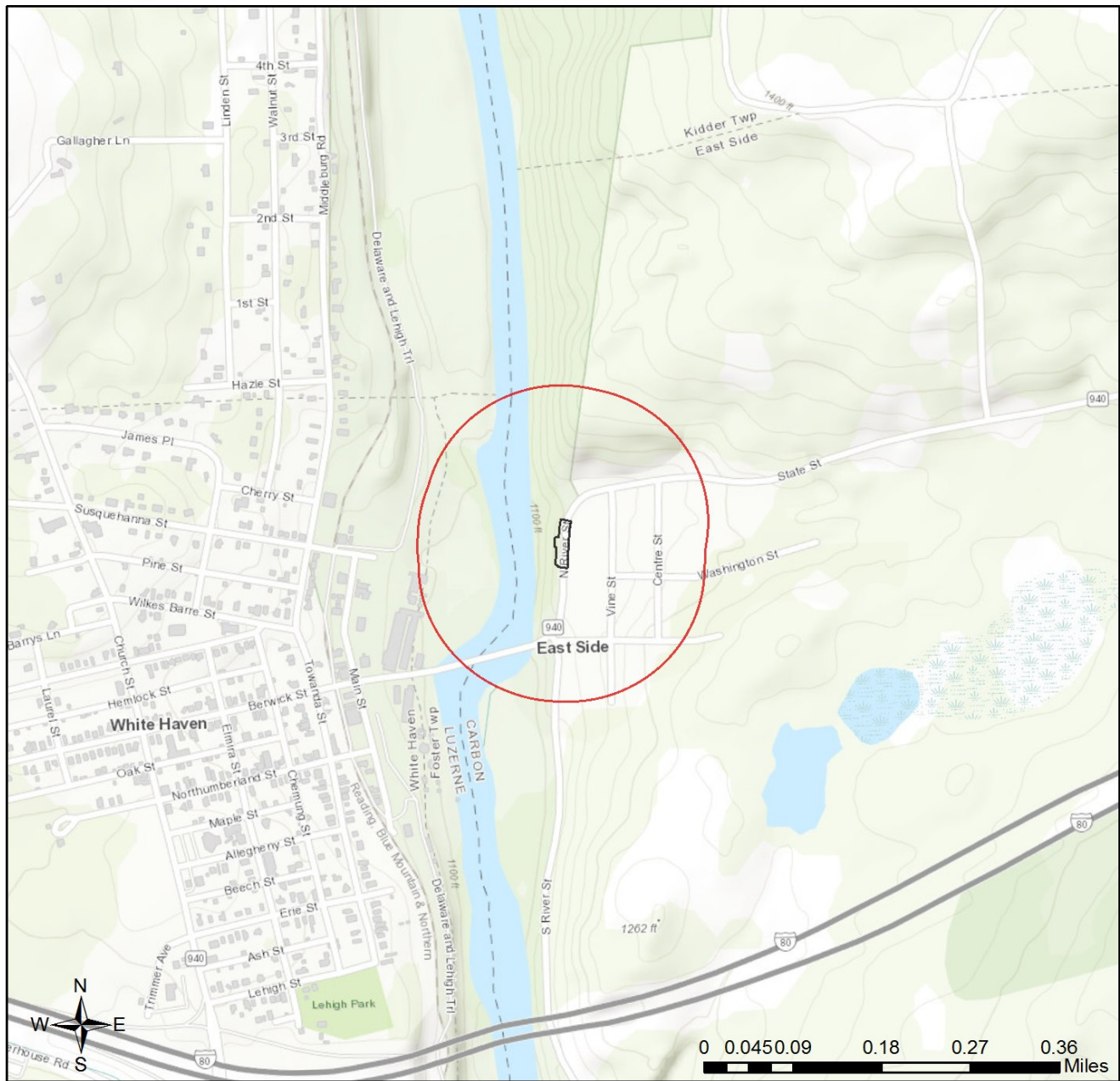


- Project Boundary
- Buffered Project Boundary



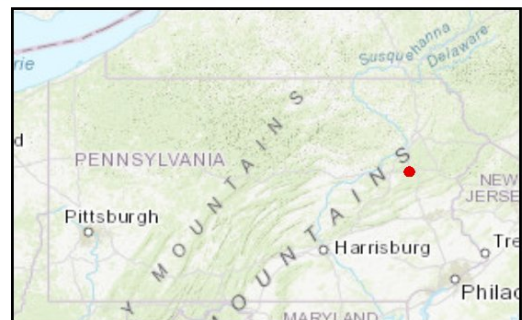
Service Layer Credits: Source: Esri, Maxar, GeoEye, Earthstar Geographics, CNES/Airbus DS, USDA, USGS, AeroGRID, IGN, and the GIS User Community  
Esri, HERE, Garmin, (c) OpenStreetMap contributors, and the GIS user community  
Sources: Esri, HERE, Garmin, Intermap, increment P Corp., GEBCO, USGS, FAO, NPS, NRCAN, GeoBase, IGN, Kadaster NL, Ordnance Survey, Esri Japan, METI, Esri China

## PennDOT Pathways - I-80 Lehigh River, SR 940 Gabion Wall Replacement



- Project Boundary
- Buffered Project Boundary

Service Layer Credits: Sources: Esri, HERE, Garmin, Intemap, increment P Corp., GEBCO, USGS, FAO, NPS, NRCAN, GeoBase, IGN, Kadaster NL, Ordnance Survey, Esri Japan, METI, Esri China (Hong Kong), (c) OpenStreetMap contributors, and the GIS User Community





## RESPONSE TO QUESTION(S) ASKED

**Q1:** The proposed project is in the range of the Indiana bat. Describe how the project will affect bat habitat (forests, woodlots and trees) and indicate what measures will be taken in consideration of this. Round acreages up to the nearest acre (e.g., 0.2 acres = 1 acre).

**Your answer is:** The project will affect 1 to 39 acres of forests, woodlots and trees.

**Q2:** Is tree removal, tree cutting or forest clearing of 40 acres or more necessary to implement all aspects of this project?

**Your answer is:** No

### 3. AGENCY COMMENTS

Regardless of whether a DEP permit is necessary for this proposed project, any potential impacts to threatened and endangered species and/or special concern species and resources must be resolved with the appropriate jurisdictional agency. In some cases, a permit or authorization from the jurisdictional agency may be needed if adverse impacts to these species and habitats cannot be avoided.

These agency determinations and responses are **valid for two years** (from the date of the review), and are based on the project information that was provided, including the exact project location; the project type, description, and features; and any responses to questions that were generated during this search. If any of the following change: 1) project location, 2) project size or configuration, 3) project type, or 4) responses to the questions that were asked during the online review, the results of this review are not valid, and the review must be searched again via the PNDI Environmental Review Tool and resubmitted to the jurisdictional agencies. The PNDI tool is a primary screening tool, and a desktop review may reveal more or fewer impacts than what is listed on this PNDI receipt. The jurisdictional agencies **strongly advise against** conducting surveys for the species listed on the receipt prior to consultation with the agencies.

#### PA Game Commission

##### RESPONSE:

No Impact is anticipated to threatened and endangered species and/or special concern species and resources.

#### PA Department of Conservation and Natural Resources

##### RESPONSE:

No Impact is anticipated to threatened and endangered species and/or special concern species and resources.

#### PA Fish and Boat Commission

##### RESPONSE:

No Impact is anticipated to threatened and endangered species and/or special concern species and resources.

#### U.S. Fish and Wildlife Service

##### RESPONSE:

No impacts to **federally** listed or proposed species are anticipated. Therefore, no further consultation/coordination under the Endangered Species Act (87 Stat. 884, as amended; 16 U.S.C. 1531 et seq. is required. Because no take of federally listed species is anticipated, none is authorized. This response does not reflect potential Fish and Wildlife Service concerns under the Fish and Wildlife Coordination Act or other authorities.

## 4. DEP INFORMATION

The Pa Department of Environmental Protection (DEP) requires that a signed copy of this receipt, along with any required documentation from jurisdictional agencies concerning resolution of potential impacts, be submitted with applications for permits requiring PNDI review. Two review options are available to permit applicants for handling PNDI coordination in conjunction with DEP's permit review process involving either T&E Species or species of special concern. Under sequential review, the permit applicant performs a PNDI screening and completes all coordination with the appropriate jurisdictional agencies prior to submitting the permit application. The applicant will include with its application, both a PNDI receipt and/or a clearance letter from the jurisdictional agency if the PNDI Receipt shows a Potential Impact to a species or the applicant chooses to obtain letters directly from the jurisdictional agencies. Under concurrent review, DEP, where feasible, will allow technical review of the permit to occur concurrently with the T&E species consultation with the jurisdictional agency. The applicant must still supply a copy of the PNDI Receipt with its permit application. The PNDI Receipt should also be submitted to the appropriate agency according to directions on the PNDI Receipt. The applicant and the jurisdictional agency will work together to resolve the potential impact(s). See the DEP PNDI policy at <https://conservationexplorer.dcnr.pa.gov/content/resources>.



### 5. ADDITIONAL INFORMATION

The PNDI environmental review website is a preliminary screening tool. There are often delays in updating species status classifications. Because the proposed status represents the best available information regarding the conservation status of the species, state jurisdictional agency staff give the proposed statuses at least the same consideration as the current legal status. If surveys or further information reveal that a threatened and endangered and/or special concern species and resources exist in your project area, contact the appropriate jurisdictional agency/agencies immediately to identify and resolve any impacts.

For a list of species known to occur in the county where your project is located, please see the species lists by county found on the PA Natural Heritage Program (PNHP) home page ([www.naturalheritage.state.pa.us](http://www.naturalheritage.state.pa.us)). Also note that the PNDI Environmental Review Tool only contains information about species occurrences that have actually been reported to the PNHP.

### 6. AGENCY CONTACT INFORMATION

**PA Department of Conservation and Natural Resources**  
Bureau of Forestry, Ecological Services Section  
400 Market Street, PO Box 8552  
Harrisburg, PA 17105-8552  
Email: [RA-HeritageReview@pa.gov](mailto:RA-HeritageReview@pa.gov)

**U.S. Fish and Wildlife Service**  
Pennsylvania Field Office  
Endangered Species Section  
110 Radnor Rd; Suite 101  
State College, PA 16801  
Email: [IR1\\_ESPenn@fws.gov](mailto:IR1_ESPenn@fws.gov)  
NO Faxes Please

**PA Fish and Boat Commission**  
Division of Environmental Services  
595 E. Rolling Ridge Dr., Bellefonte, PA 16823  
Email: [RA-FBPACENOTIFY@pa.gov](mailto:RA-FBPACENOTIFY@pa.gov)

**PA Game Commission**  
Bureau of Wildlife Habitat Management  
Division of Environmental Planning and Habitat Protection  
2001 Elmerton Avenue, Harrisburg, PA 17110-9797  
Email: [RA-PGC\\_PNDI@pa.gov](mailto:RA-PGC_PNDI@pa.gov)  
NO Faxes Please

### 7. PROJECT CONTACT INFORMATION

Name: Kathleen I. Krommes  
Company/Business Name: HDR  
Address: 4900 Ritter Road, Suite 101  
City, State, Zip: Mechanicsburg, PA 17055  
Phone: (717) 516-3158 Fax: (717) 516-3145  
Email: kathy.krommes@hdrinc.com

### 8. CERTIFICATION

I certify that ALL of the project information contained in this receipt (including project location, project size/configuration, project type, answers to questions) is true, accurate and complete. In addition, if the project type, location, size or configuration changes, or if the answers to any questions that were asked during this online review change, I agree to re-do the online environmental review.

Kath I. Krommes  
applicant/project proponent signature

3/22/2022  
date

**Appendix G**  
**Section 4(f) Evaluation**



On Behalf of the Federal Highway  
Administration—Pennsylvania Division Office

**Determination of Section 4(f) De Minimis Use**  
**Section 2002 No Adverse Use**  
**Public Parks, Recreation Areas,**  
**Wildlife and/or Waterfowl Refuges,**  
**State Forest Land, and State Game Land**  
**May 2014 Version**

<b>County:</b>	Luzerne and Carbon	<b>State Route:</b>	I-80	<b>Section:</b>	08B
<b>Project Name:</b>	I-80 Lehigh River Bridges	<b>FPN:</b>	Enter FPN.	<b>MPMS:</b>	99552

**SELECT ONE:**     EIS                       EA                       CE                       EER                       ED

**PROJECT DESCRIPTION:**

(Provide a concise but thorough description of the proposed action.)

The proposed project consists of the replacement of the I-80 Section 08B Lehigh River Bridges. The dual bridges carry I-80 over the Reading Blue Mountain Northern (RBMN) railroad, Lehigh Gorge State Park and Trail, Lehigh River, and SR 1005 (River Road), in White Haven Borough, Luzerne County, and East Side Borough and Kidder Township, Carbon County, Pennsylvania.

The purpose of the project is to address the deterioration of the I-80 Lehigh River bridges and thereby provide safe and structurally sufficient bridges that will provide connectivity for interstate travelers, commuters, commercial users, emergency services, tourists, and residents.

The needs for this project include:

- The existing I-80 dual bridges are more than 50 years old. The fracture critical bridges show deterioration and are approaching the end of their serviceable lifespan.
- I-80 provides a critical link in the interstate commerce system, and as such is vital to the efficient movement of goods and people. Further deterioration of the I-80 Lehigh River bridges could cause the bridges to be load posted.

Originally constructed in 1965, the I-80 Eastbound and Westbound structures are 836 feet long and 757 feet long, respectively. The structures consist of steel two-girder superstructures. The substructures consist of reinforced concrete abutments and reinforced concrete piers that reach heights of over 110 feet.

I-80 in the vicinity of the project has two 12' lanes of travel in both directions with 8' left shoulders and 10' right shoulders. The median width varies from 60' to 150'. This section of I-80 carries east-west traffic across Pennsylvania's northern tier with Average Daily Traffic (ADT) of 28,774 vehicles per day (2021) with 34 percent truck traffic and is projected to grow to 53,736 vehicles per day by 2046 (design year).

The Eastbound bridge will be replaced on a new alignment immediately to the south of the existing Eastbound bridge. The new Eastbound alignment will tie into the existing alignment approximately 1370 feet to the west of the bridge and 1310 feet to the east of the bridge. The



On Behalf of the Federal Highway  
Administration—Pennsylvania Division Office

**Determination of Section 4(f) De Minimis Use**  
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**Public Parks, Recreation Areas,**  
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Westbound bridge will be replaced on the same approximate alignment as the existing Westbound bridge. At Exit 273 (SR 940), the Eastbound on-ramp will be reconstructed on a new alignment shifted south of existing, and the Westbound off-ramp will be reconstructed on the same approximate alignment as the existing ramp.

The project includes the extension and repair of the existing cast-in-place reinforced concrete box culvert which carries an Unnamed Tributary to the Lehigh River under I-80, approximately 1000 feet to the east of the Lehigh River bridges. The existing wing walls and approximately 8 feet of the existing culvert will be removed and replaced with a new section of 39- and 1/2-foot-long reinforced box culvert.

The project also includes the incorporation of an open road cashless tolling system, including a new toll gantry on I-80 Eastbound, a utility shed, utility service, and advance signing. The limits of the work for the cashless tolling system and associated features overlap and extend beyond the limits of work for the bridge replacement project. The original project study area is shown on Figure 1. The expanded project study area after addition of the tolling facility to the project is shown on Figure 2.

**IDENTIFICATION OF SECTION 4(f)/SECTION 2002 PROPERTY:**

(List the property and provide a description of the property as per Chapter 6 of the *Section 4(f)/Section 2002 Handbook*. Attach a map, photo(s), etc. as appropriate.)

The stretch of the Lehigh River within the project study area is considered a navigable waterway by the U.S. Coast Guard (USCG) and a Water Trail designated by Pennsylvania Fish and Boat Commission (PFBC). Water based recreation, including recreational boating, is conducted along the 72-mile-long Lehigh River Water Trail, which emphasizes low-impact use and promotes stewardship of water resources. The 41-mile Northern section of the Lehigh River Water Trail begins at the White Haven boat access underneath the I-80 Lehigh River bridges within the Lehigh Gorge State Park. Figure 3 shows the Lehigh River in the vicinity of the I-80 bridges.

The stretch of the Lehigh River within the project study area is also designated as a Pennsylvania Scenic River by Pennsylvania Department of Conservation and Natural Resources (DCNR), with the scenic designation from the Francis E. Walter Dam north of the project to Jim Thorpe to the south. This designation encompasses a 32-mile upper section of the Lehigh River and is intended to help preserve the primitive qualities, as well as the natural aesthetic values, of our rivers and adjacent land environment. Designated rivers shall be free-flowing and capable of supporting both aquatic life and water-based recreation.



On Behalf of the Federal Highway  
Administration—Pennsylvania Division Office

**Determination of Section 4(f) De Minimis Use**  
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Approximately 650 linear feet of the Lehigh River extend within the project study area. The water trail's White Haven boat access is located underneath the I-80 Lehigh River Bridges on the west shore of the river within Lehigh Gorge State Park. Although not published on the Lehigh River Water Trail map, there is an access point on the east shore of the river within Lehigh Gorge State Park south of the project study area. This access is used by one of the whitewater outfitters permitted by DCNR to operate within the Lehigh Gorge State Park. The other permitted outfitters and the general public use the White Haven boat access on the west shore under the I-80 Lehigh River bridges.

The river is used for both recreational boating and fishing. The segment of the Lehigh River within the project study area is classified as High Quality-Cold Water Fishes and Migratory Fishes (HQ-CWF, MF) according to the PA Code, Title 25, Chapter 93. It is also designated by PFBC as both an approved trout stream and naturally reproducing trout stream. These designations would prohibit in-stream construction from February 15 to June 1 and October 1 to December 31, respectively.

Photographs of Lehigh River and boat launch area are included in Attachment B.

**FOR PARKS, IDENTIFY KEY COMPONENTS OF ANY EXISTING MANAGEMENT PLAN (if it exists):**

In 2003, Wildlands Conservancy completed the Lehigh River Watershed Conservation Management Plan, which resulted in the Lehigh River's listing on the Pennsylvania DCNR's Rivers Conservation Program in February 2005. Through this process, the Lehigh River Watershed Conservation Management Plan was developed. The purpose of the Conservation Management Plan is to: identify the cultural, natural, biological, historical, and recreational resources of the watershed; identify problems; seek solutions; and list recommendations for the preservation, protection, and enhancement of the Lehigh River and its watershed. The Conservation Management Plan was updated in 2018 and provided the opportunity to compile the conservation accomplishments the watershed community has achieved in the 15 years since the original Conservation Management Plan was developed.

The PFBC developed the Lehigh River Fisheries Management Plan in 2007, which was designed to guide future fisheries management actions, including environmental protection efforts, of the PFBC, based on biological, environmental, and social data. The goal of this fisheries management plan is to protect, conserve, and where possible, enhance the fishery of the Lehigh River.



On Behalf of the Federal Highway  
Administration—Pennsylvania Division Office

**Determination of Section 4(f) De Minimis Use**  
**Section 2002 No Adverse Use**  
**Public Parks, Recreation Areas,**  
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**OFFICIAL WITH JURISDICTION OVER SECTION 4(f)/SECTION 2002 PROPERTY:**

1. Identify agency with jurisdiction:  
Pennsylvania Fish and Boat Commission (PFBC) for Water Trail; Pennsylvania Department of Conservation and Natural Resources (DCNR) for PA Scenic River System
2. Name and title of contact person at agency:  
Paul Fawcett, Chief, Waterways and Marina Management Branch, PFBC  
Kelly Rossiter, AICP, Rivers Program Specialist, DCNR

**APPLICABILITY DETERMINATION:**

1. Provide the total acreage of the property: The Lehigh River Water Trail is 72 miles long from White Haven to the river's confluence with the Delaware River in Easton. Approximately 650 linear feet of the Lehigh River extend within the project study area.

Describe the use of land from the property (identify amount of the property to be used, including temporary and permanent acquisition):

The anticipated layout of the replacement I-80 Lehigh River Bridges shown on Figure 4 would place two piers within the Lehigh River delineated ordinary high water mark. One pier of each bridge is anticipated to be placed along the western edge of and extending into the river (approximately 230 square feet total within the river). The existing bridges have 4 piers in the river (approximately 600 square feet total). The final determination of the pier locations will be dependent on the contractor's selection of bridge structure type; however, no piers will be allowed to be placed in the middle portion of the river shown on Figure 5. By restricting the contractor from placing piers in the center of the river, the permanent pier footprint in the river is expected to be less than or equal to existing. Conceptual structure plans are included in Attachment C.

Approximately 500 linear feet (1.5 acres) of temporary impact to the Lehigh River is anticipated due to temporary causeways needed to construct the bridges. Temporary causeways will extend from both riverbanks but will not extend completely across the river allowing for safe passage of recreational boat traffic through the project construction site. A series of causeways is proposed for staged construction lasting approximately four years.





On Behalf of the Federal Highway  
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**Determination of Section 4(f) De Minimis Use**  
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Attachment C includes the preliminary Aids to Navigation (ATON) Plan, which shows the proposed causeways and staging.

The existing boat launch for the Lehigh River Water Trail is underneath the existing I-80 eastbound bridge within an existing aerial easement in Lehigh Gorge State Park. DCNR has initiated a capital improvements project in Lehigh Gorge State Park (LGSP) that started in 2021 and is expected to be completed in 2022, prior to PennDOT's bridge replacement project. DCNR's project will construct a new LGSP entrance from SR 940 south of the I-80 bridges. In addition to the new entrance, improvements will include four boat launches, parking lots, and flush facilities. DCNR intends for all park vehicular traffic, including commercial boating outfitters, to utilize the new park entrance from SR 940 and the new boat launches. Anticipated temporary impacts to park property and facilities, including the future facilities, are shown on Figure 6. Temporary impacts specific to the Lehigh River and access include:

#### Lehigh River:

- Temporary causeways will extend from both riverbanks but will not extend completely across the river.
- Boat traffic through the bridge construction site will be maintained for most of construction. Temporary closures of boat traffic will be allowed for certain activities, including bridge demolition, construction of piers near or within the river, and setting of beams.
- An Aids to Navigation (ATON) Plan will be implemented and will include advance signing. Signs will be placed at boat launches upstream of the project. Additional advanced notification to PFBC, DCNR and the public will be provided.

#### Boat launches:

- The boat launch directly under the bridges will be closed during construction. The planned improvements in this area, including upgraded boat launch, turnaround and handicap parking will be disturbed and will be reconstructed at the end of the project. These facilities may need to be reconfigured depending upon the chosen bridge pier location. Any such reconfiguration will be coordinated with DCNR.
- The three future boat launches to the south (downstream) of the bridges will not be impacted and will remain open during construction. These boat launches can be used for commercial outfitters and general public during construction.



On Behalf of the Federal Highway  
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**Determination of Section 4(f) De Minimis Use**  
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A preliminary ATON plan was prepared and submitted to PFBC for review. PFBC approved the ATON plan on August 16, 2021. The ATON plan and approval letter is provided in Attachment C. Exclusion buoys are to be placed along the edges of the causeways to indicate area for safe passage of river traffic. Signs will be placed upstream and downstream of the bridges to warn boaters of the bridge construction site. If the ATON plan is revised by the contractor, additional coordination with PFBC is required.

2. The project **does not** adversely affect the activities, features, and attributes of the resource that qualify it for protection under Section 4(f) or Section 2002. **(If this statement cannot be verified as true, de minimis/no adverse use does not apply.)**  YES

Describe the effect to the qualities, activities, features, or attributes of the resource that qualify it for protection. Include a description of any mitigation included when making the determination regarding effects to the resource:

The project will not adversely affect the Lehigh River Water Trail/PA Scenic River. The river will remain open for recreational boating through the bridge construction site during most of the bridge construction (except for temporary closures noted below), and access to the Lehigh River will be maintained via the three new boat launches currently being constructed by DCNR south of the bridges (the existing boat launch under the bridge will be temporarily closed). The contractor will restore disturbed areas, and full access to the park amenities, including the boat launches, will be provided upon completion.

Mitigation measures will include:

- An Aids to Navigation Plan will be implemented and will include advance signing. Signs will be placed at boat launches upstream of the project. Additional advanced notification to PFBC, DCNR and the public will be provided.
- Temporary closures of boat traffic will be allowed for certain activities, including bridge demolition, construction of piers near or within the river, and setting of beams.
- If necessary due to pier locations, reconfiguration of the driveway, parking, or boat launches will be coordinated with DCNR.



On Behalf of the Federal Highway  
Administration—Pennsylvania Division Office

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- The contractor will restore disturbed areas, and full access to the park amenities will be provided upon completion. As part of restoring the disturbed areas, a planting plan will be prepared in coordination with DCNR during Final Design.

In addition, due to the PA Scenic Rivers designation, PennDOT has agreed to incorporate the following actions into the project:

- Vegetative buffers will be provided to the extent practicable.
  - Piers will not be allowed in the middle portion of the river, as shown on Figure 5.
  - Steel girders, if used on the new bridge, will be weathering steel or painted a “natural” color to blend with the setting.
  - Where possible, riprap will be covered with soil and vegetation.
  - Scenic river signs will be placed at the leading ends of each bridge. Signs will also be placed on the bridge and/or piers for water trail users.
3. The public was afforded an opportunity to review and comment on the effects of the project on the protected activities, features, and attributes of the resource.  YES

Identify the opportunity(ies) for public comment and describe the input received (provide attachments as appropriate to document the public involvement activity):

A public notice was prepared and included a map of LGSP amenities and a written notice describing the anticipated impacts to LGSP and the Lehigh River Water Trail (Attachment D). The notice requested public input and provided an email and physical address for comments. On September 24, 2021, laminated copies of the public notice were delivered to DCNR for posting in LGSP and nearby Hickory Run State Park. On the same day, the public notice was emailed to White Haven Borough, East Side Borough, Kidder Township, D&L National Heritage Corridor, and the whitewater and bicycle outfitters that operate in the park. Additionally, the public notice was handed out to local businesses in White Haven Borough. Upon the suggestion of LGSP Manager, Rex Bradish, a newspaper advertisement was published in the local newspaper, the Journal-Herald. Proof of publication in the October 7, 2021, and October 14, 2021 issues of the Journal-Herald are included in Attachment D.

The public comment period began September 24, 2021 and ended October 26, 2021. Three comments were received during this 30-day comment period. Only one comment was related to Section 4(f), LGSP, and the potential impacts to park amenities and visitors. The



On Behalf of the Federal Highway Administration—Pennsylvania Division Office

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comment from a local resident and business owner included concerns of negative affects to White Haven Borough’s local economy due to a four-year closure of the Lehigh Gorge Trail and requested highway signs to the downtown business district. Comments are included in Attachment D.

LGSP’s Manager has reported that this comment and request for signs was voiced at a prior public meeting conducted for DCNR’s LGSP improvement project, which moves the state park main entrance from its current location in downtown White Haven to SR 940 south of I-80. PennDOT will coordinate with DCNR and White Haven Borough to provide wayfinding signs.

In addition to the public outreach conducted September 24, 2021 to October 26, 2021, one written comment related to the Lehigh Gorge Trail was received from D&L National Heritage Corridor during the NEPA public meeting and comment period November 17, 2021 to December 17, 2021. PennDOT will continue coordination with D&L National Heritage Corridor as the project progresses. The full comment is included in Attachment D.

- 4. The official with jurisdiction over the property was informed of FHWA's and/or PennDOT's intent to make a *de minimis*/no adverse use finding.  YES

Identify the method used to notify the official with jurisdiction, and attach appropriate correspondence.

Letters describing the impacts to Lehigh River Water Trail/PA Scenic River and PennDOT’s intention of making a Section 4(f) De Minimis Use determination were provided to PFBC and DCNR. Mitigation measures were detailed in the letters. Signed copies of the letters are attached to this form.

Correspondence documenting notification of the official with jurisdiction is included in the following Attachment: PFBC Section 4(f) De Minimis Agreement Letter dated 2/25/2022; DCNR Section 4(f) De Minimis Agreement Letter dated 2/28/2022

- 5. The official with jurisdiction over the property concurred in writing with FHWA's and/or PennDOT's determination that the project will not adversely affect the property. (NOTE: Public input must be received and considered prior to the official with jurisdiction making a final determination.)  YES



On Behalf of the Federal Highway Administration—Pennsylvania Division Office

**Determination of Section 4(f) De Minimis Use**  
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Identify the official with jurisdiction and date of concurrence and attach written concurrence:

Paul Fawcett, Chief, Waterways and Marina Management Branch, PFBC  
 Kelly Rossiter, AICP, Rivers Program Specialist, DCNR

Written concurrence from the official with jurisdiction is included in the following Attachment:  
 PFBC Section 4(f) De Minimis Agreement Letter dated 2/25/2022; DCNR Section 4(f) De Minimis Agreement Letter dated 2/28/2022

6. If the Section 4(f)/Section 2002 use involves State Game Land, verify that the use is considered *de minimis* in accordance with the *Cooperative Interagency Agreement for Interdepartmental Land Transfer of State Game Lands*. (Describe and obtain PA Game Commission concurrence signature below for use of a State Game Land Bank and/or Interdepartmental Land Transfer.)  YES

**State Game Land Bank**

Debiting [Click here to enter text.](#) (acres)

From [Click here to enter text.](#) SGL bank

PGC Signature:

Date: [Click here to enter a date.](#)

**Interdepartmental Land Transfer**

PGC Signature:

Date: [Click here to enter a date.](#)

7. Have Federal or State funds [LWCF 6(f)/Project 70/Project 500/other recreation grants] been used in the acquisition of, or for any improvements to, the Section 4(f) property?  YES  NO

If Yes, the appropriate Federal agency has been coordinated with and is in agreement with the land conversion or transfer.  YES

**Provide more information regarding the Section 6(f)/Project 70/Project 500/other recreation grants coordination:**

Construction of the project will involve construction activities around, over and in the Lehigh Gorge State Park. DCNR has confirmed that LWCF funds (42-00081) were used to purchase land for the state park. The entire state park is afforded protection under Section 6(f). The Section 4(f) Evaluation for the Lehigh Gorge State Park is documented in a



On Behalf of the Federal Highway  
Administration—Pennsylvania Division Office

**Determination of Section 4(f) De Minimis Use  
Section 2002 No Adverse Use  
Public Parks, Recreation Areas,  
Wildlife and/or Waterfowl Refuges,  
State Forest Land, and State Game Land  
May 2014 Version**

<b>County:</b>	Luzerne and Carbon	<b>State Route:</b>	I-80	<b>Section:</b>	08B
<b>Project Name:</b>	I-80 Lehigh River Bridges	<b>FPN:</b>	Enter FPN.	<b>MPMS:</b>	99552

separate *Determination of Section 4(f) De Minimis Use/ Section 2002 No Adverse Use Public Parks, Recreation Areas, Wildlife and/or Waterfowl Refuges, State Forest Land, and State Game Land Form.*

8. The project does not involve any uses that would require an individual Section 4(f) evaluation. (It is acceptable if there are other Section 4(f) uses that are covered by one of the nationwide programmatic Section 4(f) evaluations or meet temporary occupancy criteria.)  YES

If there are other Section 4(f) properties used, list them here, briefly describe the use, and identify which form(s) will be completed to address the use:

Construction of the project will involve construction activities around, over and in the Lehigh Gorge State Park. The Section 4(f) Evaluation for the Lehigh Gorge State Park is documented in a separate *Determination of Section 4(f) De Minimis Use/ Section 2002 No Adverse Use Public Parks, Recreation Areas, Wildlife and/or Waterfowl Refuges, State Forest Land, and State Game Land Form.*

In accordance with PA Act 120 Section 2002 requirements, briefly summarize the impacts to other Section 2002 areas of concern that would occur if the use of the public park, recreation area, or wildlife or waterfowl refuge was avoided. Other Section 2002 areas of concern to be discussed could include the following:

(1) residential and neighborhood character and location, (2) conservation including air, erosion, sedimentation, wildlife and general ecology of area, (3) noise, and air and water pollution, (4) multiple use of space, (5) replacement housing, (6) displacement of families and business, (7) aesthetics, (8) public health and safety, (9) fast, safe and efficient transportation, (10) civil defenses, (11) economic activity, (12) employment, (13) fire protection, (14) public utilities, (15) religious institutions, (16) conduct and financing of government including the effect on the local tax base and social service costs, (17) property values, (18) education, including the disruption of school district operations, (19) engineering, right-of-way and construction costs of the project and related facilities, (20) maintenance and operating costs of the project and related facilities, and (21) operation and use of existing transportation routes and programs during construction and after completion.

The project will replace the I-80 Section 08B, Lehigh River Bridges, which span over the Lehigh River Water Trail and PA Scenic River. To meet the project's purpose and need, there is no alternative that would avoid the use of the Lehigh River Water Trail and PA Scenic River. The No-Build Alternative would impact public health and safety as the existing I-80 dual bridges are more than 50 years old and are approaching the end of their



On Behalf of the Federal Highway  
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**Determination of Section 4(f) De Minimis Use**  
**Section 2002 No Adverse Use**  
**Public Parks, Recreation Areas,**  
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**State Forest Land, and State Game Land**  
**May 2014 Version**

<b>County:</b>	Luzerne and Carbon	<b>State Route:</b>	I-80	<b>Section:</b>	08B
<b>Project Name:</b>	I-80 Lehigh River Bridges	<b>FPN:</b>	Enter FPN.	<b>MPMS:</b>	99552

service life. Vehicular traffic and interstate commerce would be impacted as further deterioration of the I-80 Lehigh River Bridges could cause the bridges to be load posted.

Ecological Services Evaluation

As part of DCNR's review under the Environmental Rights Amendment, the following mitigation addresses ecological and other impacts associated with the Lehigh River Water Trail and PA Scenic River:

- The I-80 Lehigh River Bridges are nearing the end of their serviceable life. This means the bridges will need ever increasing inspections and repairs. These repairs have the potential to interrupt access to the park and to the boat launch area under the bridges. If the condition of the bridges continues to deteriorate, they may need to be weight posted, or at some point be closed. Much of the tourist traffic headed to LGSP and this stretch of the Lehigh Water Trail/PA Scenic River uses I-80 to get to the park, making replacement of the bridges important not just for interstate commerce, but also for tourism and park and river access.
- PennDOT is responsible for providing safe and reliable facilities for the traveling public. The replacement of the I-80 Lehigh River Bridges is needed to maintain a safe and reliable crossing of the Lehigh River not only for those traveling on the interstate, but also for those biking, hiking, rafting, kayaking and fishing in the vicinity of the bridge within LGSP, the Lehigh Gorge Trail, and the Lehigh River Water Trail/PA Scenic River.
- An Aids to Navigation Plan will be implemented and will include advanced signing. Signs will be placed at boat launches upstream of the project. Additional advanced notification to PFBC, DCNR and the public will be provided.
- Due to the PA Scenic Rivers designation, PennDOT has agreed to incorporate the following actions into the project:
  - Vegetative buffers will be provided to the extent practicable.
  - Piers will not be allowed in the middle portion of the river, as shown on Figure 5.
  - Steel girders, if used on the new bridge, will be weathering steel or painted a "natural" color to blend with the setting.
  - Where possible, riprap will be covered with soil and vegetation.
  - Scenic river signs will be placed at the leading ends of each bridge. Signs will also be placed on the bridge and/or piers for water trail users.



On Behalf of the Federal Highway  
Administration—Pennsylvania Division Office

**Determination of Section 4(f) De Minimis Use**  
**Section 2002 No Adverse Use**  
**Public Parks, Recreation Areas,**  
**Wildlife and/or Waterfowl Refuges,**  
**State Forest Land, and State Game Land**  
**May 2014 Version**

<b>County:</b>	Luzerne and Carbon	<b>State Route:</b>	I-80	<b>Section:</b>	08B
<b>Project Name:</b>	I-80 Lehigh River Bridges	<b>FPN:</b>	Enter FPN.	<b>MPMS:</b>	99552

- Replacement land is being provided for the Section 6(f) conversion (Required Right-of-Way, Proposed Slope Easement, Proposed Aerial Easements, and Temporary Construction Easement) within LGSP totaling approximately 6 acres. This replacement land also serves as mitigation of the Section 4(f) use of LGSP. The replacement land being provided is forested land; therefore, the replacement land offsets the removal of trees in the construction staging area.
- The contractor will restore disturbed areas, and full access to the park amenities, including boat launches, will be provided upon project completion. As part of restoring the disturbed areas, a planting plan will be prepared in coordination with DCNR during final design.
- The area within proposed aerial easements adjacent to the bridges will be available for use by LGSP after construction.
- The majority of the state park will remain open during bridge construction, and access to the Lehigh River Water Trail/PA Scenic River will be maintained via the three new boat launches currently being constructed by DCNR south of the bridges (the existing boat launch under the bridge will be temporarily closed). Efforts will be made to minimize effects to the park and Lehigh River Water Trail/PA Scenic River throughout construction.
- If necessary due to pier locations, reconfiguration of the driveway, parking, or boat launches will be coordinated with DCNR.
- Access to and use of the Lehigh Gorge Trail will be maintained to the extent possible during construction:
  - April through October: Overnight closure of the trail (during hours that the state park is closed) will be allowed. Short-term closure of the trail (up to 15 minutes) will be allowed during daylight hours Monday through Friday using flaggers and advance signage.
  - November through March: Long-term closure of the trail will be allowed for certain construction activities including bridge superstructure demolition, pier demolition, pier construction, and setting of beams and deck forms, with consideration of working room and safety.
  - Advance notice of trail closures will be provided to DCNR, D&L National Heritage Corridor, the Borough of White Haven, and the public.





On Behalf of the Federal Highway  
Administration–Pennsylvania Division Office

**Determination of Section 4(f) *De Minimis* Use**  
**Section 2002 No Adverse Use**  
**Public Parks, Recreation Areas,**  
**Wildlife and/or Waterfowl Refuges,**  
**State Forest Land, and State Game Land**  
**May 2014 Version**

<b>County:</b>	Luzerne and Carbon	<b>State Route:</b>	I-80	<b>Section:</b>	08B
<b>Project Name:</b>	I-80 Lehigh River Bridges	<b>FPN:</b>	Enter FPN.	<b>MPMS:</b>	99552

- The contractor may temporarily realign the trail through the project site if approved by DCNR. A 10’ wide trail will be maintained except as needed for closures noted above.
- PennDOT will coordinate with DCNR and White Haven Borough to provide wayfinding signs



On Behalf of the Federal Highway  
Administration—Pennsylvania Division Office

**Determination of Section 4(f) De Minimis Use**  
**Section 2002 No Adverse Use**  
**Public Parks, Recreation Areas,**  
**Wildlife and/or Waterfowl Refuges,**  
**State Forest Land, and State Game Land**  
**May 2014 Version**

<b>County:</b>	Luzerne and Carbon	<b>State Route:</b>	I-80	<b>Section:</b>	08B
<b>Project Name:</b>	I-80 Lehigh River Bridges	<b>FPN:</b>	Enter FPN.	<b>MPMS:</b>	99552

**Include any additional information related to the park impact that is relevant to the determination of *de minimis*/no adverse use:**

Click here to enter text.

**SUMMARY AND DETERMINATION:**

The project involves a *de minimis*/no adverse use on the Section 4(f)/Section 2002 property as evidenced through the minimization of harm to a public park, recreation land, or wildlife and waterfowl refuge as a result of mitigation to or avoidance of impacts to the qualifying characteristics and/or the functions of the resource. Based on the scope of the undertaking; the fact that the undertaking does not adversely affect the function/qualities of the Section 4(f)/Section 2002 property on a permanent or temporary basis; and with agreement from the official with jurisdiction, the proposed action constitutes a *de minimis*/no adverse use; and therefore, no analysis of avoidance alternatives is required.

Name and Organization of Preparer: Kathleen Krommes, HDR

Date: 2/16/2022

Project Manager: *Brandy L Rotz*

Date: 03/02/2022

Environmental Manager: *Shant J Neal*

Date: 3-2-22

PennDOT, BOPD: **Nina Ertel**

Digitally signed by Nina Ertel  
Date: 2022.03.08 08:31:34 -05'00'

Date:

FHWA: **CAMILLE A OTTO**

Digitally signed by CAMILLE A  
OTTO  
Date: 2022.03.18 08:48:30 -04'00'

Date:

**LIST OF ATTACHMENTS**

**Attachment A: Figures**

Figure 1 – Project Location Map, Original Project Study Area

Figure 2 – Project Location Map, Expanded Project Study Area

Figure 3 – Section 4(f) / 6(f) Location Map

Figure 4 – Lehigh River Water Trail and PA Scenic River Impact Map

Figure 5 – Pier Restriction Area

Figure 6 – Temporary Closure to Lehigh Gorge State Park, Public Notice

**Attachment B: Photographs**

**Attachment C: Design Plans**

Structure Plans (CTS&L)

ATON Plan and PFBC Approval

**Attachment D: Public Outreach**

Public Notice Flyer, Temporary Closure to Lehigh Gorge State Park

Newspaper Advertisement Proof of Publication



On Behalf of the Federal Highway  
Administration–Pennsylvania Division Office

**Determination of Section 4(f) De Minimis Use  
Section 2002 No Adverse Use  
Public Parks, Recreation Areas,  
Wildlife and/or Waterfowl Refuges,  
State Forest Land, and State Game Land  
May 2014 Version**

<b>County:</b>	Luzerne and Carbon	<b>State Route:</b>	I-80	<b>Section:</b>	08B
<b>Project Name:</b>	I-80 Lehigh River Bridges	<b>FPN:</b>	Enter FPN.	<b>MPMS:</b>	99552

Comments Received

Attachment E: Concurrence Letters

PFBC Section 4(f) De Minimis Agreement Letter, dated 2/25/2022

DCNR Section 4(f) De Minimis Agreement Letter, dated 2/28/2022



On Behalf of the Federal Highway  
Administration—Pennsylvania Division Office

**Determination of Section 4(f) De Minimis Use**  
**Section 2002 No Adverse Use**  
**Public Parks, Recreation Areas,**  
**Wildlife and/or Waterfowl Refuges,**  
**State Forest Land, and State Game Land**  
**May 2014 Version**

<b>County:</b>	Luzerne and Carbon	<b>State Route:</b>	I-80	<b>Section:</b>	08B
<b>Project Name:</b>	I-80 Lehigh River Bridges	<b>FPN:</b>	Enter FPN.	<b>MPMS:</b>	99552

**List Section 4(f) mitigation measures associated with this use that are part of this project:**

Mitigation measures will include:

- An Aids to Navigation Plan will be implemented and will include advance signing. Signs will be placed at boat launches upstream of the project. Additional advanced notification to PFBC, DCNR and the public will be provided.
- Temporary closures of boat traffic will be allowed for certain activities, including bridge demolition, construction of piers near or within the river, and setting of beams.
- If necessary due to pier locations, reconfiguration of the driveway, parking, or boat launches will be coordinated with DCNR.
- The contractor will restore disturbed areas, and full access to the park amenities will be provided upon completion. As part of restoring the disturbed areas, a planting plan will be prepared in conjunction with DCNR during Final Design.

In addition, due to the PA Scenic Rivers designation, PennDOT has agreed to incorporate the following actions into the project:


- Vegetative buffers will be provided to the extent practicable.
- Piers will not be allowed in the middle portion of the river, as shown on Figure 5.
- Steel girders, if used on the new bridge, will be weathering steel or painted a “natural” color to blend with the setting.
- Where possible, riprap will be covered with soil and vegetation.
- Scenic river signs will be placed at the leading ends of each bridge. Signs will also be placed on the bridge and/or piers for water trail users.

**Typical attachments for this form include, but are not limited to:**

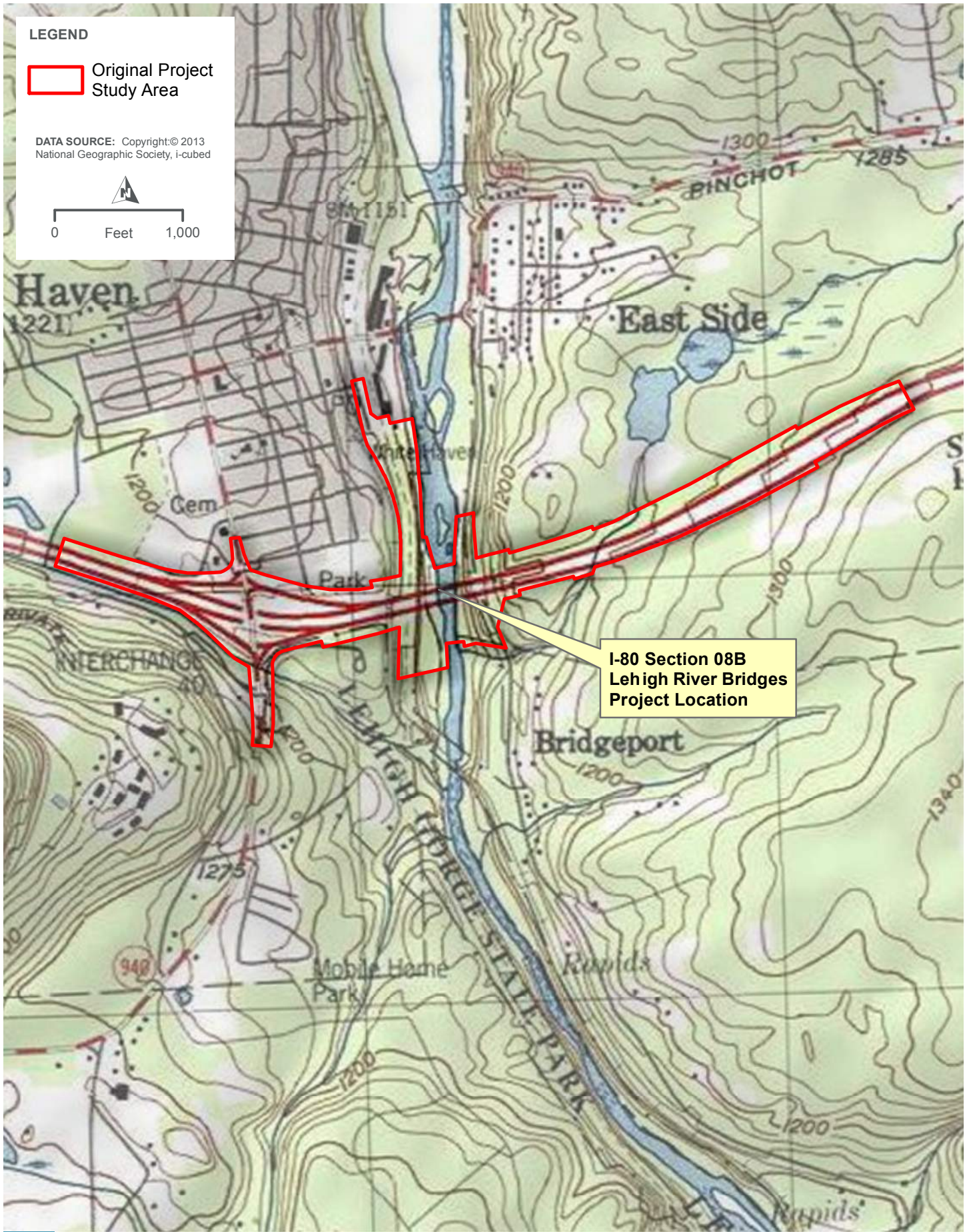
- Project location map
- Map of affected Section 4(f) property and other Section 4(f) property(ies) in the project vicinity
- Photographs of the Section 4(f) property
- Project plan sheet to show impacts
- Correspondence with the official with jurisdiction
- Public involvement information

**Attachment A**  
**Figures**

LEGEND

 Original Project Study Area

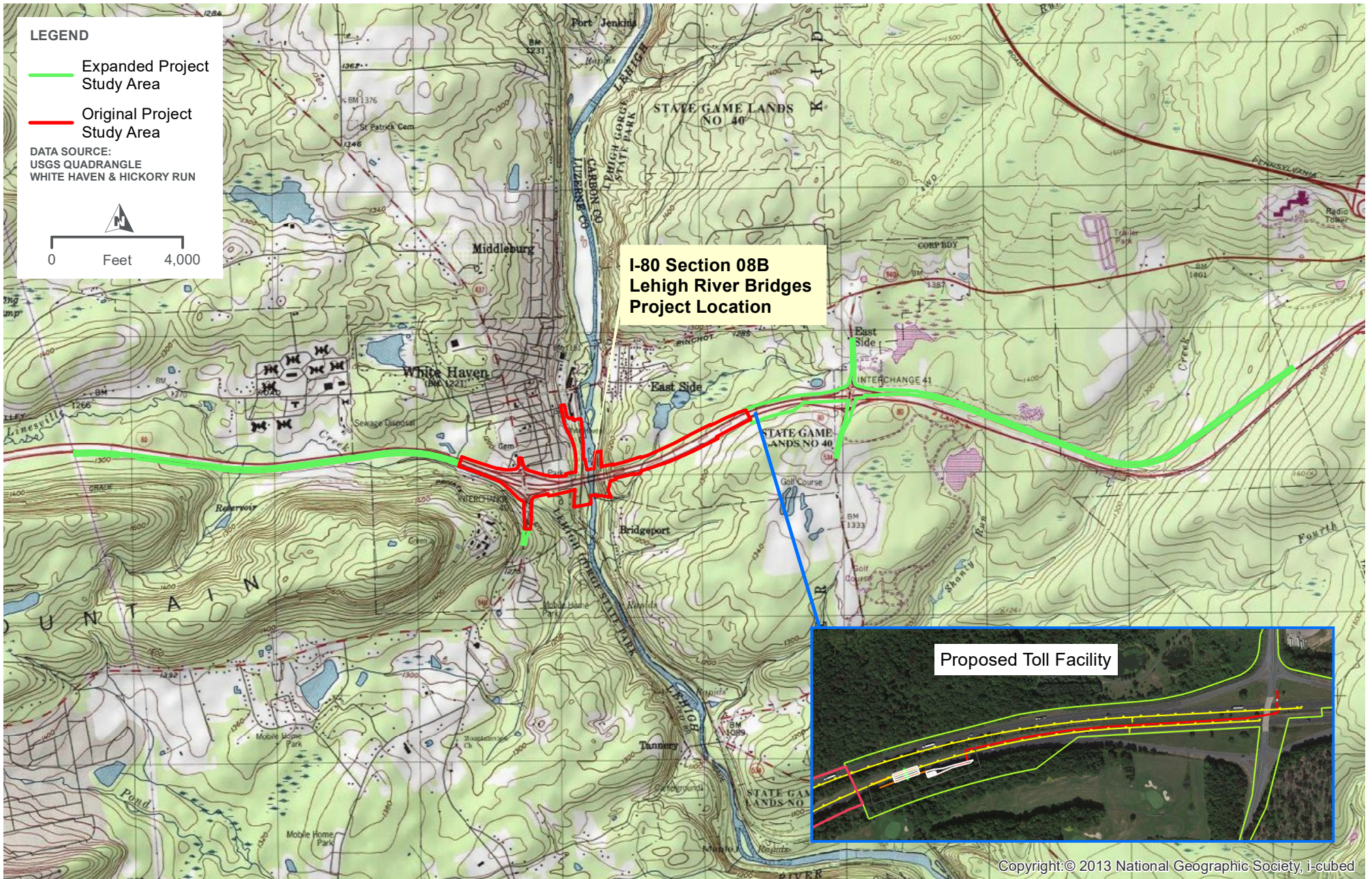
DATA SOURCE: Copyright © 2013  
National Geographic Society, i-cubed



I-80 Section 08B  
Lehigh River Bridges  
Project Location



I-80 SECTION 08B, LEHIGH RIVER BRIDGES  
PROJECT LOCATION MAP  
ORIGINAL PROJECT STUDY AREA

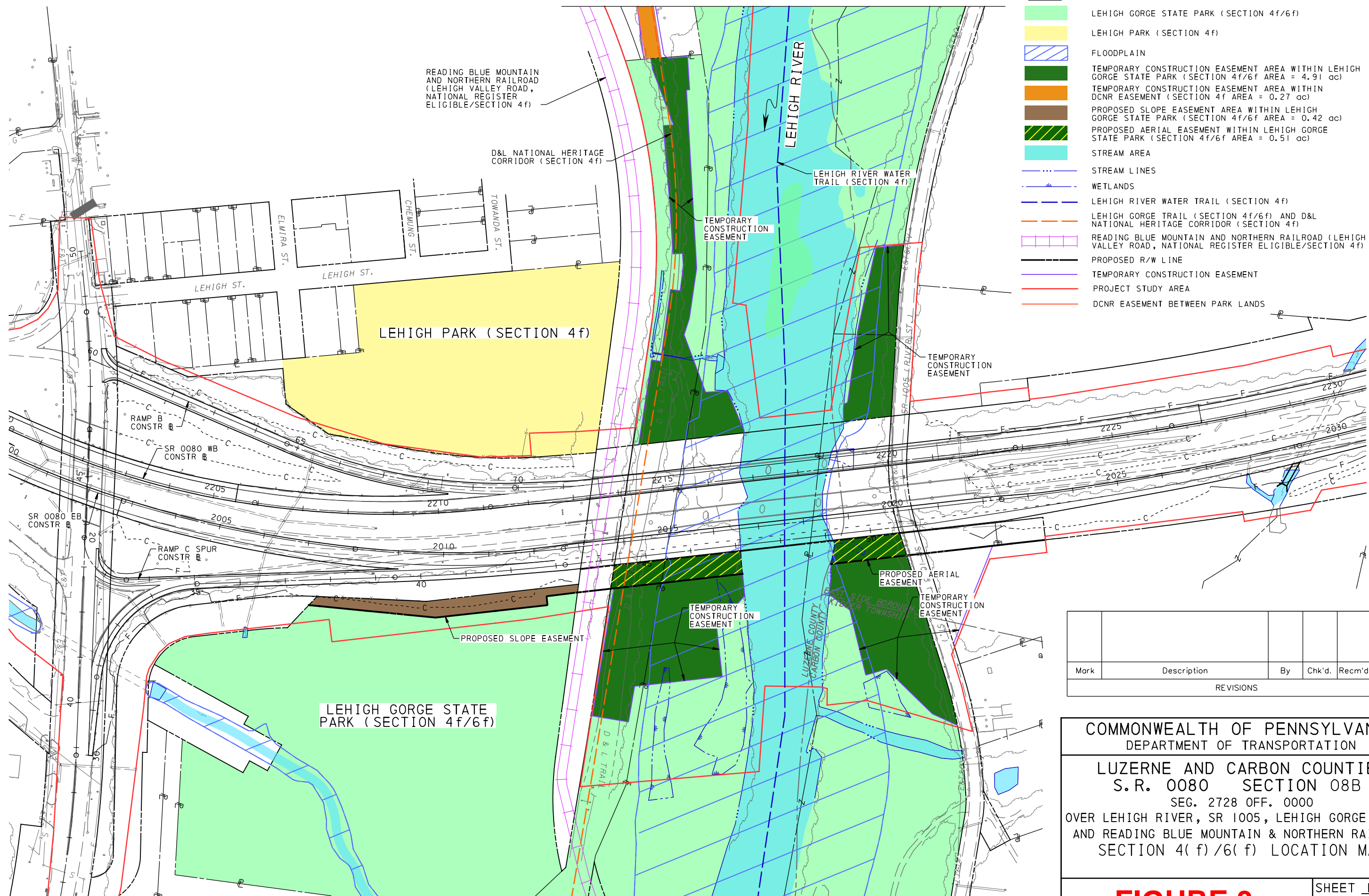


Copyright:© 2013 National Geographic Society, i-cubed

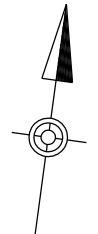
**I-80 SECTION 08B, LEHIGH RIVER BRIDGES  
PROJECT LOCATION MAP  
EXPANDED PROJECT STUDY AREA**

**FIGURE 2**





- LEGEND:**
- LEHIGH GORGE STATE PARK (SECTION 4f/6f)
  - LEHIGH PARK (SECTION 4f)
  - FLOODPLAIN
  - TEMPORARY CONSTRUCTION EASEMENT AREA WITHIN LEHIGH GORGE STATE PARK (SECTION 4f/6f AREA = 4.91 ac)
  - TEMPORARY CONSTRUCTION EASEMENT AREA WITHIN DCNR EASEMENT (SECTION 4f AREA = 0.27 ac)
  - PROPOSED SLOPE EASEMENT AREA WITHIN LEHIGH GORGE STATE PARK (SECTION 4f/6f AREA = 0.42 ac)
  - PROPOSED AERIAL EASEMENT WITHIN LEHIGH GORGE STATE PARK (SECTION 4f/6f AREA = 0.51 ac)
  - STREAM AREA
  - STREAM LINES
  - WETLANDS
  - LEHIGH RIVER WATER TRAIL (SECTION 4f)
  - LEHIGH GORGE TRAIL (SECTION 4f/6f) AND D&L NATIONAL HERITAGE CORRIDOR (SECTION 4f)
  - READING BLUE MOUNTAIN AND NORTHERN RAILROAD (LEHIGH VALLEY ROAD, NATIONAL REGISTER ELIGIBLE/SECTION 4f)
  - PROPOSED R/W LINE
  - TEMPORARY CONSTRUCTION EASEMENT
  - PROJECT STUDY AREA
  - DCNR EASEMENT BETWEEN PARK LANDS

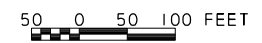


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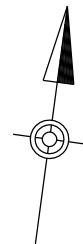
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REVISIONS					

**COMMONWEALTH OF PENNSYLVANIA**  
 DEPARTMENT OF TRANSPORTATION  
**LUZERNE AND CARBON COUNTIES**  
**S.R. 0080 SECTION 08B**  
 SEG. 2728 OFF. 0000  
 OVER LEHIGH RIVER, SR 1005, LEHIGH GORGE TRAIL,  
 AND READING BLUE MOUNTAIN & NORTHERN RAILROAD  
 SECTION 4(f)/6(f) LOCATION MAP

FIGURE 3







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- LEGEND:**
- LEHIGH GORGE STATE PARK (SECTION 4f/6f)
  - LEHIGH PARK (SECTION 4f)
  - FLOODPLAIN
  - TEMPORARY CONSTRUCTION EASEMENT AREA WITHIN LEHIGH GORGE STATE PARK (SECTION 4f/6f AREA = 4.91 ac)
  - TEMPORARY CONSTRUCTION EASEMENT AREA WITHIN DCNR EASEMENT (SECTION 4f AREA = 0.27 ac)
  - PROPOSED SLOPE EASEMENT AREA WITHIN LEHIGH GORGE STATE PARK (SECTION 4f/6f AREA = 0.42 ac)
  - PROPOSED AERIAL EASEMENT WITHIN LEHIGH GORGE STATE PARK (SECTION 4f/6f AREA = 0.51 ac)
  - STREAM AREA
  - STREAM LINES
  - WETLANDS
  - LEHIGH RIVER WATER TRAIL (SECTION 4f)
  - LEHIGH GORGE TRAIL (SECTION 4f/6f) AND D&L NATIONAL HERITAGE CORRIDOR (SECTION 4f)
  - READING BLUE MOUNTAIN AND NORTHERN RAILROAD (LEHIGH VALLEY ROAD, NATIONAL REGISTER ELIGIBLE/SECTION 4f)
  - PROPOSED R/W LINE
  - TEMPORARY CONSTRUCTION EASEMENT
  - PROJECT STUDY AREA
  - DCNR EASEMENT BETWEEN PARK LANDS

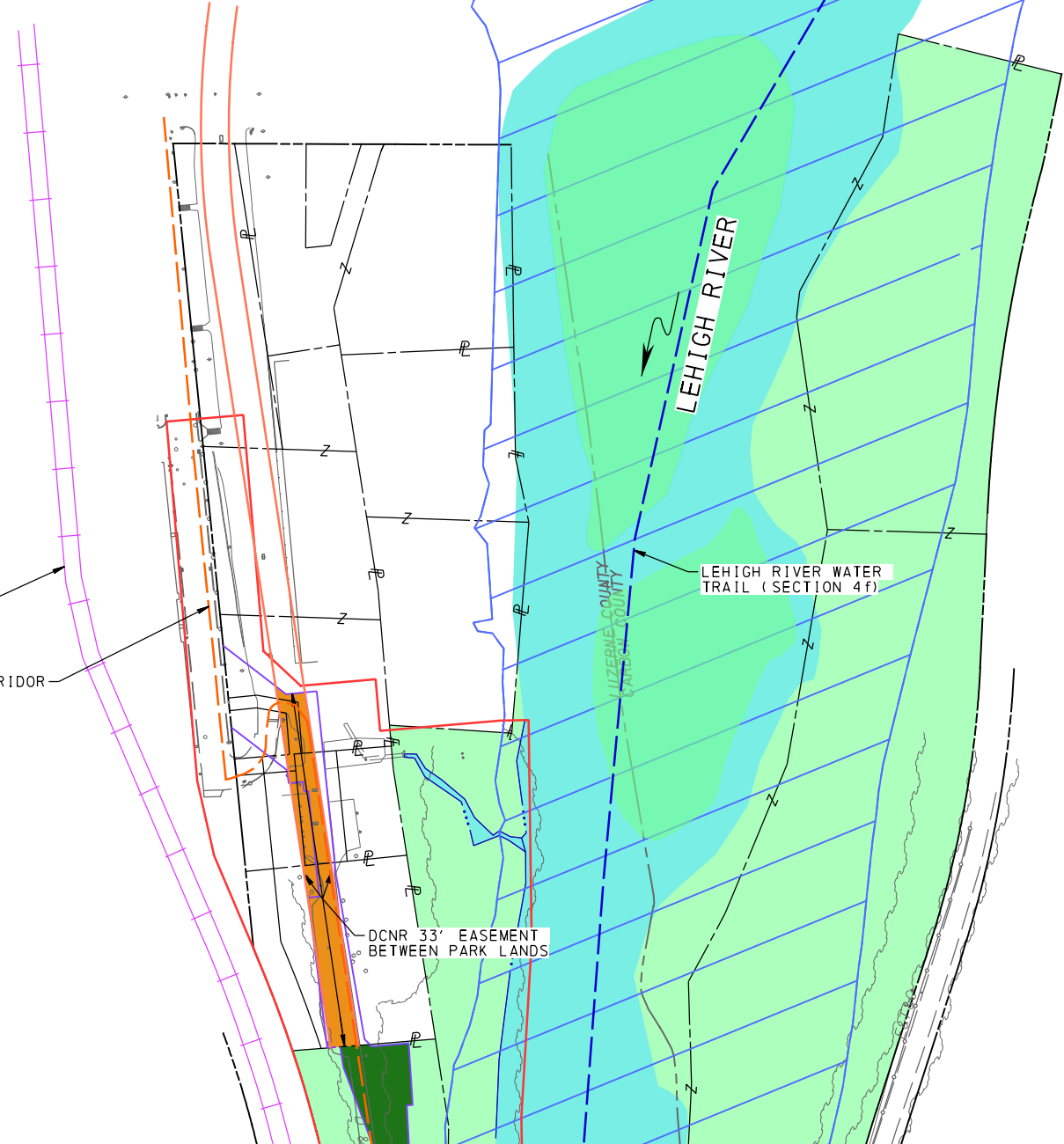
READING BLUE MOUNTAIN AND NORTHERN RAILROAD (LEHIGH VALLEY ROAD, NATIONAL REGISTER ELIGIBLE/SECTION 4f)

D&L NATIONAL HERITAGE CORRIDOR

LEHIGH RIVER

LEHIGH RIVER WATER TRAIL (SECTION 4f)

DCNR 33' EASEMENT BETWEEN PARK LANDS



SEE SHEET 1 OF 2

50 0 50 100 FEET

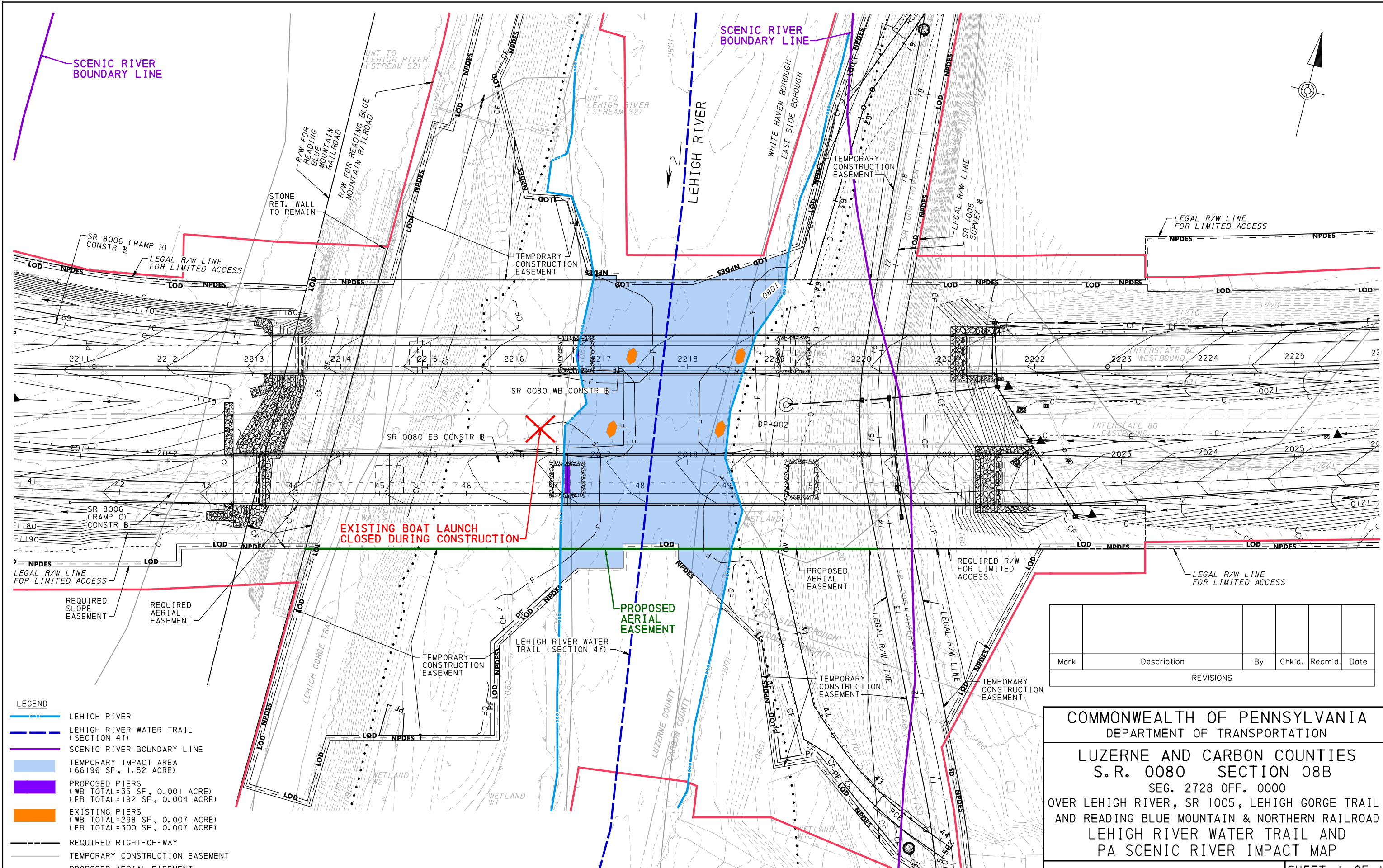
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REVISIONS					

COMMONWEALTH OF PENNSYLVANIA  
 DEPARTMENT OF TRANSPORTATION  
 LUZERNE AND CARBON COUNTIES  
 S.R. 0080 SECTION 08B  
 SEG. 2728 OFF. 0000  
 OVER LEHIGH RIVER, SR 1005, LEHIGH GORGE TRAIL,  
 AND READING BLUE MOUNTAIN & NORTHERN RAILROAD  
 SECTION 4(f)/6(f) LOCATION MAP

**FIGURE 3**

SHEET 2 OF 2

S-XXXXX



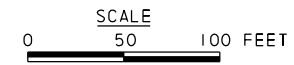
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- LEGEND**
- LEHIGH RIVER
  - - - LEHIGH RIVER WATER TRAIL (SECTION 4f)
  - SCENIC RIVER BOUNDARY LINE
  - TEMPORARY IMPACT AREA (66196 SF, 1.52 ACRE)
  - PROPOSED PIERS (WB TOTAL=35 SF, 0.001 ACRE) (EB TOTAL=192 SF, 0.004 ACRE)
  - EXISTING PIERS (WB TOTAL=298 SF, 0.007 ACRE) (EB TOTAL=300 SF, 0.007 ACRE)
  - REQUIRED RIGHT-OF-WAY
  - TEMPORARY CONSTRUCTION EASEMENT
  - PROPOSED AERIAL EASEMENT
  - PROJECT STUDY AREA

Mark	Description	By	Chk'd.	Recm'd.	Date
REVISIONS					

**COMMONWEALTH OF PENNSYLVANIA**  
 DEPARTMENT OF TRANSPORTATION  
**LUZERNE AND CARBON COUNTIES**  
**S.R. 0080 SECTION 08B**  
 SEG. 2728 OFF. 0000  
 OVER LEHIGH RIVER, SR 1005, LEHIGH GORGE TRAIL,  
 AND READING BLUE MOUNTAIN & NORTHERN RAILROAD  
 LEHIGH RIVER WATER TRAIL AND  
 PA SCENIC RIVER IMPACT MAP

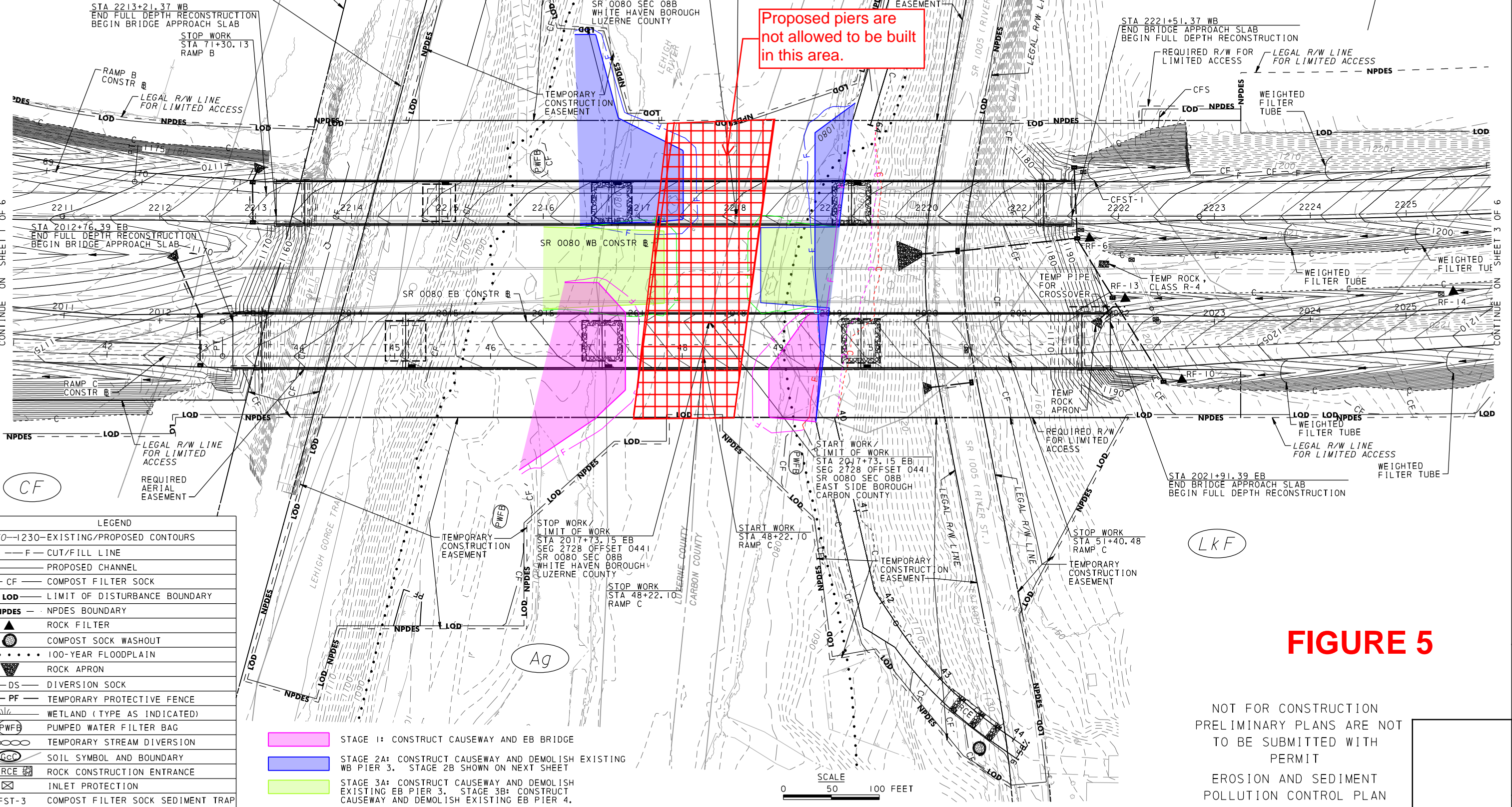
FIGURE 4



DISTRICT	COUNTY	ROUTE	SECTION	SHEET
5-0	*	0080	08B	2 OF 6
WHITE HAVEN AND EAST SIDE BOROUGHS				
REVISION NUMBER	REVISIONS	DATE	BY	

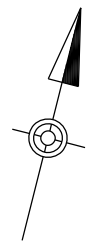
- NOTE:
- NPDES AND LOD OFFSET IS FOR CLARITY. NPDES AND LOD BOUNDARY FOLLOWS LEGAL RIGHT-OF-WAY, AND TEMPORARY CONSTRUCTION EASEMENT.
  - CAUSEWAY LOCATIONS SHOWN ON PLANS IS FOR STRUCTURE OPTION A (STEEL) ONLY.

WIB



Proposed piers are not allowed to be built in this area.

LKB



CF

LKF

Ag

LEGEND	
-1230--1230-	EXISTING/PROPOSED CONTOURS
-C--F-	CUT/FILL LINE
- - -	PROPOSED CHANNEL
CF	COMPOST FILTER SOCK
LOD	LIMIT OF DISTURBANCE BOUNDARY
NPDES	NPDES BOUNDARY
▲	ROCK FILTER
▲	COMPOST SOCK WASHOUT
.....	100-YEAR FLOODPLAIN
▲	ROCK APRON
DS	DIVERSION SOCK
PF	TEMPORARY PROTECTIVE FENCE
WETLAND	WETLAND (TYPE AS INDICATED)
PWFB	PUMPED WATER FILTER BAG
○	TEMPORARY STREAM DIVERSION
Co	SOIL SYMBOL AND BOUNDARY
RCE	ROCK CONSTRUCTION ENTRANCE
⊠	INLET PROTECTION
CFST-3	COMPOST FILTER SOCK SEDIMENT TRAP

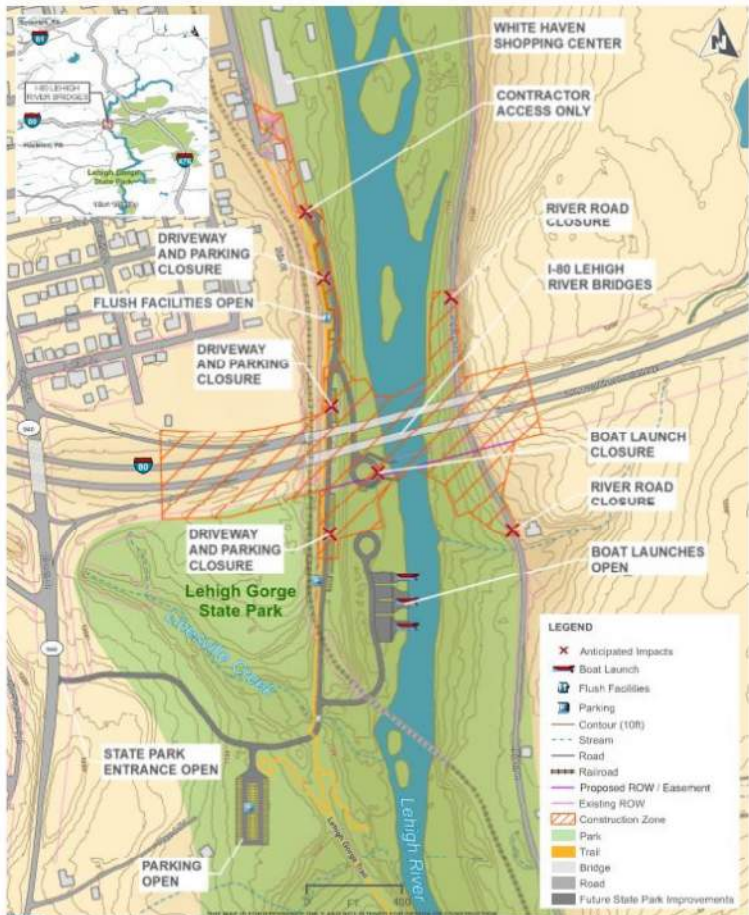
- STAGE 1: CONSTRUCT CAUSEWAY AND EB BRIDGE
- STAGE 2A: CONSTRUCT CAUSEWAY AND DEMOLISH EXISTING WB PIER 3. STAGE 2B SHOWN ON NEXT SHEET
- STAGE 3A: CONSTRUCT CAUSEWAY AND DEMOLISH EXISTING EB PIER 3. STAGE 3B: CONSTRUCT CAUSEWAY AND DEMOLISH EXISTING EB PIER 4.

SCALE  
0 50 100 FEET

**FIGURE 5**

NOT FOR CONSTRUCTION  
PRELIMINARY PLANS ARE NOT  
TO BE SUBMITTED WITH  
PERMIT  
EROSION AND SEDIMENT  
POLLUTION CONTROL PLAN

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**I-80 LEHIGH RIVER BRIDGE PROJECT**  
**TEMPORARY CLOSURES TO LEHIGH GORGE STATE PARK**  
**WHITE HAVEN & EAST SIDE BOROUGHS AND KIDDER TOWNSHIP**  
**LUZERNE & CARBON COUNTIES, PA**

140, SECTION 01E, LEHIGH RIVER BRIDGES PROJECT

**FIGURE 6**

**Attachment B**  
**Photographs**

Photographs



Lehigh Gorge State Park White Haven Access, facing south



Lehigh Gorge Trail and driveway, facing south



Lehigh Gorge Trail and driveway to existing boat launch, facing south



Boat launch area under I-80 Lehigh River Bridge on west shore of the Lehigh River, facing east



Underneath the I-80 Lehigh River Bridges on west shore of the Lehigh River, facing east



River gauge and raft requirements posted on the eastbound I-80 Lehigh River bridge pier





West shore of Lehigh River in Lehigh Gorge State Park, facing upstream (north)



West shore of Lehigh River in Lehigh Gorge State Park, facing downstream (south)



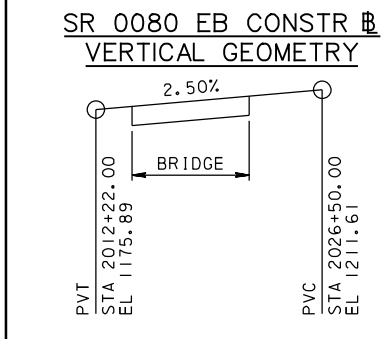
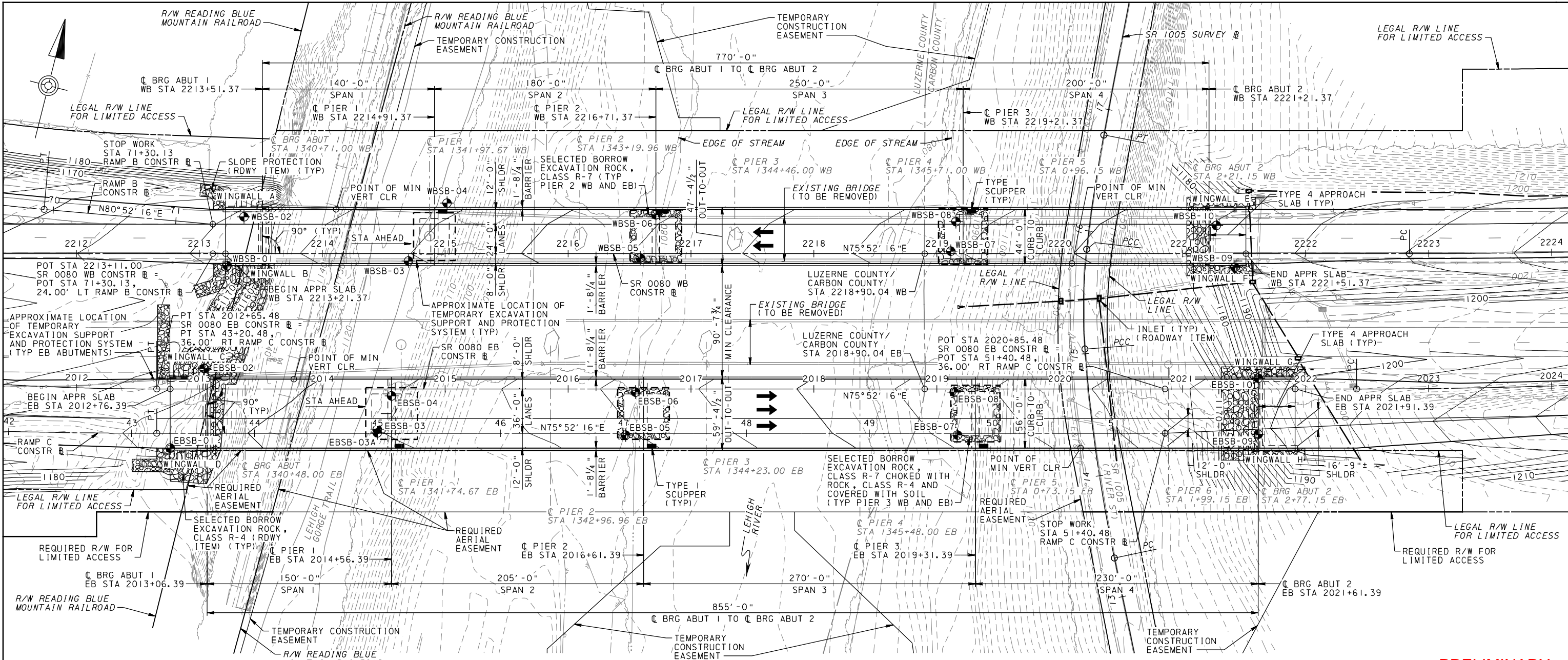
View of I-80 Lehigh River Bridges and Lehigh River, facing southeast



Lehigh Gorge Trail and driveway, facing north

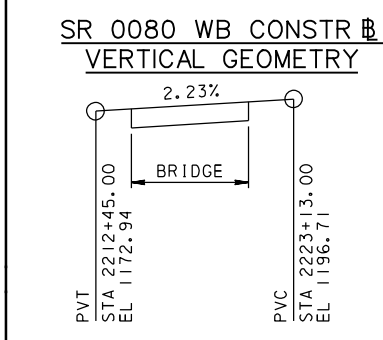
# **Attachment C**

## **Design Plans**



**HYDRAULIC DATA**  
DRAINAGE AREA= 311 SQ MI

EXISTING SR 0080 WB STRUCTURE	PROPOSED SR 0080 WB STRUCTURE
DESIGN FLOOD (50 YEAR): Q = 13,502 CFS VELOCITY= 6.7 FPS WS EL= 1086.64	DESIGN FLOOD (50 YEAR): Q = 13,502 CFS VELOCITY= 7.2 FPS WS EL= 1086.14
100 YEAR FLOOD: Q = 15,373 CFS VELOCITY= 6.9 FPS WS EL= 1087.30	100 YEAR FLOOD: Q = 15,373 CFS VELOCITY= 7.4 FPS WS EL= 1086.79

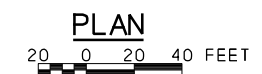


**AS-DRILLED BORING LOCATIONS (WB)**

BORING	STATION	OFFSET (FT)	SURFACE EL (FT)
WBSB-01	2213+22.7	9.7 RT	1174.1
WBSB-02	2213+36.4	30.0 LT	1174.4
WBSB-03	2214+70.2	6.0 RT	1120.0
WBSB-04	2215+01.5	41.1 LT	1119.1
WBSB-05	2216+58.8	3.5 RT	1083.1
WBSB-06	2216+71.4	31.6 LT	1081.8
WBSB-07	2219+11.3	2.0 LT	1084.8
WBSB-08	2219+15.5	26.1 LT	1084.4
WBSB-09	2221+42.4	11.1 RT	1190.8
WBSB-10	2221+27.3	23.0 LT	1192.0

**AS-DRILLED BORING LOCATIONS (EB)**

BORING	STATION	OFFSET (FT)	SURFACE EL (FT)
EBSB-01	2012+76.4	48.0 RT	1166.3
EBSB-02	2013+04.1	16.6 LT	1175.7
EBSB-03	2014+44.8	32.7 RT	1118.4
EBSB-03A	2014+44.8	35.7 RT	1118.4
EBSB-04	2014+56.4	5.6 RT	1118.9
EBSB-05	2016+46.8	37.5 RT	1080.4
EBSB-06	2016+54.8	2.5 RT	1080.8
EBSB-07	2019+16.8	37.5 RT	1089.9
EBSB-08	2019+14.0	2.5 RT	1088.7
EBSB-09	2021+61.4	36.8 RT	1178.3
EBSB-10	2021+61.4	8.8 LT	1185.0



- LEGEND:**
- AS-DRILLED TEST BORINGS
  - EXISTING CONTOUR
  - PROPOSED CONTOUR
  - EDGE OF STREAM
  - WETLAND LINES
  - DIRECTION OF TRAVEL
  - APPROXIMATE LOCATION OF TEMPORARY EXCAVATION SUPPORT AND PROTECTION SYSTEM

- NOTES:**
- FOR GENERAL NOTES AND LIST OF SUPPLEMENTAL DRAWINGS, SEE SHEET 3.
  - FOR TYPICAL SECTION, SEE SHEET 4.
  - FOUNDATION TYPES AND LAYOUTS TO BE DETERMINED FOLLOWING SUBSURFACE INVESTIGATION.
  - THE STATIONING SHOWN FOR EXISTING BRIDGE IS MEASURED ALONG THE EXISTING #.
  - FOR SR 1005 AND RAMP B AND C VERTICAL GEOMETRY, SEE SHEET 4.
  - FOR SR 0080 HORIZONTAL GEOMETRY, SEE SHEET 4.

Mark	Description	By	Chk'd.	Recm'd.	Date
REVISIONS					

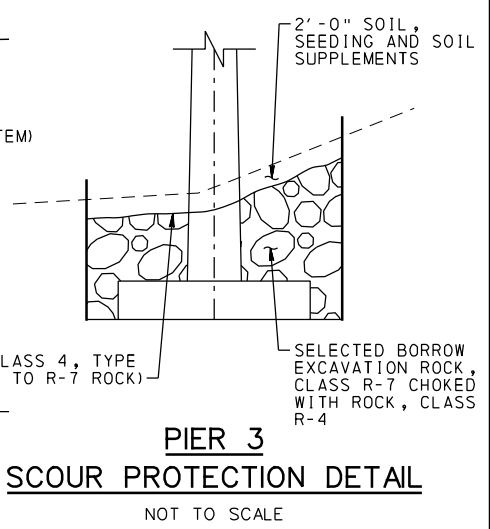
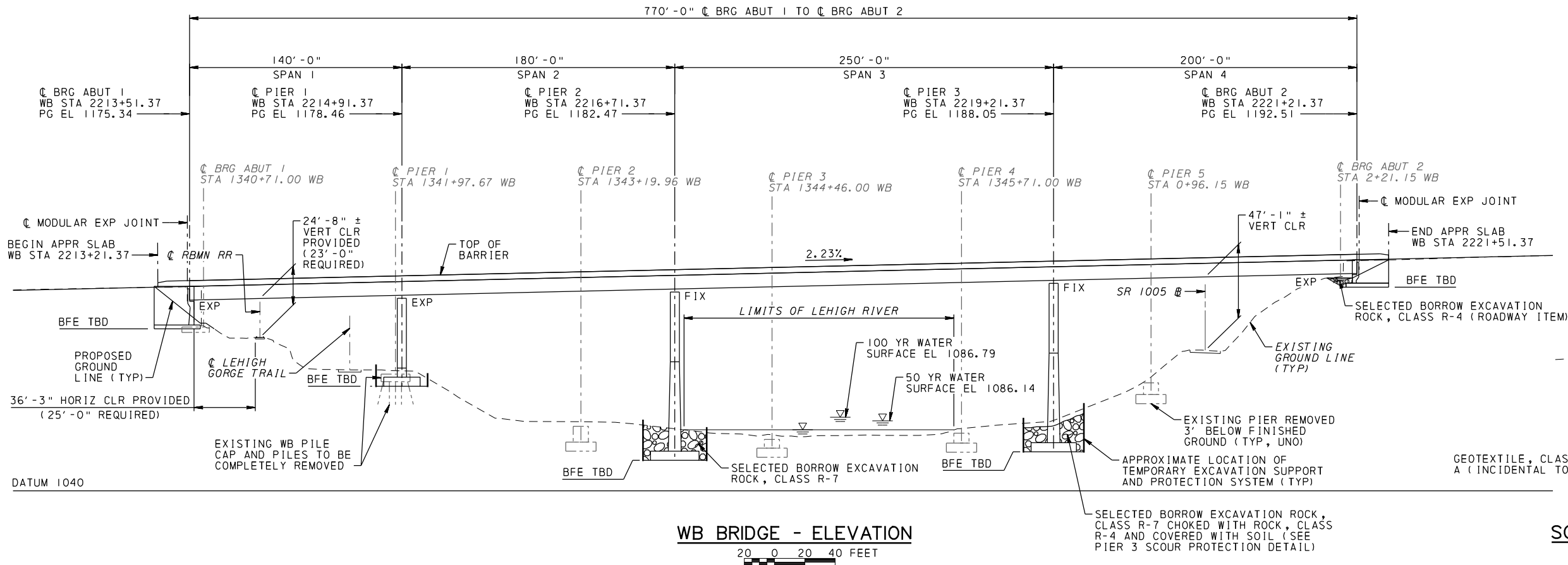
S.R. 0080 PREVIOUSLY KNOWN AS L.R. 1009 MPMS/ECMS: 99552  
 BMS STR ID: 13-0080-2728-0000, 13-0080-2729-0000 BRKEY: 8972, 8974

**COMMONWEALTH OF PENNSYLVANIA**  
**DEPARTMENT OF TRANSPORTATION**

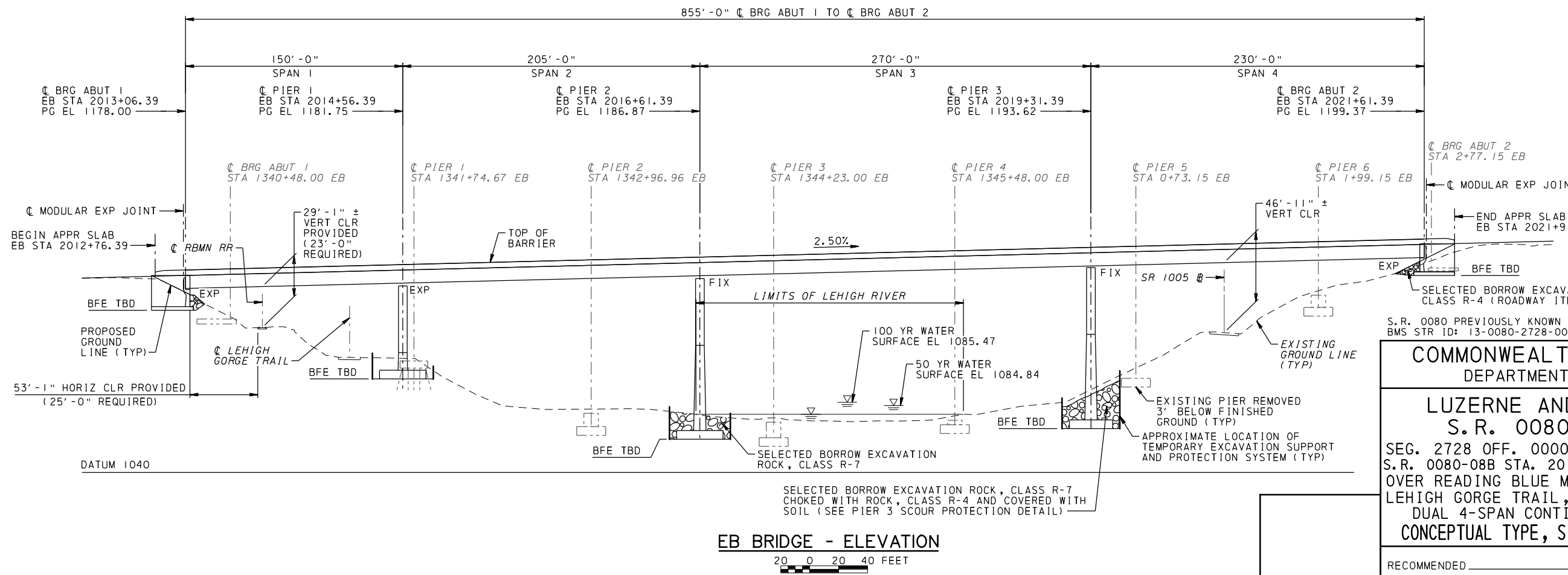
**LUZERNE AND CARBON COUNTIES**  
**S.R. 0080 SECTION 08B**

SEG. 2728 OFF. 0000 EB & SEG. 2729 OFF. 0000 WB  
 S.R. 0080-08B STA. 2017+33.89 EB & STA. 2217+36.37 WB  
 OVER READING BLUE MOUNTAIN & NORTHERN RAILROAD,  
 LEHIGH GORGE TRAIL, LEHIGH RIVER, AND S.R. 1005  
 DUAL 4-SPAN CONTINUOUS STEEL GIRDER BRIDGES  
**CONCEPTUAL TYPE, SIZE, & LOCATION - PLAN**

PREPARED BY:  
  
 HDR ENGINEERING, INC.  
 3025 CHEMICAL RD  
 SUITE 110  
 PLYMOUTH MEETING, PA 19462



- NOTES:**
- FOR GENERAL NOTES, SEE SHEET 3.
  - FOR TYPICAL SECTION, SEE SHEET 4.
  - FOUNDATION TYPES AND LAYOUTS TO BE DETERMINED FOLLOWING SUBSURFACE INVESTIGATION.
  - THE STATIONING SHOWN FOR EXISTING BRIDGE IS MEASURED ALONG THE EXISTING @.
  - BEARING FIXITY IS CONCEPTUAL. BEARING TYPE, FIXITY AND JOINT MOVEMENT TO BE DETERMINED DURING FINAL DESIGN.



**PRELIMINARY**

S.R. 0080 PREVIOUSLY KNOWN AS L.R. 1009 MPMS/ECMS: 99552  
 BMS STR ID: 13-0080-2728-0000, 13-0080-2729-0000 BRKEY: 8972, 8974

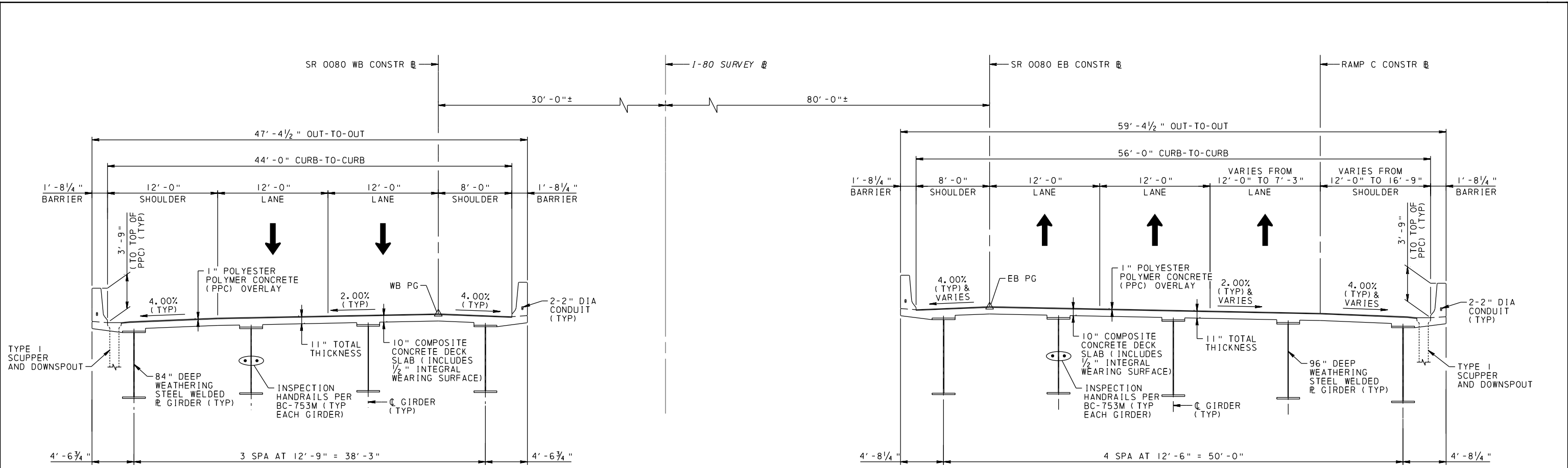
**COMMONWEALTH OF PENNSYLVANIA**  
 DEPARTMENT OF TRANSPORTATION

**LUZERNE AND CARBON COUNTIES**  
 S.R. 0080 SECTION 08B

SEG. 2728 OFF. 0000 EB & SEG. 2729 OFF. 0000 WB  
 S.R. 0080-08B STA. 2017+33.89 EB & STA. 2217+36.37 WB  
 OVER READING BLUE MOUNTAIN & NORTHERN RAILROAD,  
 LEHIGH GORGE TRAIL, LEHIGH RIVER, AND S.R. 1005  
 DUAL 4-SPAN CONTINUOUS STEEL GIRDER BRIDGES  
**CONCEPTUAL TYPE, SIZE, & LOCATION - ELEVATIONS**

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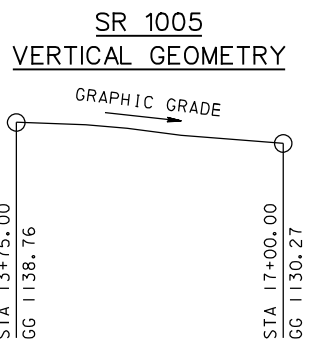
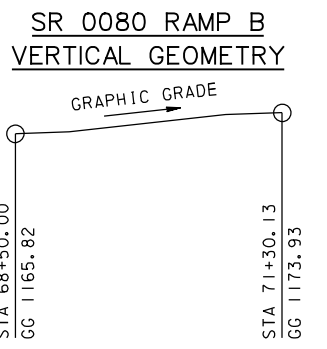


**PROPOSED TYPICAL SECTION**

(LOOKING STATIONS AHEAD)  
2 0 4 8 FEET

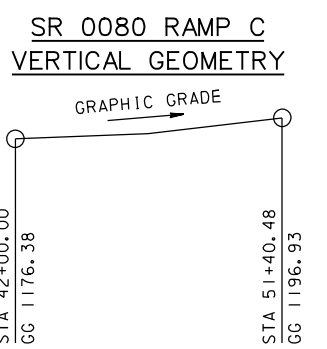
**SR 0080 WB HORIZONTAL GEOMETRY WEST APPROACH**  
 PI STA 2207+12.22  
 $\Delta = 23^\circ 19' 06''$  LT  
 T = 397.22'  
 L = 783.44'  
 R = 1925.00'  
 E = 40.56'  
 PCC STA 2203+15.00  
 PT STA 2210+98.44  
 SUPERELEVATE 8.0%  
 DESIGN SPEED 70 MPH

**SR 0080 EB HORIZONTAL GEOMETRY WEST APPROACH**  
 PI STA 2010+35.80  
 $\Delta = 12^\circ 38' 39''$  LT  
 T = 231.55'  
 L = 461.23'  
 R = 2090.00'  
 E = 12.79'  
 PCC STA 2008+04.25  
 PT STA 2012+65.48  
 SUPERELEVATE 7.8%  
 DESIGN SPEED 70 MPH



**SR 0080 WB HORIZONTAL GEOMETRY EAST APPROACH**  
 PI STA 2224+58.96  
 $\Delta = 3^\circ 05' 48''$  LT  
 T = 174.61'  
 L = 349.13'  
 R = 6460.00'  
 E = 2.36'  
 PC STA 2222+84.35  
 PCC STA 2226+33.48  
 SUPERELEVATE 3.2%  
 DESIGN SPEED 70 MPH

**SR 0080 EB HORIZONTAL GEOMETRY EAST APPROACH**  
 PI STA 2025+76.65  
 $\Delta = 7^\circ 35' 47''$  LT  
 T = 335.26'  
 L = 669.53'  
 R = 5050.00'  
 E = 11.12'  
 PC STA 2022+41.39  
 PT STA 2029+10.92  
 SUPERELEVATE 4.0%  
 DESIGN SPEED 70 MPH



NOTE:

1. FOR GENERAL NOTES, SEE SHEET 3.

**PRELIMINARY**

Mark	Description	By	Chk'd.	Recm'd.	Date
REVISIONS					

S.R. 0080 PREVIOUSLY KNOWN AS L.R. 1009 MPMS/ECMS: 99552  
 BMS STR ID: 13-0080-2728-0000, 13-0080-2729-0000 BRKEY: 8972, 8974

**COMMONWEALTH OF PENNSYLVANIA**  
 DEPARTMENT OF TRANSPORTATION

**LUZERNE AND CARBON COUNTIES**  
**S.R. 0080 SECTION 08B**

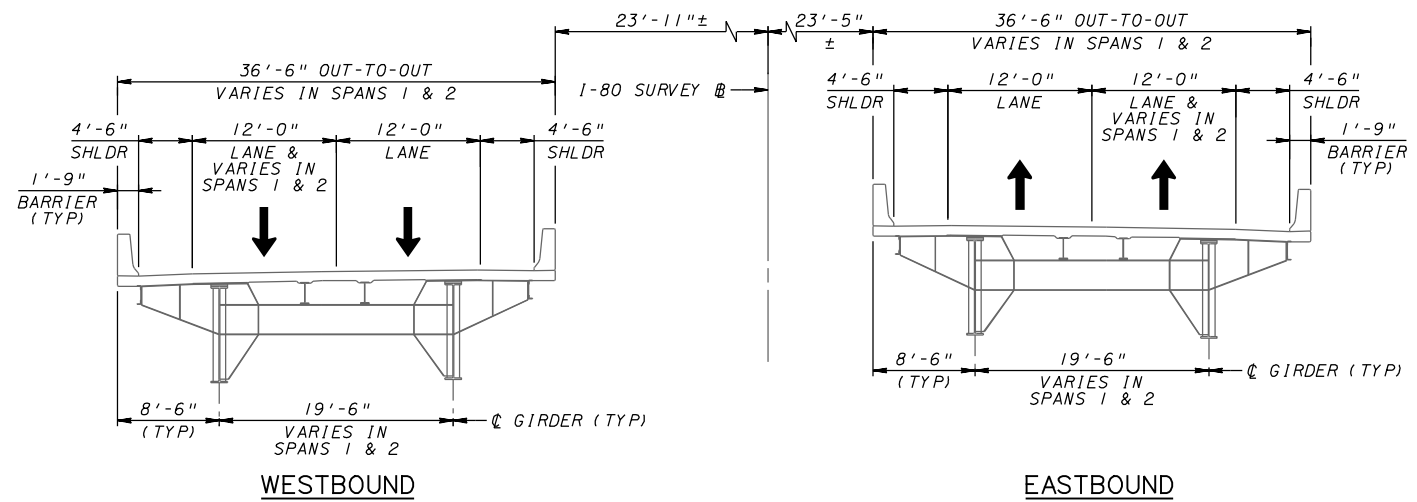
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 S.R. 0080-08B STA. 2017+33.89 EB & STA. 2217+36.37 WB  
 OVER READING BLUE MOUNTAIN & NORTHERN RAILROAD,  
 LEHIGH GORGE TRAIL, LEHIGH RIVER, AND S.R. 1005  
 DUAL 4-SPAN CONTINUOUS STEEL GIRDER BRIDGES

**TYPICAL SECTION**

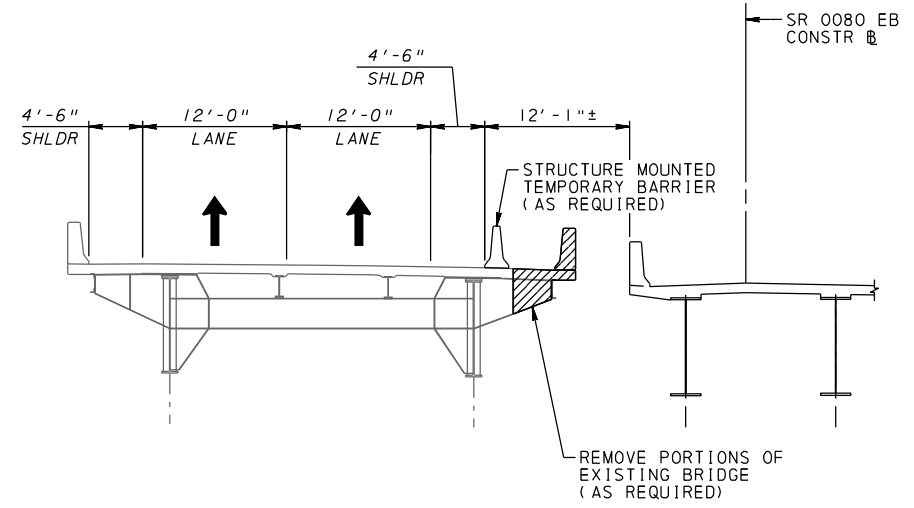
RECOMMENDED \_\_\_\_\_ SHEET 4 OF 7

S-XXXXX

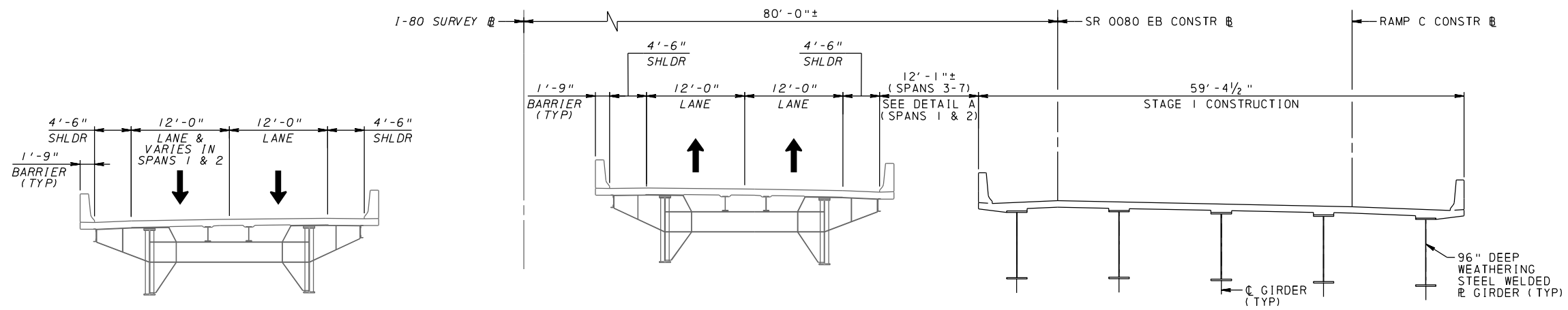
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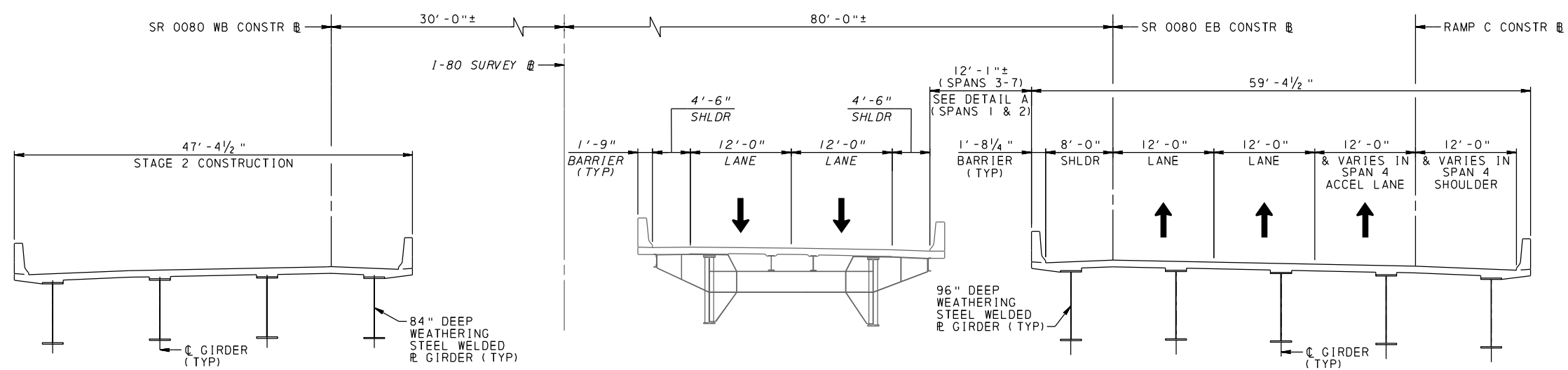
**EXISTING**  
(LOOKING STATIONS AHEAD)  
2 0 4 8 FEET



**DETAIL A**  
(SPANS 1 & 2 ONLY)  
2 0 4 8 FEET



**STAGE 1 CONSTRUCTION**  
(LOOKING STATIONS AHEAD)  
2 0 4 8 FEET



**STAGE 2 CONSTRUCTION**  
(LOOKING STATIONS AHEAD)  
2 0 4 8 FEET

**NOTE:**  
1. FOR GENERAL NOTES, SEE SHEET 3.

PRELIMINARY

Mark	Description	By	Chk'd.	Recm'd.	Date
REVISIONS					

S.R. 0080 PREVIOUSLY KNOWN AS L.R. 1009 MPMS/ECMS: 99552  
BMS STR ID: 13-0080-2728-0000, 13-0080-2729-0000 BRKEY: 8972, 8974

**COMMONWEALTH OF PENNSYLVANIA**  
DEPARTMENT OF TRANSPORTATION

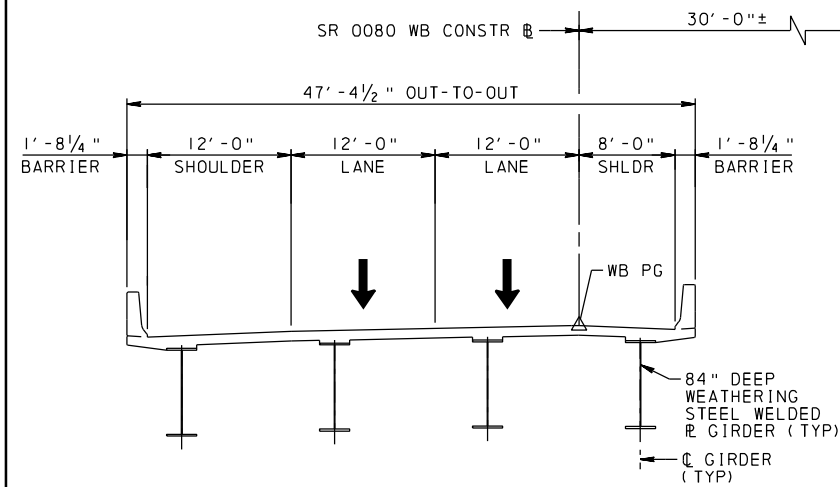
**LUZERNE AND CARBON COUNTIES**  
**S.R. 0080 SECTION 08B**

SEG. 2728 OFF. 0000 EB & SEG. 2729 OFF. 0000 WB  
S.R. 0080-08B STA. 2017+33.89 EB & STA. 2217+36.37 WB  
OVER READING BLUE MOUNTAIN & NORTHERN RAILROAD,  
LEHIGH GORGE TRAIL, LEHIGH RIVER, AND S.R. 1005  
DUAL 4-SPAN CONTINUOUS STEEL GIRDER BRIDGES

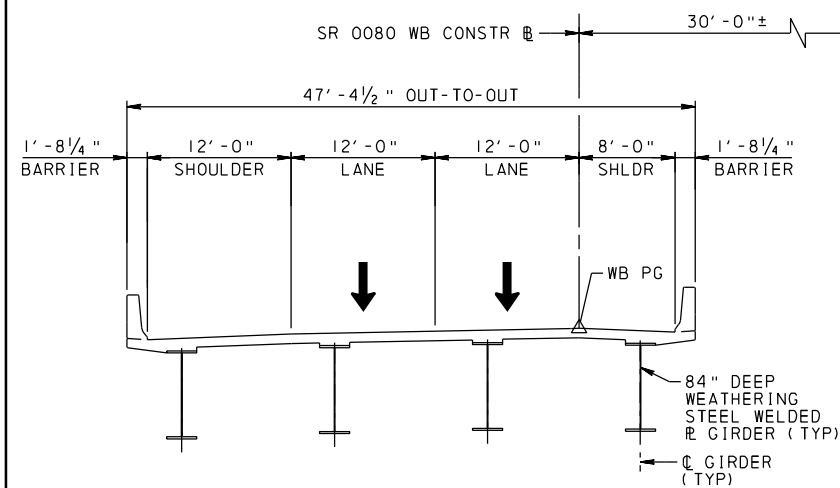
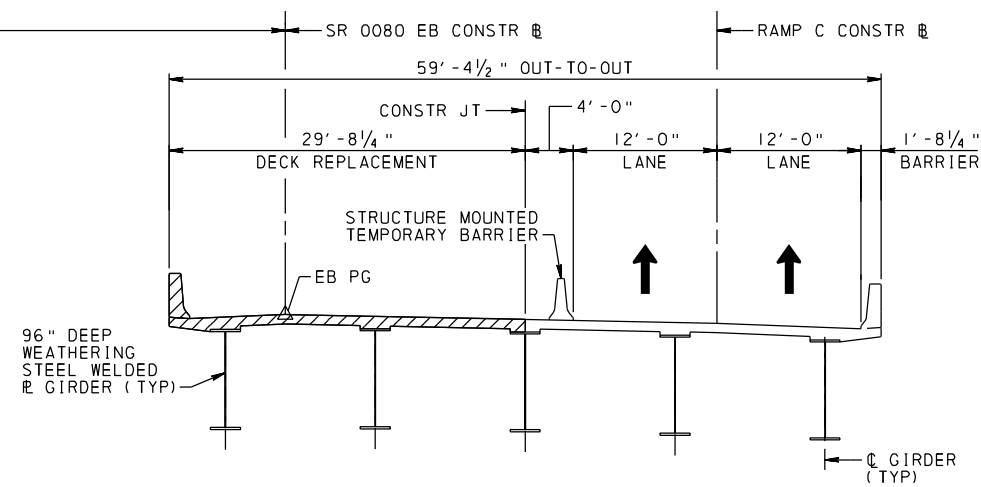
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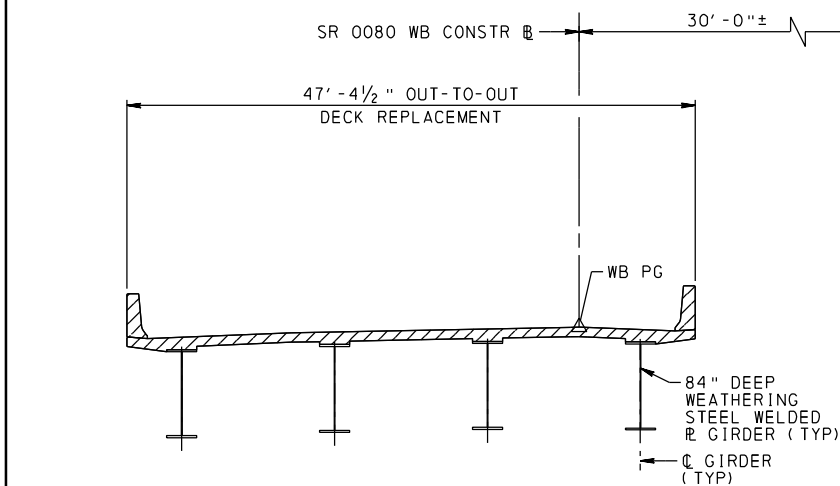
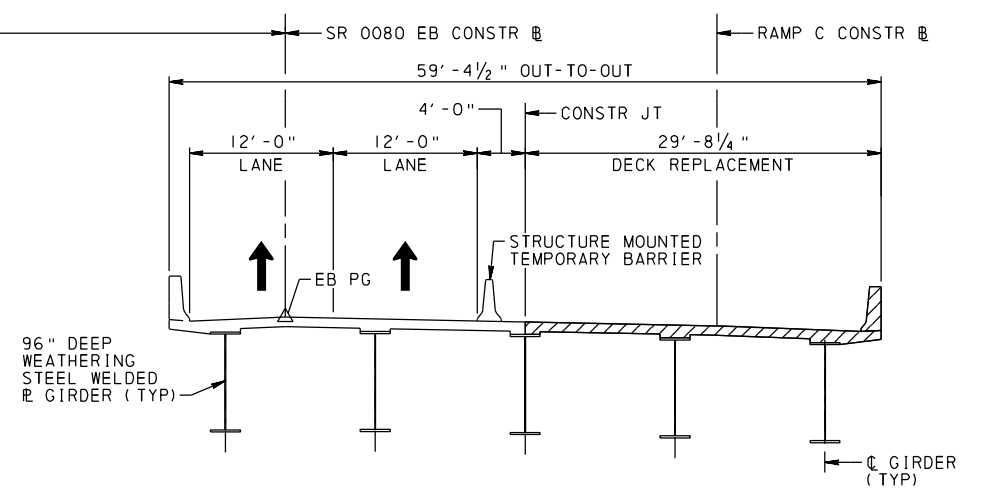




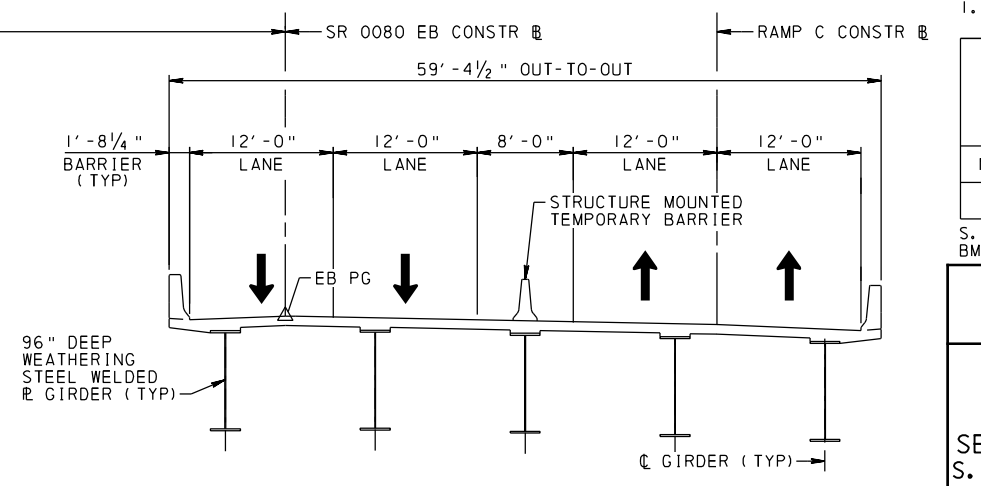
PHASE 1



PHASE 2



PHASE 3



**FUTURE DECK REPLACEMENT**

(LOOKING STATIONS AHEAD)  
2 0 4 8 FEET

**NOTE:**

1. FOR GENERAL NOTES, SEE SHEET 3.

**PRELIMINARY**

Mark	Description	By	Chk'd.	Recm'd.	Date
REVISIONS					

S.R. 0080 PREVIOUSLY KNOWN AS L.R. 1009 MPMS/ECMS: 99552  
BMS STR ID: 13-0080-2728-0000, 13-0080-2729-0000 BRKEY: 8972, 8974

**COMMONWEALTH OF PENNSYLVANIA**  
DEPARTMENT OF TRANSPORTATION

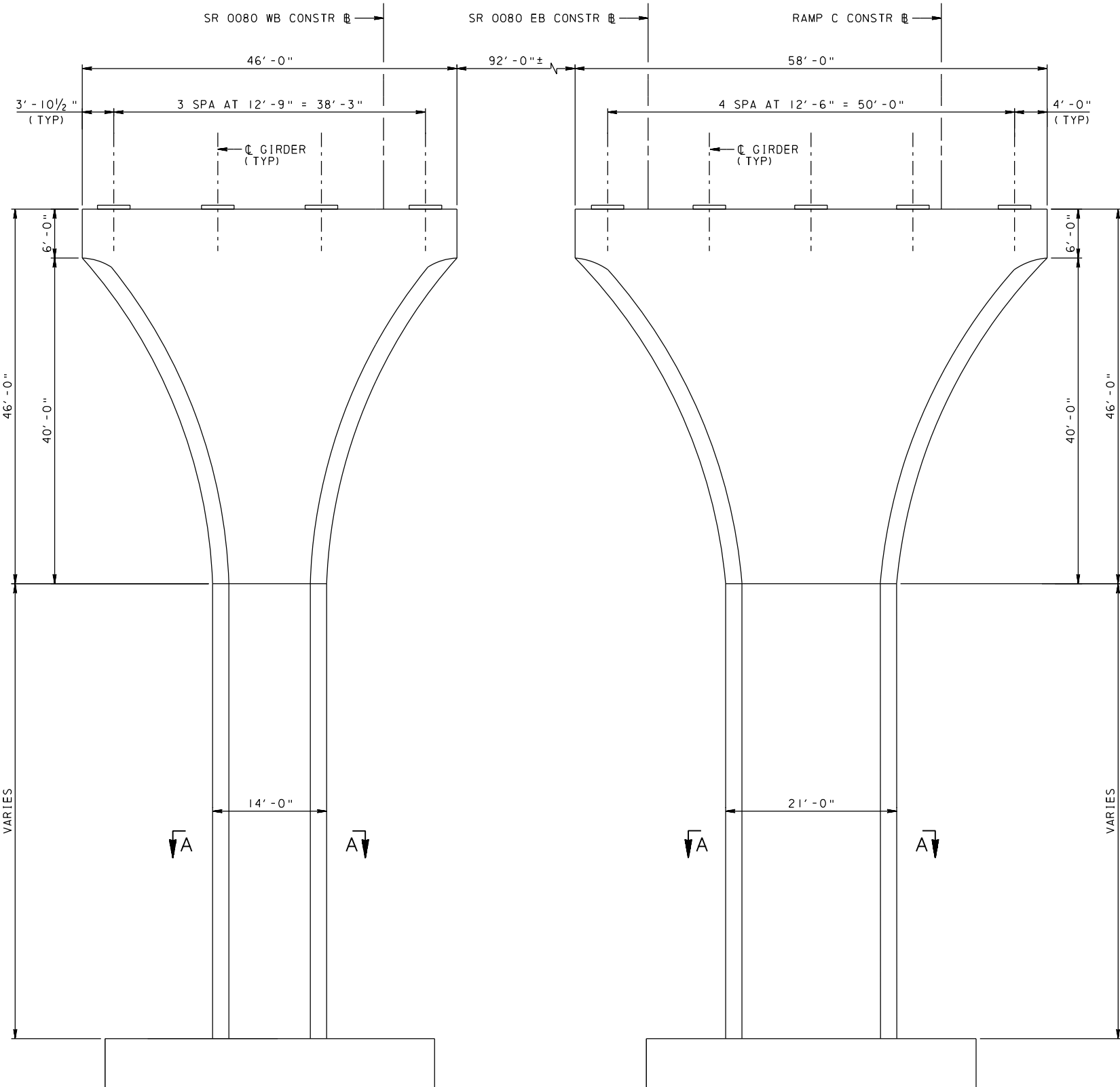
**LUZERNE AND CARBON COUNTIES**  
**S.R. 0080 SECTION 08B**

SEG. 2728 OFF. 0000 EB & SEG. 2729 OFF. 0000 WB  
S.R. 0080-08B STA. 2017+33.89 EB & STA. 2217+36.37 WB  
OVER READING BLUE MOUNTAIN & NORTHERN RAILROAD,  
LEHIGH GORGE TRAIL, LEHIGH RIVER, AND S.R. 1005  
DUAL 4-SPAN CONTINUOUS STEEL GIRDER BRIDGES  
**FUTURE DECK REPLACEMENT**

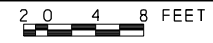
RECOMMENDED \_\_\_\_\_

SHEET 6 OF 7

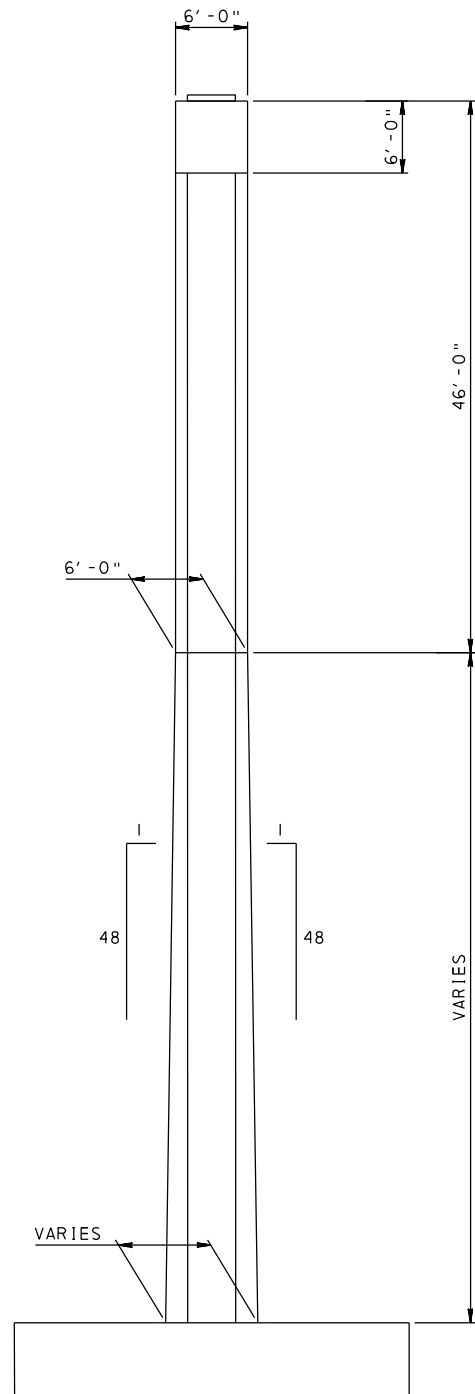
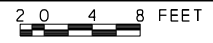
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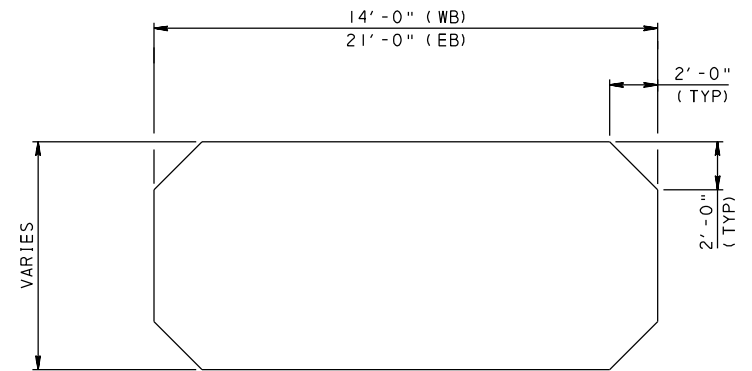
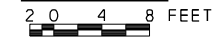
WB PIER - ELEVATION



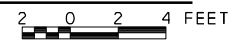
EB PIER - ELEVATION



SIDE VIEW



SECTION A-A



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 PATH: c:\pwworking\mg\esd\01\194488\ FILE: 00808B-EB-PIER-DET.dgn MODEL: De fou 1 r

**NOTE:**

1. FOR GENERAL NOTES, SEE SHEET 3.

**PRELIMINARY**

Mark	Description	By	Chk'd.	Recm'd.	Date
REVISIONS					

S.R. 0080 PREVIOUSLY KNOWN AS L.R. 1009 MPMS/ECMS: 99552  
 BMS STR ID: 13-0080-2728-0000, 13-0080-2729-0000 BRKEY: 8972, 8974

**COMMONWEALTH OF PENNSYLVANIA**  
 DEPARTMENT OF TRANSPORTATION  
**LUZERNE AND CARBON COUNTIES**  
**S.R. 0080 SECTION 08B**  
 SEG. 2728 OFF. 0000 EB & SEG. 2729 OFF. 0000 WB  
 S.R. 0080-08B STA. 2017+33.89 EB & STA. 2217+36.37 WB  
 OVER READING BLUE MOUNTAIN & NORTHERN RAILROAD,  
 LEHIGH GORGE TRAIL, LEHIGH RIVER, AND S.R. 1005  
 DUAL 4-SPAN CONTINUOUS STEEL GIRDER BRIDGES  
**PIER DETAILS**

RECOMMENDED \_\_\_\_\_ SHEET 7 OF 7

S-XXXXX



# Pennsylvania Fish & Boat Commission

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August 16, 2021

Jennifer Nolan-Kremm, PE  
HDR  
301 Grant St. Suite 1700  
Pittsburgh, PA 15219

I have reviewed the recently submitted Aids to Navigation Plan for the SR 080 section 08B bridge replacement project over the Lehigh River in White Haven Borough, Luzerne County and East Side Borough, Carbon County. The plan is acceptable as submitted and has been approved. All signage/buoys must be installed prior to construction and inspected frequently to ensure it is always visible to boaters. Any changes to this plan must be submitted directly to this office for further review. If buoys are to be utilized PFBC-277 Application to Install Floating Structures/Private Aids to Navigation must be submitted and approved prior to installation.

I will share the approved ATON plan with the PA Fish & Boat Commission Division of Environmental Services and the Regional Law Enforcement Office to ensure they are aware of the project. Please advise the contractor that the project site may be inspected at any time by Bureau of Law Enforcement personnel for compliance with this plan. A copy of the approved plan along with this letter should be available for inspection if requested.

Please feel free to contact me if you have any questions concerning this project or the PA Fish & Boat Commission Aids to Navigation program in general.

Sincerely,

Clyde Warner, Colonel  
Bureau of Law Enforcement

Cc: (electronic) NE Region BLE-DES

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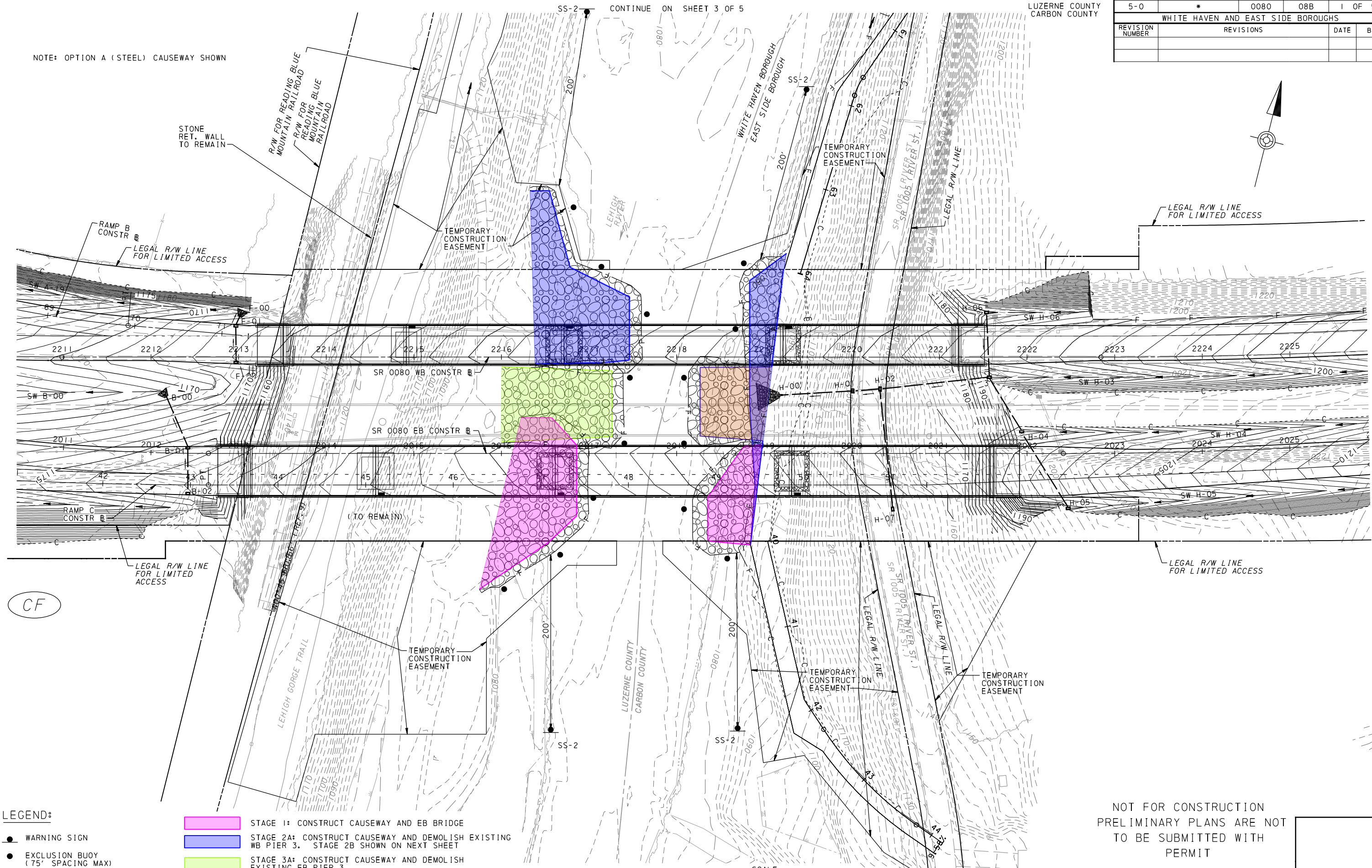
## Our Mission:

*To protect, conserve and enhance the Commonwealth's aquatic resources and provide fishing and boating opportunities.*

[www.fishandboat.com](http://www.fishandboat.com)

DISTRICT	COUNTY	ROUTE	SECTION	SHEET	
5-0	*	0080	08B	1 OF 5	
WHITE HAVEN AND EAST SIDE BOROUGHS					
REVISION NUMBER	REVISIONS			DATE	BY

NOTE: OPTION A (STEEL) CAUSEWAY SHOWN



**LEGEND:**

- WARNING SIGN
- EXCLUSION BUOY (75' SPACING MAX)

- STAGE 1: CONSTRUCT CAUSEWAY AND EB BRIDGE
- STAGE 2A: CONSTRUCT CAUSEWAY AND DEMOLISH EXISTING WB PIER 3. STAGE 2B SHOWN ON NEXT SHEET
- STAGE 3A: CONSTRUCT CAUSEWAY AND DEMOLISH EXISTING EB PIER 3.
- STAGE 3B: CONSTRUCT CAUSEWAY AND DEMOLISH EXISTING EB PIER 4.

AT 500' UPSTREAM ADD - SS1A NEXT TO SIGN SYMBOL BOTH SIDES

SCALE 0 50 100 FEET

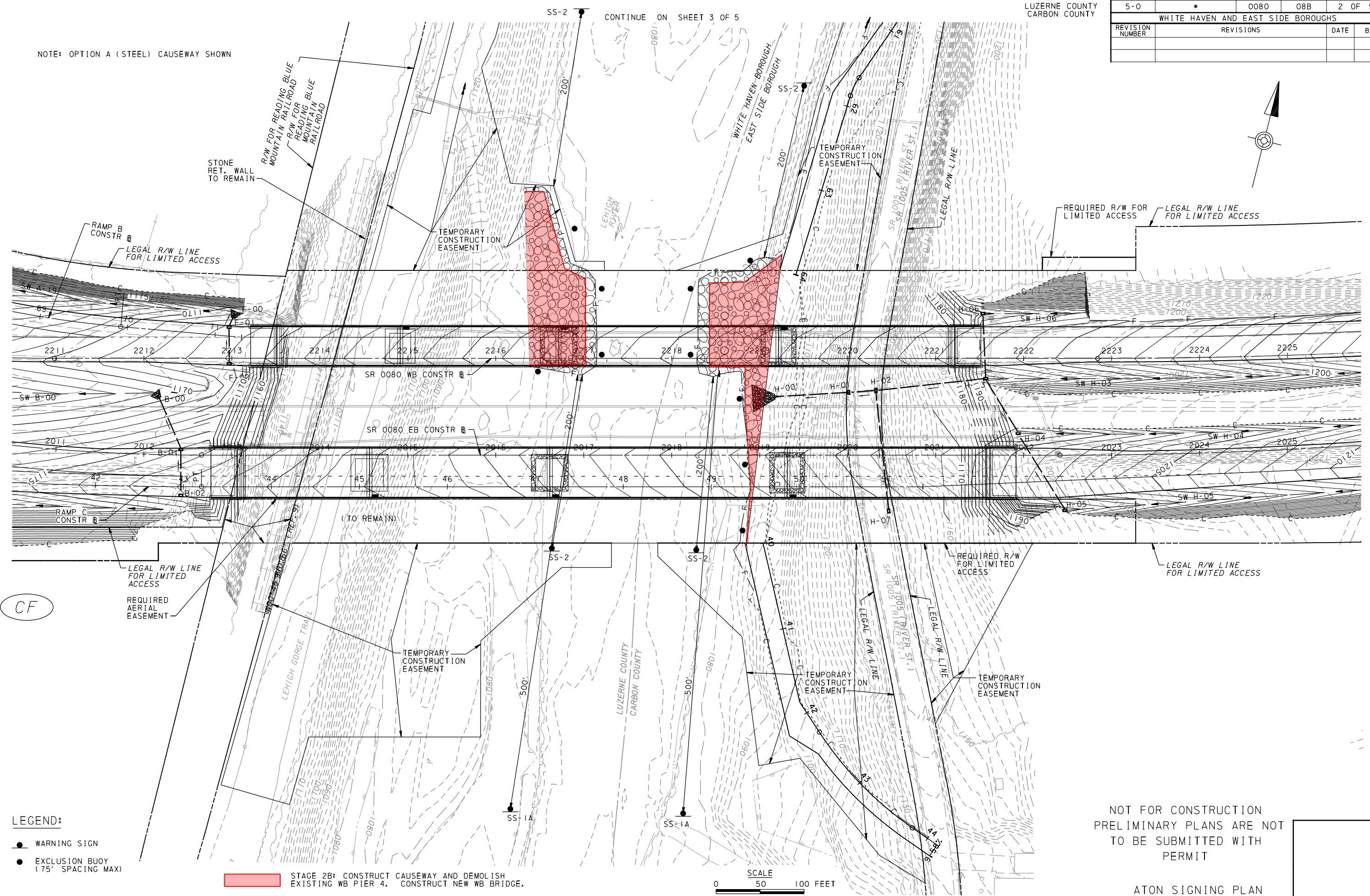
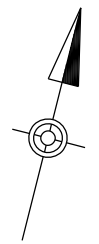
NOT FOR CONSTRUCTION  
PRELIMINARY PLANS ARE NOT  
TO BE SUBMITTED WITH  
PERMIT

ATON SIGNING PLAN

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 MODEL:road plan 2

DISTRICT	COUNTY	ROUTE	SECTION	SHEET	
5-0	*	0080	08B	2 OF 5	
WHITE HAVEN AND EAST SIDE BOROUGHS					
REVISION NUMBER	REVISIONS			DATE	BY

NOTE: OPTION A (STEEL) CAUSEWAY SHOWN



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 PLOT DRIVER: PcmDOT\_PDF\_Color.plt  
 PLOT DATE: 05-20-2021 3:06:55 PM  
 MODEL: road plan 2 - STAGE 2B

CF

- LEGEND:**
- WARNING SIGN
  - EXCLUSION BUOY (75' SPACING MAX)

STAGE 2B: CONSTRUCT CAUSEWAY AND DEMOLISH EXISTING WB PIER 4. CONSTRUCT NEW WB BRIDGE.

SCALE  
0 50 100 FEET

NOT FOR CONSTRUCTION  
PRELIMINARY PLANS ARE NOT  
TO BE SUBMITTED WITH  
PERMIT

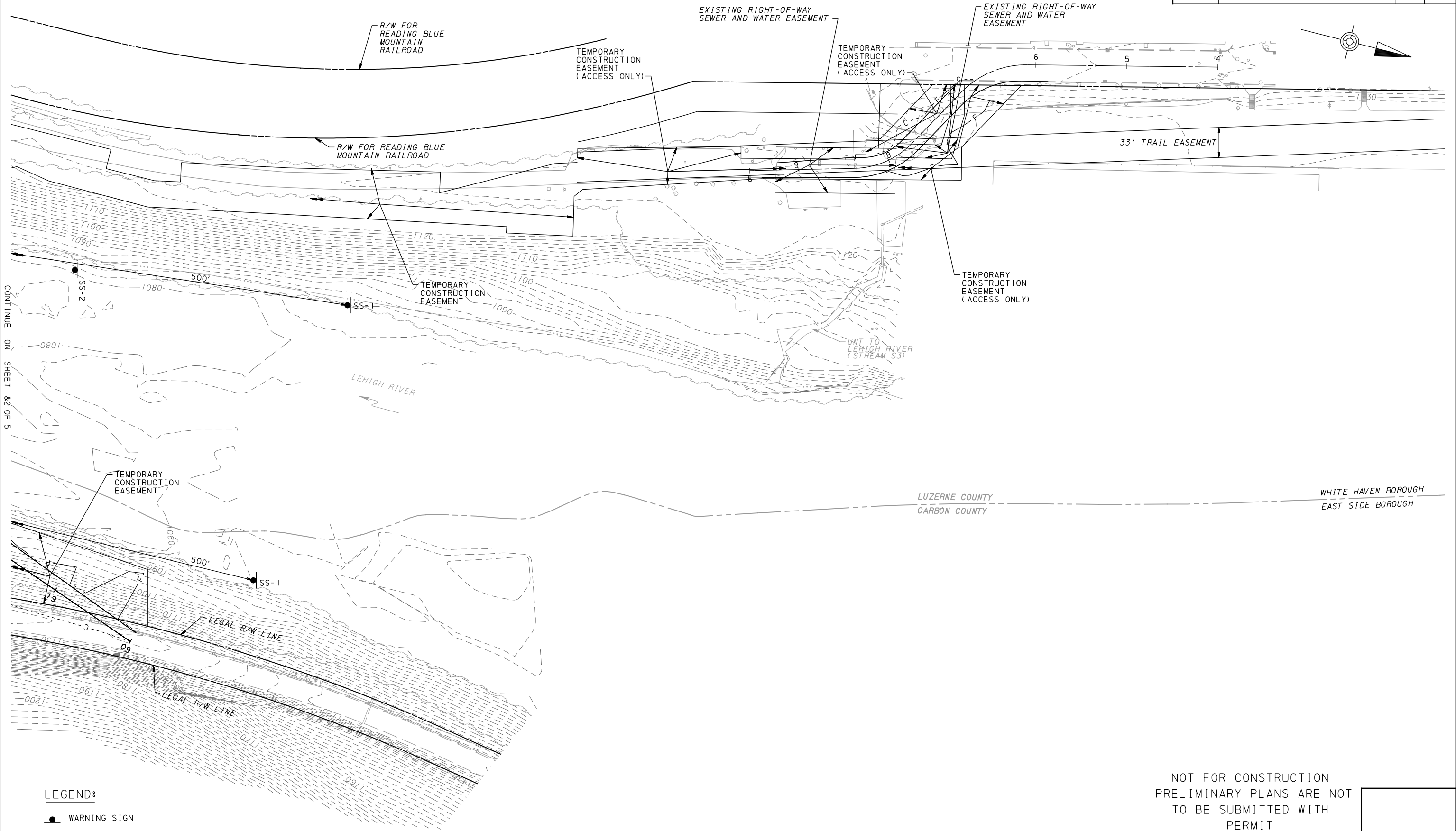
ATON SIGNING PLAN

DES:      DWG:      CKD:

LUZERNE COUNTY  
CARBON COUNTY

DISTRICT	COUNTY	ROUTE	SECTION	SHEET	
5-0	*	0080	08B	3 OF 5	
WHITE HAVEN AND EAST SIDE BOROUGHS					
REVISION NUMBER	REVISIONS			DATE	BY

NOTE: OPTION A (STEEL) CAUSEWAY SHOWN



LEGEND:

- WARNING SIGN
- EXCLUSION BUOY (75' SPACING MAX)

NOT FOR CONSTRUCTION  
PRELIMINARY PLANS ARE NOT  
TO BE SUBMITTED WITH  
PERMIT

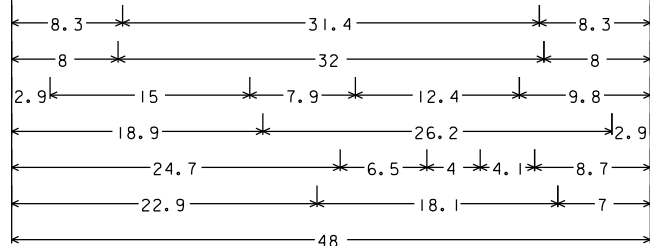
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 MODEL: road plan 5

DES:    DWG:    CKD:

DISTRICT	COUNTY	ROUTE	SECTION	SHEET	
5-0	*	0080	08B	4 OF 5	
WHITE HAVEN AND EAST SIDE BOROUGH					
REVISION NUMBER	REVISIONS			DATE	BY

SS-1A (BLACK LEGEND ON WHITE BACKGROUND)



2.5" RADIUS, 1.0" BORDER, 1.0" INDENT, ORANGE ON NONE;  
[WARNING] BLACK B; HORIZONTAL LINE FLUORESCENT ORANGE;  
[BRIDGE] BLACK B; [CONSTRUCTION] BLACK B;  
[500 FT.] BLACK B; [UPSTREAM] BLACK B;  
TABLE OF WIDTHS AND SPACES.

8.3	W	5.0	0.4	A	4.3	1.2	R	3.4	1.5	N	3.3	1.5	I	1.0	1.5	N	3.4	1.5	G	3.4	8.3
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8.0	—	32.0	8.0
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2.9	◇	15.0
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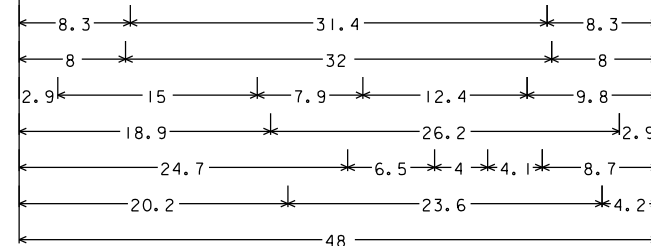
7.9	B	1.7	0.8	R	1.6	0.8	I	0.5	0.7	D	1.7	0.6	G	1.7	0.8	E	1.5	9.8
-----	---	-----	-----	---	-----	-----	---	-----	-----	---	-----	-----	---	-----	-----	---	-----	-----

18.9	C	1.7	0.6	O	1.8	0.7	N	1.7	0.8	S	1.7	0.6	T	1.5	0.6	R	1.6	0.8	U	1.7	0.7	C	1.7	0.4	T	1.5	0.6	I	0.5	0.8	O	1.8	0.7	N	1.7	2.9
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24.7	5	1.7	0.6	O	1.8	0.6	O	1.8	4.0	F	1.5	0.4	T	1.5	0.2	.	0.5	8.7
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22.9	U	1.7	0.8	P	1.7	0.6	S	1.7	0.6	T	1.5	0.6	R	1.6	0.8	E	1.5	0.4	A	2.1	0.6	M	1.9	7.0
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SS-1 (BLACK LEGEND ON WHITE BACKGROUND)



2.5" RADIUS, 1.0" BORDER, 1.0" INDENT, ORANGE ON NONE;  
[WARNING] BLACK B; HORIZONTAL LINE FLUORESCENT ORANGE;  
[BRIDGE] BLACK B; [CONSTRUCTION] BLACK B;  
[500 FT.] BLACK B; [DOWNSTREAM] BLACK B;  
TABLE OF WIDTHS AND SPACES.

8.3	W	5.0	0.4	A	4.3	1.2	R	3.4	1.5	N	3.3	1.5	I	1.0	1.5	N	3.4	1.5	G	3.4	8.3
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8.0	—	32.0	8.0
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2.9	◇	15.0
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7.9	B	1.7	0.8	R	1.6	0.8	I	0.5	0.7	D	1.7	0.6	G	1.7	0.8	E	1.5	9.8
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18.9	C	1.7	0.6	O	1.8	0.7	N	1.7	0.8	S	1.7	0.6	T	1.5	0.6	R	1.6	0.8	U	1.7	0.7	C	1.7	0.4	T	1.5	0.6	I	0.5	0.8	O	1.8	0.7	N	1.7	2.9
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24.7	5	1.7	0.6	O	1.8	0.6	O	1.8	4.0	F	1.5	0.4	T	1.5	0.2	.	0.5	8.7
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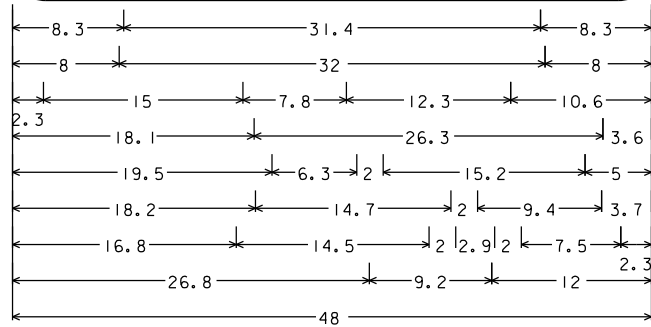
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NOT FOR CONSTRUCTION  
PRELIMINARY PLANS ARE NOT  
TO BE SUBMITTED WITH  
PERMIT  
ATON DETAIL SHEET

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
5-0	*	0080	08B	5 OF 5
WHITE HAVEN AND EAST SIDE BOROUGHES				
REVISION NUMBER	REVISIONS	DATE	BY	

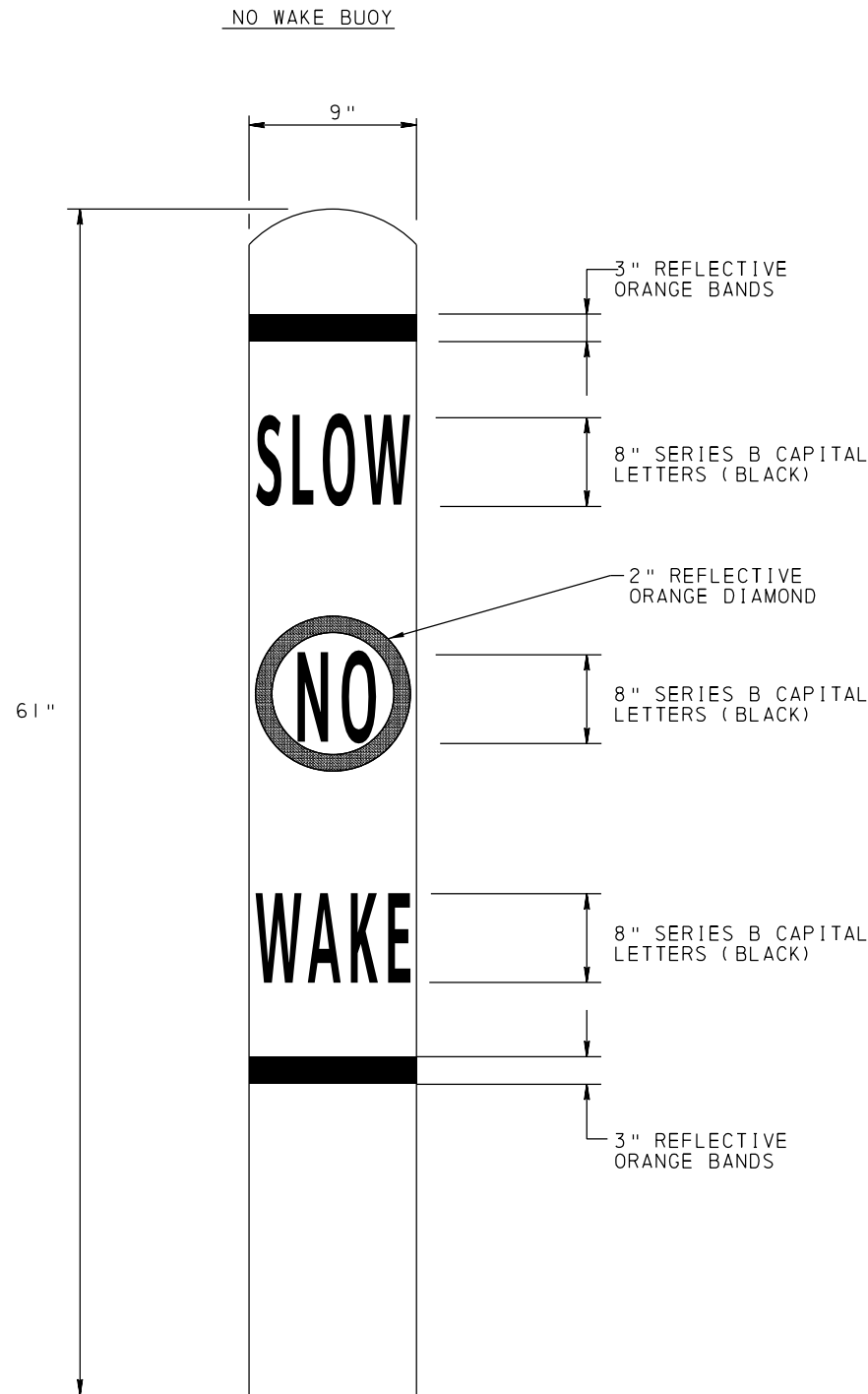
SS-2 (BLACK LEGEND ON WHITE BACKGROUND)



2.5" RADIUS, 1.0" BORDER, 1.0" INDENT, ORANGE ON WHITE;  
 [WARNING] BLACK B; [BRIDGE] BLACK B;  
 [CONSTRUCTION] BLACK B; [USE EXTREME] BLACK B;  
 [CAUTION WHEN] BLACK B; [BOATING IN THIS] BLACK B;  
 [AREA] BLACK B;

TABLE OF WIDTHS AND SPACES.

8.3	W	5.0	0.4	A	4.3	1.2	R	3.4	1.5	N	3.3	1.5	I	1.0	1.5	N	3.4	1.5	G	3.4	8.3												
8.0	32.0		8.0																														
2.3	15.0																																
7.8	B	1.7	0.7	R	1.7	0.7	I	0.5	0.8	D	1.7	0.6	G	1.7	0.7	E	1.5	10.6															
18.1	C	1.7	0.6	O	1.8	0.8	N	1.7	0.7	S	1.7	0.6	T	1.5	0.6	R	1.7	0.7	U	1.7	0.8	C	1.7	0.4	T	1.5	0.6	O	1.8	0.8	N	1.7	3.6
19.5	U	1.7	0.7	S	1.7	0.7	E	1.5																									
2.0	E	1.5	0.6	X	1.9	0.4	T	1.5	0.6	R	1.7	0.7	E	1.5	0.6	M	2.0	0.7	E	1.5	5.0												
18.2	C	1.7	0.4	A	2.2	0.6	U	1.6	0.6	T	1.5	0.6	I	0.5	0.8	O	1.8	0.7	N	1.7													
2.0	W	2.5	0.6	H	1.7	0.8	E	1.5	0.6	N	1.7	3.7																					
16.8	B	1.7	0.6	O	1.8	0.6	A	2.1	0.2	T	1.5	0.6	I	0.5	0.8	N	1.7	0.7	G	1.7													
2.0	I	0.5	0.7	N	1.7	2.0	T	1.5	0.6	H	1.7	0.8	I	0.5	0.7	S	1.7	2.3															
26.8	A	2.1	0.6	R	1.7	0.8	E	1.5	0.4	A	2.1	12.0																					

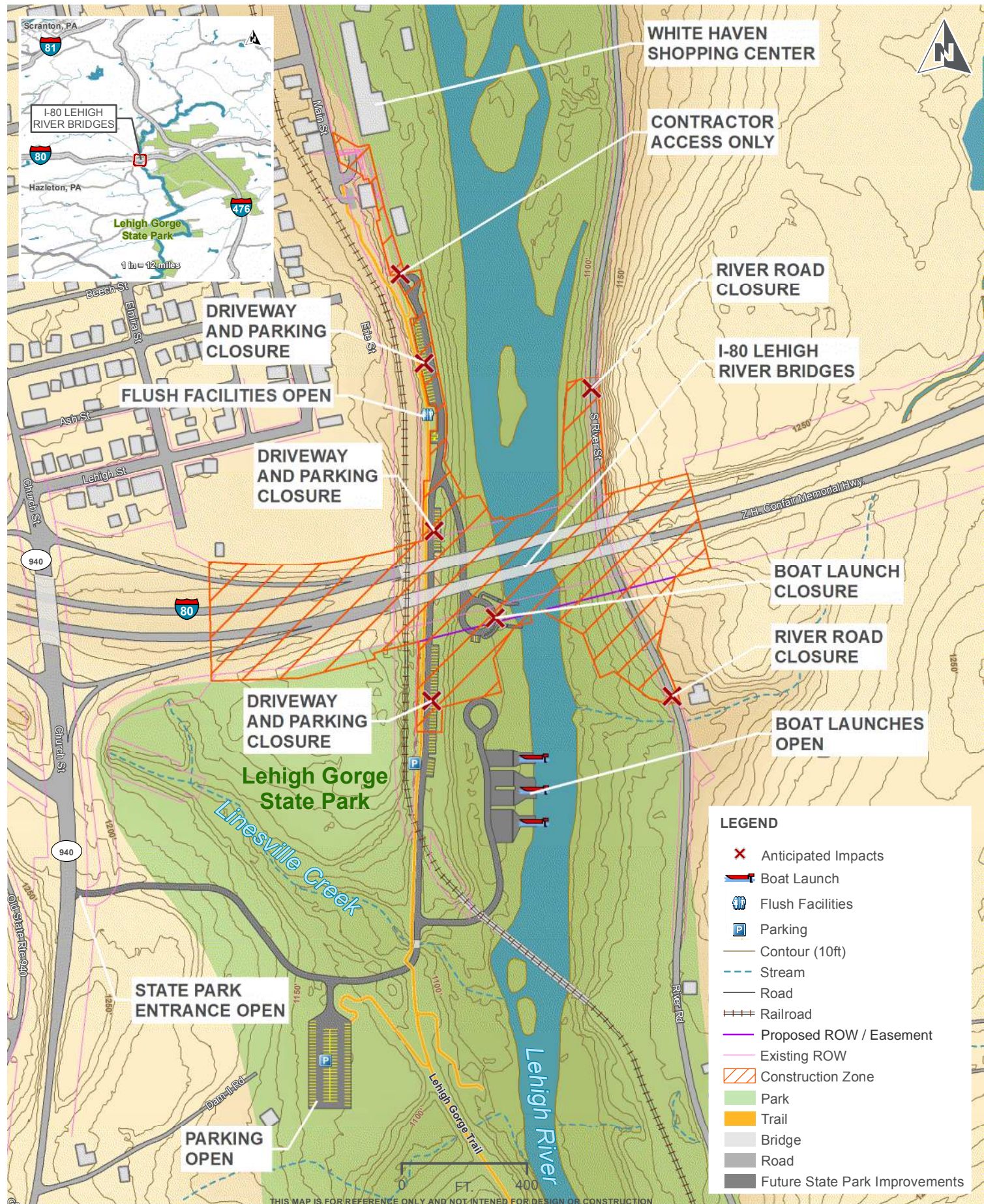


NOT FOR CONSTRUCTION  
 PRELIMINARY PLANS ARE NOT  
 TO BE SUBMITTED WITH  
 PERMIT  
 ATON DETAIL SHEET

USER:LSALOM PLOT DRIVER:Pgmdot.PDF Mono.pltcfgr PLOT DATE:05-20-2021 11:47:54 AM  
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**Attachment D**  
**Public Outreach**



**NOTICE OF PUBLIC INVOLVEMENT**  
**I-80, Section 08B, Lehigh River Bridges Project**

The Pennsylvania Department of Transportation (PennDOT) is proposing to replace the I-80 Lehigh River Bridges. The dual bridges carry I-80 over the Lehigh River, Lehigh Gorge State Park and Trail, Reading Blue Mountain & Northern Railroad, and River Road in White Haven Borough, Luzerne County, and East Side Borough and Kidder Township, Carbon County. Required Right-of-Way and Easements are necessary for the project, including approximately 7 acres from Lehigh Gorge State Park.

Construction of the I-80 bridges is expected to take approximately four years beginning in 2023. DCNR has broken ground on a capital improvements project in Lehigh Gorge State Park that is planned to be completed prior to PennDOT's bridge replacement project. Anticipated temporary impacts to Lehigh Gorge State Park property and the Lehigh River Water Trail are summarized below.

Lehigh Gorge Trail:

- April through October: Overnight trail closures are anticipated. Short-term trail closures are anticipated during daylight hours Monday through Friday using flaggers and advance signage.
- November through March: Long-term trail closures in the vicinity of the bridges are anticipated.

Lehigh Gorge State Park driveway and parking:

- The driveway and parking will be closed from the White Haven Shopping Center to the future park facilities south of the bridges.
- The future park entrance south of the bridges will remain open during construction, including the overflow parking area and a portion of the parking along the driveway.

Boat launches:

- The boat launch directly under the bridges will be closed during construction.
- The three future boat launches downstream of the bridges will remain open during construction.

Lehigh River Water Trail:

- Temporary causeways will extend from both riverbanks but will not completely block the river.
- An Aids to Navigation Plan will be implemented to ensure safe passage for boaters through the construction area and will include advance signing.

PennDOT will also provide replacement land as part of mitigation for Required Right-of-Way and Temporary Construction Easements within Lehigh Gorge State Park.

The majority of Lehigh Gorge State Park will remain open during bridge construction, and access to the boat launches and Lehigh River will be maintained. The contractor will restore disturbed areas, and full access to park amenities will be provided upon completion. PennDOT's opinion is the project will not adversely impact the recreational functions of the resources.

PennDOT is seeking public input. Those wishing to comment on the impacts of the project on Lehigh Gorge State Park and Lehigh River Water Trail, can send written comments to the address or email below. All comments should be received by October 26, 2021. If you require additional information concerning this project, contact Brandy Rotz, AICP, Consultant Project Manager (717) 540-6040, c-brotz@pa.gov.

Mr. Michael W. Rebert, P.E., District Executive  
 PennDOT Engineering District 5-0  
 1002 Hamilton Street  
 Allentown, PA 18101  
 Attn: Brandy Rotz, AICP – Consultant Project Manager  
 c-brotz@pa.gov

**LEGAL NOTICE**

IN RE: ESTATE OF LUISE H. MCFADDEN, a/k/a LUISE H. SCHILLER, Deceased, late of the Borough of Weatherly, County of Carbon and Commonwealth of Pennsylvania.

Notice is hereby given that Letters Testamentary have been granted in the Estate of Luise H. McFadden, a/k/a Luise H. Schiller, who died on the 16th day of January, 2021. All persons having claims against the estate are

requested to make known the same, and all persons indebted to the decedent to make payment without delay to the Executor, Anthony G. McFadden, 21 East Main Street, Weatherly, PA 18255, or his attorney:

**CYNTHIA S. YURCHAK, ESQUIRE**  
**121 Carbon Street**  
**Post Office Box 49**  
**Weatherly, PA 18255**

10/14

The Borough of White Haven will receive sealed bids for Lawn Mowing and Landscape Maintenance of right of ways and Borough owned properties. The bids will be received at the White Haven Municipal Building 312 Main Street, White Haven PA 18661 until 2:00 p.m. local time on Friday, October 22, 2021. The bids will be publicly opened and read aloud at the borough building. Bids will be considered by Borough Council at their meeting on October 25th, 7pm.

**ADVERTISEMENT FOR BIDS**

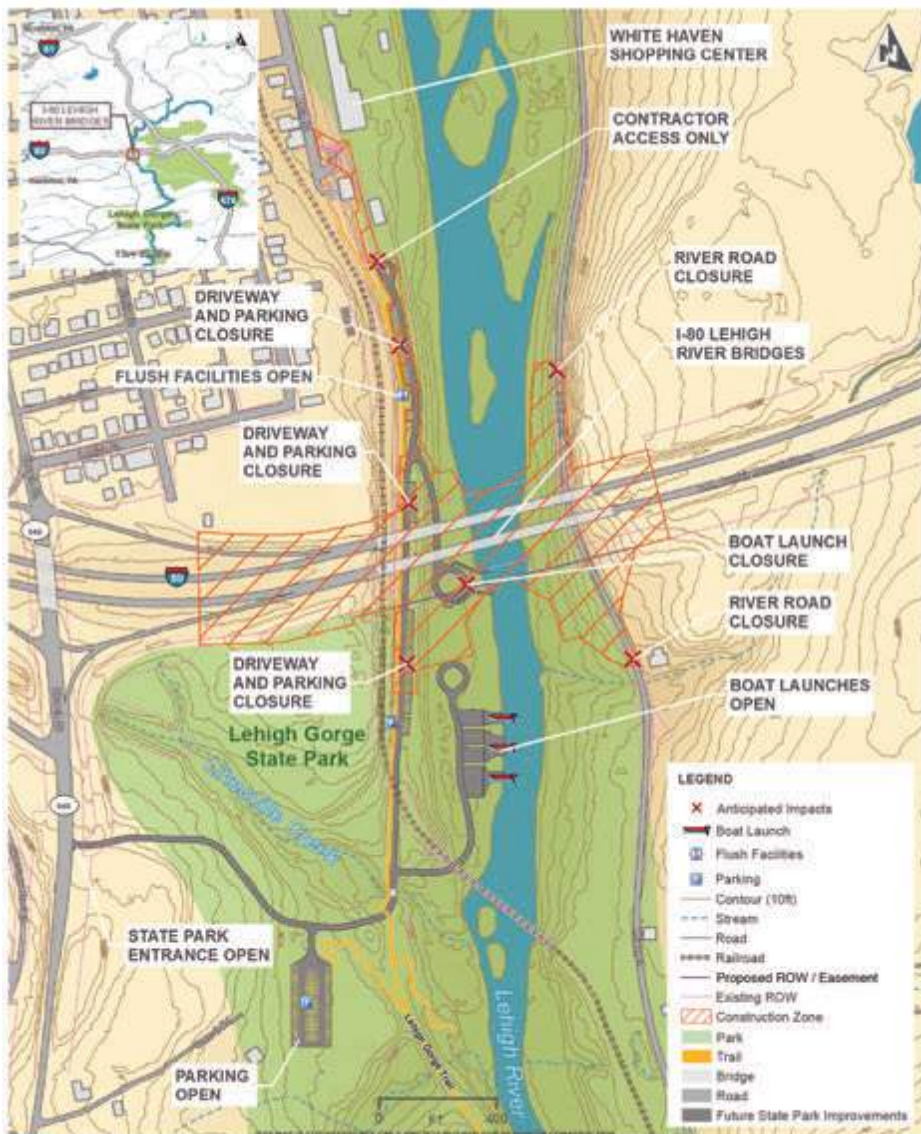
SEALED BID  
 Contractor Name  
 Contractor Address  
 Re: Lawn Mowing and Landscape Maintenance Borough of White Haven  
 Attn: Linda Szoke, Borough Manager  
 312 Main Street  
 White Haven PA 18661  
 570-443-9129

The bid packet can be emailed by emailing the Borough at [whborough@whboro.org](mailto:whborough@whboro.org) or can

be picked up at borough building beginning October 5, 2021 Bidders are welcome by appointment to meet with Borough Manager and look over locations listed for lawn maintenance

**Linda Szoke**  
**Borough Manager**  
**Zoning and Code Officer**  
**312 Main Street**  
**White Haven PA 18661**  
**570-443-9129**

10/21



**I-80 LEHIGH RIVER BRIDGE PROJECT**  
**TEMPORARY CLOSURES TO LEHIGH GORGE STATE PARK**  
 WHITE HAVEN & EAST SIDE BOROUGHS AND KIDDER TOWNSHIP  
 LUZERNE & CARBON COUNTIES, PA

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 PennDOT Engineering District 5-0  
 1002 Hamilton Street  
 Allentown, PA 18101  
 Attn: Brandy Rotz, AICP – Consultant Project Manager  
[c-brotz@pa.gov](mailto:c-brotz@pa.gov)





# THE JOURNAL-HERALD

THURSDAY, OCTOBER 14, 2021 • Volume 41 – No. 7

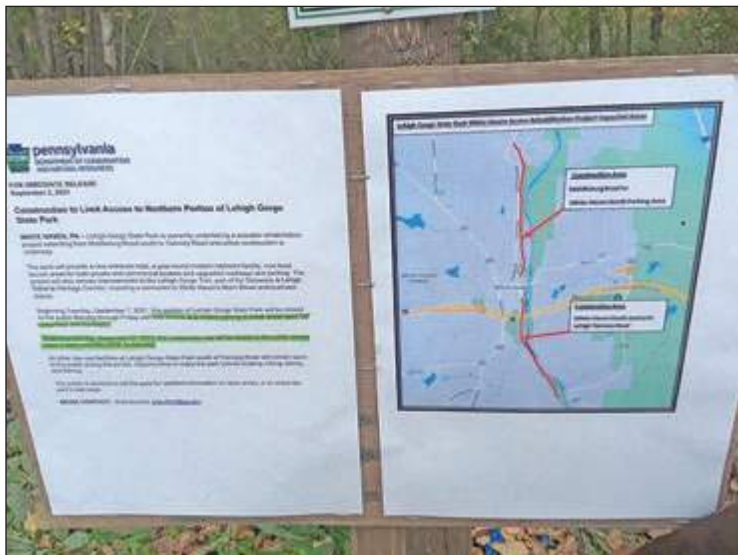
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SINGLE COPY— 75¢

(USPS 277440)

CONTINUING: *THE WHITE HAVEN JOURNAL*  
ESTABLISHED 1879–142<sup>nd</sup> YEAR, NO. 46

CONTINUING: *THE WEATHERLY HERALD*  
ESTABLISHED 1880–142<sup>nd</sup> YEAR, NO. 19



**CLOSED: Construction to the White Haven portion of Lehigh Gorge State Park will close the trail at Tannery; at left a posting gives details of the project. The fence was down over part of the weekend, but back up by Tuesday. Other signs around the parking area give details of the proposed Interstate 80 bridge project, also on page 3 of this issue. Comments on that plan are due by October 26.** *JH: Ruth Isenberg*

## Both of Kidder's Fire Companies need new equipment

by Seth Isenberg

Some fire engines in the bays of the Albrightsville (AVFD) and Lake Harmony Volunteer Fire Companies (LHVFD) have gotten old. Through 2021, both fire companies have been working on plans to get new equipment.

Kidder Township supervisors called a public meeting on September 28 to talk to the companies about what they need, and how much the new equipment will cost.

Members of each fire company understood that in order to get something new, they had to retire two of their existing trucks. The LHVFD wants to replace its two oldest vehicles with a rescue pumper, with an estimated price tag of \$836,000 if ordered this year.

The AVFD is looking to replace both fire apparatus in the Albrightsville #2 Station with one new pumper tanker, AND buy a rescue pumper with an aerial flow tower for the Albrightsville main station, at a combined cost of just over \$1.5 million.

Placing the orders on these trucks before the end of the year will save about 3%

each — over \$70,000 total for the three.

Kidder Township has about \$1.2 million in its Emergency Services Capital Equipment Fund. It has been getting \$200,000 added from the township budget each year. There was discussion as to how much the township is allowed to spend from the fund. As the ordinance governing the fund is now written, the township can only cover about 60% of the value of the trucks. The ordinance also does not allow payments over time, which will have to be amended.

It was suggested that, given how fire company fundraisers are raising less and less, the ordinance should also be amended to allow 100% funding. While the trend was acknowledged, supervisors are more likely to boost the percentage over time, and as township finances allow.

For now, the 60% limit remains in place.

Supervisors were unanimous in approving a letter to

See KIDDER FIRE, page 7



# PUBLIC NOTICES

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ESQUIRE  
121 Carbon Street  
Post Office Box 49  
Weatherly, PA 18255**

10/14

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## ADVERTISEMENT FOR BIDS

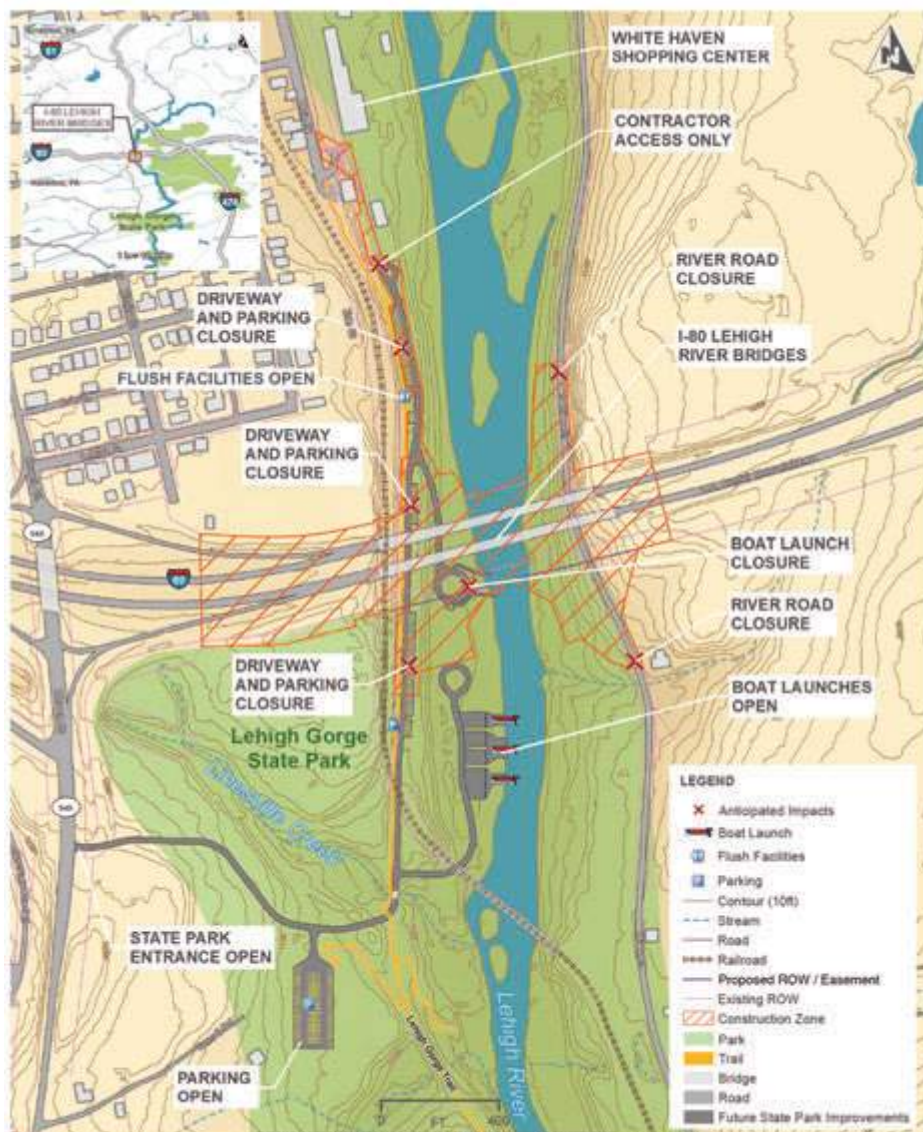
SEALED BID  
Contractor Name  
Contractor Address  
Re: Lawn Mowing and Landscape Maintenance Borough of White Haven  
Attn: Linda Szoke, Borough Manager  
312 Main Street  
White Haven PA 18661  
570-443-9129

The bid packet can be emailed by emailing the Borough at [whborough@whboro.org](mailto:whborough@whboro.org) or can

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**Linda Szoke  
Borough Manager  
Zoning and Code Officer  
312 Main Street  
White Haven PA 18661  
570-443-9129**

10/21



**I-80 LEHIGH RIVER BRIDGE PROJECT  
TEMPORARY CLOSURES TO LEHIGH GORGE STATE PARK  
WHITE HAVEN & EAST SIDE BOROUGHS AND KIDDER TOWNSHIP  
LUZERNE & CARBON COUNTIES, PA**

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PennDOT Engineering District 5-0  
1002 Hamilton Street  
Allentown, PA 18101  
Attn: Brandy Rotz, AICP – Consultant Project Manager  
[c-brotz@pa.gov](mailto:c-brotz@pa.gov)

**From:** Markowitz, Katherine  
**Sent:** Friday, January 7, 2022 2:08 PM  
**To:** Markowitz, Katherine  
**Subject:** RE: [External] Lehigh River Bridges I-80 08B Project Feedback

**From:** Cwalina Construction <cwalinaconstruction@gmail.com>  
**Sent:** Friday, October 15, 2021 2:43 PM  
**To:** Rotz, Brandy <c-brotz@pa.gov>  
**Subject:** [External] Lehigh River Bridges I-80 08B Project Feedback

**ATTENTION:** *This email message is from an external sender. Do not open links or attachments from unknown sources. To report suspicious email, forward the message as an attachment to CWOPA\_SPAM@pa.gov.*

Hi,

In response to your request for public feedback, I'd like you to consider noise reduction barriers on the bridge. We reside at 100 Towanda St in White Haven (directly up the path of the river to the other side of town) and the truck traffic traveling on the bridge causes significant noise to echo up the river which can be heard in the house even if the windows are closed. Additionally, a barrier might deter any people from attempting suicide off the bridge.

Is it possible to deaden sound, keep the public safe and keep the aesthetic view of the town as part of this project?

Thank you,

Valerie Love  
570-947-2690

--

Brian Cwalina  
Cwalina Construction  
570-606-6016  
[cwalinaconstruction@gmail.com](mailto:cwalinaconstruction@gmail.com)

**From:** Markowitz, Katherine  
**Sent:** Friday, January 7, 2022 2:15 PM  
**To:** Markowitz, Katherine  
**Subject:** RE: [External] White Haven bridge - Public Comment

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**From:** Barbara Murphy <bmurphy@pa.metrocast.net>  
**Sent:** Tuesday, October 26, 2021 2:34 PM  
**To:** Rotz, Brandy <c-brotz@pa.gov>  
**Subject:** [External] White Haven bridge

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Consultant Project Manager  
Brandy Rotz,

As a concerned citizen/neighbor of the Borough of White Haven we would like to know if the proposed new bridge (east and west bound route 80) will be built in the exact same area as the existing bridge? If so how will this be accomplished? For instance: lane closures , new on and off ramps which could possibly include taking land from our Lehigh Ballpark. How will the construction impact our local neighborhood? Please respond to my email [bmurphy@pa.metro cast.net](mailto:bmurphy@pa.metrocast.net) or [patmurf81@gmail.com](mailto:patmurf81@gmail.com). Thank you ,

Patrick and Barbara Murphy  
909 Elmira St.  
White Haven, PA 18661

Sent from my iPhone

**From:** Markowitz, Katherine  
**Sent:** Friday, January 7, 2022 2:15 PM  
**To:** Markowitz, Katherine  
**Subject:** RE: [External] White Haven bridge project

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**From:** Seth Isenberg <journalseth@pa.metrocast.net>  
**Sent:** Tuesday, October 26, 2021 11:21 PM  
**To:** Rotz, Brandy  
**Cc:** Seth Isenberg  
**Subject:** [External] White Haven bridge project

ATTENTION: This email message is from an external sender. Do not open links or attachments from unknown sources. To report suspicious email, forward the message as an attachment to CWOPA\_SPAM@pa.gov.

Brandy Rotz  
Consultant Project Manager

October 26, 2021

Four years of closure of the Lehigh Gorge Heritage Corridor trail could be very damaging to the economy of White Haven borough. Even the temporary closure of the trail this year has had a measurable impact on plaza retailers this month - as trail traffic is just not around even during the foliage season.

Looking at the plans, White Haven's downtown will be cut off from the south trail for an amazing four years. This year alone, local businesspeople were working with the Heritage Corridor to enhance the trail experience. We were looking to establish a "trail tenders" group of volunteers as ambassadors to visitors who start their trip from here, or would be passing through.

As a Main Street business ourselves, The Journal-Herald was hoping to see continued reviving of our downtown. Slowly, we have seen stores open, and recently, there is a prospect of a new building on Main Street. Losing the tourists is going to hurt. We are going to need some help from PennDOT, including highway signs indicting that we have a downtown business district, and signs on the highway for the remaining downtown trailhead.

Is it also possible for help to route bikers and hikers through town via the new driveway? Trail travelers may take the detour as an adventure.

I will add my concerns about any traffic that comes in heavy onto Route 940 that clogs up White Haven's roads. When there is an accident and the highway is closed, the simple task of pulling out from Main Street can become a ten to 15 minute wait. Through vehicles will not yield. Going to the grocery store or drug store becomes a fight with trucks and cars stuck on our little state road. We also have foot traffic, mainly young people, and I am very concerned about their safety when Route 940 becomes the only path for Interstate-using vehicles.

If an accident with a detour happens in bad weather, vehicles get stuck on the big hill on Route 940.



So, as a wrap up -

I am asking for PennDOT to provide signs to aid the downtown commercial area, and signs to promote the northern trailhead in the downtown to help draw visitors.

I am also asking for help to improve the local roads to handle the surges in traffic, and an answer to the safety problem within White Haven when bumper to bumper traffic does appear.

Seth Isenberg

General manager

The Journal-Herald of White Haven

LAKE NEWS of Lake Harmony

Journal of the Pocono Plateau of Blakeslee and Tobyhanna

And

Journal of Penn-Kidder of Albrightsville

211 Main Street, White Haven, PA 18661

570 215-0204

Comment Date	Comment Origin	Comment	Comment Tone	Concerns	Response	First Name (Person)	Last Name (Person)	Org-Company	Email (Person)	Phone (Person)	Project Association
12/16/2021	web comment	<p>The Delaware and Lehigh National Heritage Corridor (D&amp;L Trail) needs to be kept in the chain of communication for this project along with the State Park. The D&amp;L was noted in some locations of this public meeting post, but not consistently. The trail within this project scope is part of a currently 145-mile network, soon to be 165+ miles- any notice regarding the disruption of trail activities must be shared with the D&amp;L so the trail users can be accurately updated. Please add the D&amp;L to your list of organizations to keep informed (this webpage noted DCNR and White Haven).</p> <p>For the timelines regarding trail closures- Please clarify the YEAR of these anticipated impacts to the D&amp;L Trail- is the schedule outlined for the full 4 years of the project or just a portion? Are the April to October overnight closures M-F or 7 days a week? When is "night" considered to begin- will this line up with the State Park's hours of operation? Is the November to March closure M-F or 7 days a week?</p> <p>Can you expand on the "replacement" land PennDOT will provide? Does the bridge permanently impact the current alignment of the D&amp;L Trail in any way and is PennDOT considering "replacing" any portion of the D&amp;L Trail if the alignment is affected?</p> <p>When the capacity analysis was conducted did it account for pedestrian and bicycle traffic utilizing the D&amp;L Trail along Main Street? White Haven Borough is exploring intersection improvements for pedestrians (RRFB's and crosswalk repainting like you mentioned)- how might this new "stop point" affect traffic congestion and does this need to be considered when evaluating the traffic controls on adjacent intersections?</p> <p>Please note when you conduct your before/after intersection study that the D&amp;L Trail is currently closed during the State Park improvements project and any trail user counts will not be accurate until the project is complete.</p>	neutral	Information Request; Trails/Recreation/Parks		Elizabeth	Rosencrans	Delaware and Lehigh National Heritage Corridor (D&L Trail)	trail@delawareandlehigh.org	6109233548227	I-80 White Haven

**Attachment E**  
**Concurrence Letters**



February 22, 2022

Paul Fawcett  
Chief, Waterways and Marina Management Branch  
PA Fish and Boat Commission, Bureau of Boating  
PO Box 6700  
Harrisburg, PA 17106-7000

Re: I-80, Section 08B Bridges over Lehigh River Project  
White Haven Borough, Luzerne County and  
East Side Borough and Kidder Township, Carbon County, PA  
Requesting Concurrence with Section 4(f) Determination

Dear Mr. Fawcett:

The Pennsylvania Department of Transportation (PennDOT) proposes to replace I-80 Section 08B Lehigh River Bridges. The dual bridges carry I-80 over the Reading Blue Mountain Northern (RBMN) railroad, Lehigh Gorge State Park and Trail, Lehigh River, and SR 1005 (River Road), in White Haven Borough, Luzerne County, and East Side Borough and Kidder Township, Carbon County, Pennsylvania.

The bridges are proposed to be replaced using staged construction maintaining two lanes of traffic in both directions during construction. The eastbound bridge will be replaced on a new alignment immediately south of the existing eastbound bridge. The westbound bridge will be replaced on the same approximate alignment as the existing westbound bridge.

The project will have temporary impacts to the Lehigh River but will not adversely affect the Lehigh River Water Trail. Temporary impacts to State Park property and facilities, including one boat launch directly under the I-80 bridges, are anticipated. The majority of the State Park will remain open during bridge construction, and access to the Lehigh River will be maintained via the three new boat launches currently being constructed by DCNR south of the I-80 bridges (the existing boat launch under the bridges will be temporarily closed). The contractor will restore disturbed areas upon completion.

An Aids to Navigation (ATON) Plan has been prepared and approved by Colonel Clyde Warner, PFBC Bureau of Law Enforcement. Implementation of the ATON Plan is included as a mitigation commitment for the project. PennDOT has agreed to additional mitigation measures through consultation with DCNR and PFBC.

As the work will occur over, around, and in the Lehigh River and will temporarily affect the Lehigh River Water Trail, a recreational facility under your jurisdiction, we are requesting your concurrence in writing with PennDOT's determination that the project will not adversely affect the recreational use of the Lehigh River Water Trail. We have attached for your review a draft copy of the Section 4(f) De Minimis form, which provides details of the anticipated impacts to the Lehigh River Water Trail and proposed mitigation measures.

Thank you for your assistance in this matter.


If you have any questions, please feel free to contact me at 610-871-4550 or via email, [svottero@pa.gov](mailto:svottero@pa.gov)

Sincerely,



Scott G. Vottero, PE  
Acting Assistant District Executive – Design  
PennDOT District 5-0

Enclosures: Lehigh River Draft Section 4(f) De Minimis Form

Concurrence:  Date: 2/25/2022  
Paul Fawcett  
Chief, Waterways and Marina Management Branch  
Bureau of Boating  
PA Fish and Boat Commission



February 21, 2022

Kelly Rossiter, AICP  
Rivers Program Specialist  
PA Department of Conservation and Natural Resources  
400 Market Street, 5<sup>th</sup> Floor  
Harrisburg, PA 17101-2301

Re: I-80, Section 08B Bridges over Lehigh River Project  
White Haven Borough, Luzerne County and  
East Side Borough and Kidder Township, Carbon County, PA  
Requesting Concurrence with Section 4(f) Determination

Dear Ms. Rossiter:

The Pennsylvania Department of Transportation (PennDOT) proposes to replace I-80 Section 08B Lehigh River Bridges. The dual bridges carry I-80 over the Reading Blue Mountain Northern (RBMN) railroad, Lehigh Gorge State Park and Trail, Lehigh River, and SR 1005 (River Road), in White Haven Borough, Luzerne County, and East Side Borough and Kidder Township, Carbon County, Pennsylvania.

The bridges are proposed to be replaced using staged construction maintaining two lanes of traffic in both directions during construction. The eastbound bridge will be replaced on a new alignment immediately south of the existing eastbound bridge. The westbound bridge will be replaced on the same approximate alignment as the existing westbound bridge.

The project will have temporary impacts to the Lehigh River but will not adversely affect the scenic qualities of the Lehigh River. Temporary impacts to Lehigh Gorge State Park property and facilities, including one boat launch directly under the I-80 bridges, are anticipated. The majority of the State Park will remain open during bridge construction, and access to the Lehigh River will be maintained via the three new boat launches currently being constructed by DCNR south of the I-80 bridges (the existing boat launch under the bridges will be temporarily closed). The contractor will restore disturbed areas upon completion.

An Aids to Navigation (ATON) Plan has been prepared and approved by Colonel Clyde Warner, PFBC Bureau of Law Enforcement. Implementation of the ATON Plan is included as a mitigation commitment for the project. PennDOT has agreed to additional mitigation measures through consultation with DCNR and PFBC.

As the work will occur over, around, and in the Lehigh River and will temporarily affect the Lehigh River Water Trail and PA Scenic River, a recreational facility under your jurisdiction, we are requesting your concurrence in writing with PennDOT's determination that the project will not adversely affect the scenic qualities of the Lehigh River. We have attached for your review a draft copy of the Section 4(f) De Minimis form, which provides details of the anticipated impacts

to the Lehigh River and proposed mitigation measures. The attached Section 4(f) Evaluation is specific to the Lehigh River Water Trail and PA Scenic River. A Section 4(f) Evaluation for de minimis use of the Lehigh Gorge State Park has been prepared and is being coordinated separately with PA DCNR Bureau of State Parks.

Thank you for your assistance in this matter.

If you have any questions, please feel free to contact me at 610-871-4550 or via email, [svottero@pa.gov](mailto:svottero@pa.gov)

Sincerely,



Scott G. Vottero, PE  
Acting Assistant District Executive – Design  
PennDOT District 5-0

Enclosures: Lehigh River Draft Section 4(f) De Minimis Form

Concurrence: *Kelly Rossiter* Date: 2/28/2022  
Kelly Rossiter, AICP  
Rivers Program Specialist  
PA Department of Conservation and Natural Resources



On Behalf of the Federal Highway Administration—Pennsylvania Division Office

**Determination of Section 4(f) De Minimis Use  
Section 2002 No Adverse Use  
Public Parks, Recreation Areas,  
Wildlife and/or Waterfowl Refuges,  
State Forest Land, and State Game Land  
May 2014 Version**

<b>County:</b>	Luzerne and Carbon	<b>State Route:</b>	I-80	<b>Section:</b>	08B
<b>Project Name:</b>	I-80 Lehigh River Bridges	<b>FPN:</b>	Enter FPN.	<b>MPMS:</b>	99552

**SELECT ONE:**     EIS                       EA                       CE                       EER                       ED

**PROJECT DESCRIPTION:**

(Provide a concise but thorough description of the proposed action.)

The proposed project consists of the replacement of the I-80 Section 08B Lehigh River Bridges. The dual bridges carry I-80 over the Reading Blue Mountain Northern (RBMN) railroad, Lehigh Gorge State Park and Trail, Lehigh River, and SR 1005 (River Road), in White Haven Borough, Luzerne County, and East Side Borough and Kidder Township, Carbon County, Pennsylvania.

The purpose of the project is to address the deterioration of the I-80 Lehigh River bridges and thereby provide safe and structurally sufficient bridges that will provide connectivity for interstate travelers, commuters, commercial users, emergency services, tourists, and residents.

The needs for this project include:

- The existing I-80 dual bridges are more than 50 years old. The fracture critical bridges show deterioration and are approaching the end of their serviceable lifespan.
- I-80 provides a critical link in the interstate commerce system, and as such is vital to the efficient movement of goods and people. Further deterioration of the I-80 Lehigh River bridges could cause the bridges to be load posted.

Originally constructed in 1965, the I-80 Eastbound and Westbound structures are 836 feet long and 757 feet long, respectively. The structures consist of steel two-girder superstructures. The substructures consist of reinforced concrete abutments and reinforced concrete piers that reach heights of over 110 feet.

I-80 in the vicinity of the project has two 12' lanes of travel in both directions with 8' left shoulders and 10' right shoulders. The median width varies from 60' to 150'. This section of I-80 carries east-west traffic across Pennsylvania's northern tier with Average Daily Traffic (ADT) of 28,774 vehicles per day (2021) with 34 percent truck traffic and is projected to grow to 53,736 vehicles per day by 2046 (design year).

The Eastbound bridge will be replaced on a new alignment immediately to the south of the existing Eastbound bridge. The new Eastbound alignment will tie into the existing alignment approximately 1370 feet to the west of the bridge and 1310 feet to the east of the bridge. The





On Behalf of the Federal Highway  
Administration—Pennsylvania Division Office

**Determination of Section 4(f) De Minimis Use**  
**Section 2002 No Adverse Use**  
**Public Parks, Recreation Areas,**  
**Wildlife and/or Waterfowl Refuges,**  
**State Forest Land, and State Game Land**  
**May 2014 Version**

<b>County:</b>	Luzerne and Carbon	<b>State Route:</b>	I-80	<b>Section:</b>	08B
<b>Project Name:</b>	I-80 Lehigh River Bridges	<b>FPN:</b>	Enter FPN.	<b>MPMS:</b>	99552

Westbound bridge will be replaced on the same approximate alignment as the existing Westbound bridge. At Exit 273 (SR 940), the Eastbound on-ramp will be reconstructed on a new alignment shifted south of existing, and the Westbound off-ramp will be reconstructed on the same approximate alignment as the existing ramp.

The project includes the extension and repair of the existing cast-in-place reinforced concrete box culvert which carries an Unnamed Tributary to the Lehigh River under I-80, approximately 1000 feet to the east of the Lehigh River bridges. The existing wing walls and approximately 8 feet of the existing culvert will be removed and replaced with a new section of 39- and 1/2-foot-long reinforced box culvert.

The project also includes the incorporation of an open road cashless tolling system, including a new toll gantry on I-80 Eastbound, a utility shed, utility service, and advance signing. The limits of the work for the cashless tolling system and associated features overlap and extend beyond the limits of work for the bridge replacement project. The original project study area is shown on Figure 1. The expanded project study area after addition of the tolling facility to the project is shown on Figure 2.

Tolling the I-80 Eastbound bridge over the Lehigh River is expected to result in some traffic diverting to adjacent roadways by drivers to avoid paying the toll. Traffic studies were conducted and identified a primary diversion route consisting of SR 940 and SR 537. Through further traffic evaluation to determine the impact of toll diversion, traffic improvements along the diversion route were proposed. One proposed traffic improvement is to replace an existing gabion wall and guide rail along SR 940 in East Side Borough northeast of the SR 940 bridge over Lehigh River. This gabion wall is failing and causing the roadway and pavement to settle. The approximate location of the gabion wall is shown in Figure 3. SR 940 at this location is adjacent to Lehigh Gorge State Park.

**IDENTIFICATION OF SECTION 4(f)/SECTION 2002 PROPERTY:**

(List the property and provide a description of the property as per Chapter 6 of the *Section 4(f)/Section 2002 Handbook*. Attach a map, photo(s), etc. as appropriate.)

Lehigh Gorge State Park (LGSP) extends on both east and west sides of the Lehigh River on land owned by the Commonwealth of Pennsylvania and administered by PA Department of Conservation and Natural Resources (DCNR). According to DCNR's website, the 6,107-acre park follows the Lehigh River from Francis E. Walter Dam north of I-80 to Jim Thorpe to the south. Figure 4 shows the area of LGSP in the vicinity of the I-80 bridges.



On Behalf of the Federal Highway Administration—Pennsylvania Division Office

**Determination of Section 4(f) De Minimis Use  
Section 2002 No Adverse Use  
Public Parks, Recreation Areas,  
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May 2014 Version**

<b>County:</b>	Luzerne and Carbon	<b>State Route:</b>	I-80	<b>Section:</b>	08B
<b>Project Name:</b>	I-80 Lehigh River Bridges	<b>FPN:</b>	Enter FPN.	<b>MPMS:</b>	99552

The current main entrance to the LGSP in the project area is from Main Street in White Haven Borough. From the LGSP park entrance, a shared-use trail extends north to south in abandoned railroad grade along the river and provides opportunities for hiking, bicycling, sightseeing, and photography. The majority of the trail is closed to motor vehicles, although the entrance from Main Street to a parking area near the I-80 bridges is open for motor vehicle access. Within the state park, the trail is named the Lehigh Gorge Trail. According to DCNR’s LGSP website, the Lehigh Gorge Trail follows more than 20 miles of the Delaware & Lehigh (D&L) Trail, the foundation of the 165-mile Delaware & Lehigh National Heritage Corridor.

The Lehigh Gorge Trail extends north from the LGSP northern property line near the Main Street entrance within a public easement DCNR procured for the purpose of connecting the trail to LGSP property at the northern end of White Haven Borough. For the purposes of this Section 4(f) Evaluation, the trail segment within DCNR Easement is included along with the Lehigh Gorge Trail within LGSP because it is one continuous trail and has the same Official With Jurisdiction (DCNR).

After crossing through private property within DCNR Easement, the D&L Trail continues north following Main Street public right-of-way in White Haven Borough.

**FOR PARKS, IDENTIFY KEY COMPONENTS OF ANY EXISTING MANAGEMENT PLAN (if it exists):**

The LGSP does not have an existing management plan.

**OFFICIAL WITH JURISDICTION OVER SECTION 4(f)/SECTION 2002 PROPERTY:**

1. Identify agency with jurisdiction:  
Pennsylvania Department of Conservation and Natural Resources (DCNR)
  
2. Name and title of contact person at agency:  
Rex Bradish, Manager, Hickory Run State Park Complex  
Ryan Dysinger, Assistant Director, PA DCNR Bureau of State Parks



On Behalf of the Federal Highway  
Administration—Pennsylvania Division Office

**Determination of Section 4(f) De Minimis Use**  
**Section 2002 No Adverse Use**  
**Public Parks, Recreation Areas,**  
**Wildlife and/or Waterfowl Refuges,**  
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**May 2014 Version**

<b>County:</b>	Luzerne and Carbon	<b>State Route:</b>	I-80	<b>Section:</b>	08B
<b>Project Name:</b>	I-80 Lehigh River Bridges	<b>FPN:</b>	Enter FPN.	<b>MPMS:</b>	99552

**APPLICABILITY DETERMINATION:**

1. Provide the total acreage of the property: Lehigh Gorge State Park occupies 6,107 acres.

Describe the use of land from the property (identify amount of the property to be used, including temporary and permanent acquisition):

The proposed bridge replacement project will take approximately 0.1 acre Required Right-of-Way, 0.4 acre of Proposed Slope Easement, 0.5 acre Proposed Aerial Easements, 4.9 acres of Temporary Construction Easements (TCE) within LGSP, and 0.3 acres of TCE within DCNR Easement. Proposed Slope Easement is necessary to replace the eastbound bridge on an alignment south of the existing bridge. Proposed Aerial Easement is needed for future access, maintenance, and construction. TCE is necessary to provide contractor access and staging. Required Right-of-Way is for replacement of the failing gabion wall along SR 940 in East Side Borough. Construction of the bridge replacements is expected to take approximately four years. Figure 4, Section 4(f) / 6(f) Location Map, shows impact areas and includes acreages in the legend. Because the failing gabion wall along SR 940 is a distance away from the bridge replacement project, it is shown on separate location map (Figure 3). A conceptual plan for the replacement wall is Figure 5.

DCNR has initiated a capital improvements project that started in 2021 and is expected to be completed in 2022, prior to PennDOT's bridge replacement project. DCNR's project will construct a new LGSP entrance from SR 940 south of the I-80 bridges. In addition to the new entrance, improvements will include parking lots, flush facilities, and four boat launches. DCNR intends for all park vehicular traffic, including commercial biking and boating outfitters, to utilize the new park entrance from SR 940. Anticipated temporary impacts to park property and facilities, including the future facilities, are described below and shown on Figure 6.

**Lehigh Gorge Trail:**

- April through October: Overnight closure of the trail (during hours that the state park is closed) will be allowed. Short-term closure of the trail (up to 15 minutes) will be allowed during daylight hours Monday through Friday using flaggers and advance signage.
- November through March: Long-term closure of the trail will be allowed for certain construction activities including bridge superstructure demolition, pier demolition, pier construction, and setting of beams and deck forms, with consideration of working room and safety.



On Behalf of the Federal Highway Administration—Pennsylvania Division Office

**Determination of Section 4(f) De Minimis Use**  
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<b>County:</b>	Luzerne and Carbon	<b>State Route:</b>	I-80	<b>Section:</b>	08B
<b>Project Name:</b>	I-80 Lehigh River Bridges	<b>FPN:</b>	Enter FPN.	<b>MPMS:</b>	99552

- Advance notice of trail closures will be provided to DCNR, D&L National Heritage Corridor, the Borough of White Haven, and the public.
- The contractor may temporarily realign the trail through the project site if approved by DCNR. A 10’ wide trail will be maintained except as needed for closures noted above.

LGSP driveway and parking:

- The driveway and parking will be closed from the White Haven Shopping Center to the future park facilities south of the bridges. If necessary, reconfiguration of the driveway and parking due to pier locations will be coordinated with DCNR.
- The future main park entrance south of the bridges will remain open during construction, including the overflow parking area and a portion of the parking along the driveway.

Boat launches:

- The boat launch directly under the bridges will be closed during construction. The planned improvements in this area, including upgraded boat launch, turnaround and handicap parking will be disturbed and will be reconstructed at the end of the project. These facilities may need to be reconfigured depending upon the chosen bridge pier location. Any such reconfiguration will be coordinated with DCNR.
- The three future boat launches to the south (downstream) of the bridges will not be impacted and will remain open during construction. These boat launches can be used for commercial outfitters and general public during construction.

LGSP flush facilities:

- The future flush facilities north of the bridges will not be disturbed.
- Access to these facilities from the Lehigh Gorge Trail will be maintained, though access to the facilities from south of the bridges will be cut off during trail closures as discussed above. Temporary portable facilities can be provided south of the bridges when the trail is closed at the bridges.
- Public vehicular access to the flush facilities will not be provided.
- DCNR access to the flush facilities from the driveway, if needed, will be coordinated with the contractor.



On Behalf of the Federal Highway Administration—Pennsylvania Division Office

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Lehigh River:

- Temporary causeways will extend from both riverbanks but will not extend completely across the river.
- Boat traffic through the bridge construction site will be maintained for most of construction. Temporary closures of boat traffic will be allowed for certain activities, including bridge demolition, construction of piers near or within the river, and setting of beams.
- An Aids to Navigation Plan will be implemented and will include advance signing. Signs will be placed at boat launches upstream of the project. Additional advanced notification to PFBC, DCNR and the public will be provided.

Lehigh Gorge State Park:

- Proposed Slope Easement of approximately 0.4 acre and Proposed Aerial Easements of approximately 0.5 acre are necessary due to shifting the eastbound bridge and on-ramp to the south of existing alignment.
- Temporary Construction Easements are needed on both sides of the river, totaling approximately 5.2 acres within LGSP and DCNR Easement, to provide contractor access and staging areas.
- Approximately 0.1 acre of Require Right-of-Way is anticipated for replacement of the failing gabion wall along SR 940 in East Side Borough.

2. The project **does not** adversely affect the activities, features, and attributes of the resource that qualify it for protection under Section 4(f) or Section 2002. **(If this statement cannot be verified as true, de minimis/no adverse use does not apply.)**  YES

Describe the effect to the qualities, activities, features, or attributes of the resource that qualify it for protection. Include a description of any mitigation included when making the determination regarding effects to the resource:

The project will not adversely affect the Lehigh Gorge State Park. The majority of the state park will remain open during bridge construction, and access to the Lehigh River via the three new boat launches currently being constructed by DCNR south of the bridges will be maintained (the existing boat launch under the bridges will be temporarily closed). Access to the Lehigh Gorge Trail will be maintained; however, a portion of the trail may be closed under the bridge during low and no usage timeframes during construction. The contractor



On Behalf of the Federal Highway  
Administration—Pennsylvania Division Office

**Determination of Section 4(f) De Minimis Use**  
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**Public Parks, Recreation Areas,**  
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**May 2014 Version**

<b>County:</b>	Luzerne and Carbon	<b>State Route:</b>	I-80	<b>Section:</b>	08B
<b>Project Name:</b>	I-80 Lehigh River Bridges	<b>FPN:</b>	Enter FPN.	<b>MPMS:</b>	99552

will restore disturbed areas, and full access to the park amenities will be provided upon completion. The area within the Proposed Aerial Easement adjacent to the bridges will be available for use by LGSP after construction.

Mitigation measures will include:

- Access to and use of the Lehigh Gorge Trail will be maintained to the extent possible during construction:
  - April through October: Overnight closure of the trail (during hours that the state park is closed) will be allowed. Short-term closure of the trail (up to 15 minutes) will be allowed during daylight hours Monday through Friday using flaggers and advance signage.
  - November through March: Long-term closure of the trail will be allowed for certain construction activities including bridge superstructure demolition, pier demolition, pier construction, and setting of beams and deck forms, with consideration of working room and safety.
  - Advance notice of trail closures will be provided to DCNR, D&L National Heritage Corridor, the Borough of White Haven, and the public.
  - The contractor may temporarily realign the trail through the project site if approved by DCNR. A 10' wide trail will be maintained except as needed for closures noted above.
- If necessary due to pier locations, reconfiguration of the driveway, parking, or boat launches will be coordinated with DCNR.
- An Aids to Navigation Plan will be implemented and will include advance signing. Signs will be placed at boat launches upstream of the project. Additional advanced notification to PFBC, DCNR, and the public will be provided.
- The contractor will restore disturbed areas, and full access to the park amenities will be provided upon completion. As part of restoring the disturbed areas, a planting plan will be prepared in coordination with DCNR during Final Design.
- PennDOT will coordinate with DCNR and White Haven Borough to provide wayfinding signs.

LGSP is also protected under Section 6(f) of the Land and Water Conservation Fund Act. PennDOT will provide replacement land for Section 6(f) conversion (Required Right-of-Way, Proposed Slope Easement, Proposed Aerial Easements, and TCE) within LGSP totaling approximately 6 acres. This replacement land will also serve as mitigation of the



On Behalf of the Federal Highway Administration—Pennsylvania Division Office

**Determination of Section 4(f) De Minimis Use**  
**Section 2002 No Adverse Use**  
**Public Parks, Recreation Areas,**  
**Wildlife and/or Waterfowl Refuges,**  
**State Forest Land, and State Game Land**  
**May 2014 Version**

<b>County:</b>	Luzerne and Carbon	<b>State Route:</b>	I-80	<b>Section:</b>	08B
<b>Project Name:</b>	I-80 Lehigh River Bridges	<b>FPN:</b>	Enter FPN.	<b>MPMS:</b>	99552

Section 4(f) use of LGSP. The replacement land being provided is forested land; therefore, the replacement land offsets the removal of trees in the construction staging area, and in the area where the existing gabion wall along SR 940 will be replaced.

3. The public was afforded an opportunity to review and comment on the effects of the project on the protected activities, features, and attributes of the resource.  YES

Identify the opportunity(ies) for public comment and describe the input received (provide attachments as appropriate to document the public involvement activity):

A public notice was prepared and included a map of LGSP amenities and a written notice describing the anticipated impacts to LGSP (Attachment C). The notice requested public input and provided an email and physical address for comments. On September 24, 2021, laminated copies of the public notice were delivered to DCNR for posting in LGSP and nearby Hickory Run State Park. On the same day, the public notice was emailed to White Haven Borough, East Side Borough, Kidder Township, D&L National Heritage Corridor, and the whitewater and bicycle outfitters that operate in the park. Additionally, the public notice was handed out to local businesses in White Haven Borough. Upon the suggestion of LGSP Manager, Rex Bradish, a newspaper advertisement was published in the local newspaper, the Journal-Herald. Proof of publication in the October 7, 2021, and October 14, 2021 issues of the Journal-Herald are included in Attachment C.

The public comment period began September 24, 2021 and ended October 26, 2021. Three comments were received during this 30-day comment period. Only one comment was related to Section 4(f), LGSP, and the potential impacts to park amenities and visitors. The comment from a local resident and business owner included concerns of negative affects to White Haven Borough’s local economy due to a four-year closure of the Lehigh Gorge Trail and requested highway signs to the downtown business district. Comments are included in Attachment C.

LGSP’s Manager has reported that this comment and request for signs was voiced at a prior public meeting conducted for DCNR’s LGSP improvement project, which moves the state park main entrance from its current location in downtown White Haven to SR 940 south of I-80. PennDOT will coordinate with DCNR and White Haven Borough to provide wayfinding signs.



On Behalf of the Federal Highway Administration—Pennsylvania Division Office

**Determination of Section 4(f) De Minimis Use**  
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In addition to the public outreach conducted September 24, 2021, to October 26, 2021, one written comment related to the Lehigh Gorge Trail was received from D&L National Heritage Corridor during the NEPA public meeting and comment period November 17, 2021 to December 17, 2021. PennDOT will continue coordination with D&L National Heritage Corridor as the project progresses. The full comment is included in the Attachment C.

4. The official with jurisdiction over the property was informed of FHWA's and/or PennDOT's intent to make a *de minimis*/no adverse use finding.  YES

Identify the method used to notify the official with jurisdiction, and attach appropriate correspondence.

A letter describing the impacts to Lehigh Gorge State Park and PennDOT's intention of making a Section 4(f) De Minimis Use determination was provided to DCNR. Mitigation measures were detailed in the letter. A signed copy of the letter is attached to this form.

Correspondence documenting notification of the official with jurisdiction is included in the following Attachment: Section 4(f) De Minimis Concurrence Letter signed 03/25/2022

5. The official with jurisdiction over the property concurred in writing with FHWA's and/or PennDOT's determination that the project will not adversely affect the property. (NOTE: Public input must be received and considered prior to the official with jurisdiction making a final determination.)  YES

Identify the official with jurisdiction and date of concurrence and attach written concurrence:

Ryan Dysinger, Assistant Director, Bureau of State Parks, DCNR

Written concurrence from the official with jurisdiction is included in the following Attachment: Section 4(f) De Minimis Concurrence Letter signed 03/25/2022

6. If the Section 4(f)/Section 2002 use involves State Game Land, verify that the use is considered *de minimis* in accordance with the *Cooperative Interagency Agreement for Interdepartmental Land Transfer of State Game Lands*. (Describe and obtain PA Game Commission concurrence signature below for use of a State Game Land Bank and/or Interdepartmental Land Transfer.)  YES

**State Game Land Bank**

Debiting [Click here to enter text.](#) (acres)





On Behalf of the Federal Highway Administration—Pennsylvania Division Office

**Determination of Section 4(f) De Minimis Use**  
**Section 2002 No Adverse Use**  
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**May 2014 Version**

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<b>Project Name:</b>	I-80 Lehigh River Bridges	<b>FPN:</b>	Enter FPN.	<b>MPMS:</b>	99552

From [Click here to enter text.](#) SGL bank

PGC Signature:

Date: [Click here to enter a date.](#)

**Interdepartmental Land Transfer**

PGC Signature:

Date: [Click here to enter a date.](#)

7. Have Federal or State funds [LWCF 6(f)/Project 70/Project 500/other recreation grants] been used in the acquisition of, or for any improvements to, the Section 4(f) property?  YES  NO

If Yes, the appropriate Federal agency has been coordinated with and is in agreement with the land conversion or transfer.  YES

**Provide more information regarding the Section 6(f)/Project 70/Project 500/other recreation grants coordination:**

DCNR has confirmed that LWCF funds (42-00081) were used to purchase land for the Lehigh Gorge State Park. The entire state park is afforded protection under Section 6(f). As such, any land necessary to be acquired for construction of the project would be considered a conversion from public outdoor recreation use to transportation use. Temporary non-conforming use of Section 6(f) protected land (i.e., temporary construction easements) lasting longer than 180 days is considered a permanent conversion also requiring replacement of property. The National Park Service can approve such conversion only if it is found to be in accord with the comprehensive statewide outdoor recreation plan and with the replacement of property of at least equal fair market value and of reasonably equivalent usefulness and location. Approximately 6 acres of land will be replaced. The concurrence letters from DCNR and NPS are in Attachment D.

8. The project does not involve any uses that would require an individual Section 4(f) evaluation. (It is acceptable if there are other Section 4(f) uses that are covered by one of the nationwide programmatic Section 4(f) evaluations or meet temporary occupancy criteria.)  YES



On Behalf of the Federal Highway  
Administration—Pennsylvania Division Office

**Determination of Section 4(f) De Minimis Use**  
**Section 2002 No Adverse Use**  
**Public Parks, Recreation Areas,**  
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<b>County:</b>	Luzerne and Carbon	<b>State Route:</b>	I-80	<b>Section:</b>	08B
<b>Project Name:</b>	I-80 Lehigh River Bridges	<b>FPN:</b>	Enter FPN.	<b>MPMS:</b>	99552

If there are other Section 4(f) properties used, list them here, briefly describe the use, and identify which form(s) will be completed to address the use:

Construction of the project will involve construction activities around, over and in the Lehigh River. The section of the Lehigh River within the project area is designated by PA Fish and Boat Commission as the Northern Section of the Lehigh River Water Trail. The Lehigh River is also designated as a Pennsylvania Scenic River within the project area. The Section 4(f) Evaluation for the Lehigh River Water Trail and PA Scenic River is documented in a separate *Determination of Section 4(f) De Minimis Use/ Section 2002 No Adverse Use Public Parks, Recreation Areas, Wildlife and/or Waterfowl Refuges, State Forest Land, and State Game Land Form*.

In accordance with PA Act 120 Section 2002 requirements, briefly summarize the impacts to other Section 2002 areas of concern that would occur if the use of the public park, recreation area, or wildlife or waterfowl refuge was avoided. Other Section 2002 areas of concern to be discussed could include the following:

(1) residential and neighborhood character and location, (2) conservation including air, erosion, sedimentation, wildlife and general ecology of area, (3) noise, and air and water pollution, (4) multiple use of space, (5) replacement housing, (6) displacement of families and business, (7) aesthetics, (8) public health and safety, (9) fast, safe and efficient transportation, (10) civil defenses, (11) economic activity, (12) employment, (13) fire protection, (14) public utilities, (15) religious institutions, (16) conduct and financing of government including the effect on the local tax base and social service costs, (17) property values, (18) education, including the disruption of school district operations, (19) engineering, right-of-way and construction costs of the project and related facilities, (20) maintenance and operating costs of the project and related facilities, and (21) operation and use of existing transportation routes and programs during construction and after completion.

The project will replace the I-80 Section 08B, Lehigh River Bridges, which span over the Lehigh River and Lehigh Gorge State Park and Trail. To meet the project's purpose and need, there is no alternative that would avoid the use of the Lehigh Gorge State Park. The No-Build Alternative would impact public health and safety as the existing I-80 dual bridges are more than 50 years old and are approaching the end of their service life. Vehicular traffic and interstate commerce would be impacted as further deterioration of the I-80 Lehigh River Bridges could cause the bridges to be load posted.

**Ecological Services Evaluation**

As part of DCNR's review under the Environmental Rights Amendment, the following mitigation addresses ecological and other impacts associated with the Lehigh Gorge State Park (LGSP) and Trail:



On Behalf of the Federal Highway  
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**Determination of Section 4(f) De Minimis Use**  
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- The I-80 Lehigh River Bridges are nearing the end of their serviceable life. This means the bridges will need ever increasing inspections and repairs. These repairs have the potential to interrupt access to the park and to the boat launch area under the bridges. If the condition of the bridges continues to deteriorate, they may need to be weight posted, or at some point be closed. Much of the tourist traffic headed to LGSP uses I-80 to get to the park, making replacement of the bridges important not just for interstate commerce, but also for tourism and park access.
- PennDOT is responsible for providing safe and reliable facilities for the traveling public. The replacement of the I-80 Lehigh River Bridges is needed to maintain a safe and reliable crossing of the Lehigh River not only for those traveling on the interstate, but also for those biking, hiking, rafting, kayaking and fishing in the vicinity of the bridge within LGSP, the Lehigh Gorge Trail, and the Lehigh River Water Trail.
- Replacement land is being provided for the Section 6(f) conversion (Required Right-of-Way, Proposed Slope Easement, Proposed Aerial Easements, and Temporary Construction Easement) within LGSP totaling approximately 6 acres. This replacement land also serves as mitigation of the Section 4(f) use of LGSP. The replacement land being provided is forested land; therefore, the replacement land offsets the removal of trees in the construction staging area, and in the area where the existing gabion wall along SR 940 will be replaced.
- The contractor will restore disturbed areas, and full access to the park amenities will be provided upon project completion. As part of restoring the disturbed areas, a planting plan will be prepared in coordination with DCNR during final design.
- The area within proposed aerial easements adjacent to the bridges will be available for use by LGSP after construction.
- The majority of the state park will remain open during bridge construction, and access to the Lehigh River via the three new boat launches currently being constructed by DCNR south of the bridges will be maintained (the existing boat launch under the bridge will be temporarily closed). Efforts will be made to minimize effects to the park and trail throughout construction.
- If necessary due to pier locations, reconfiguration of the driveway, parking, or boat launches will be coordinated with DCNR.



On Behalf of the Federal Highway Administration—Pennsylvania Division Office

**Determination of Section 4(f) De Minimis Use**  
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- Access to and use of the Lehigh Gorge Trail will be maintained to the extent possible during construction:
  - April through October: Overnight closure of the trail (during hours that the state park is closed) will be allowed. Short-term closure of the trail (up to 15 minutes) will be allowed during daylight hours Monday through Friday using flaggers and advance signage.
  - November through March: Long-term closure of the trail will be allowed for certain construction activities including bridge superstructure demolition, pier demolition, pier construction, and setting of beams and deck forms, with consideration of working room and safety.
  - Advance notice of trail closures will be provided to DCNR, D&L National Heritage Corridor, the Borough of White Haven, and the public.
  - The contractor may temporarily realign the trail through the project site if approved by DCNR. A 10’ wide trail will be maintained except as needed for closures noted above.
- An Aids to Navigation Plan will be implemented and will include advanced signing. Signs will be placed at boat launches upstream of the project. Additional advanced notification to PFBC, DCNR and the public will be provided.
- PennDOT will coordinate with DCNR and White Haven Borough to provide wayfinding signs



On Behalf of the Federal Highway  
Administration—Pennsylvania Division Office

**Determination of Section 4(f) *De Minimis* Use**  
**Section 2002 No Adverse Use**  
**Public Parks, Recreation Areas,**  
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**Include any additional information related to the park impact that is relevant to the determination of *de minimis*/no adverse use:**

[Click here to enter text.](#)

**SUMMARY AND DETERMINATION:**

The project involves a *de minimis*/no adverse use on the Section 4(f)/Section 2002 property as evidenced through the minimization of harm to a public park, recreation land, or wildlife and waterfowl refuge as a result of mitigation to or avoidance of impacts to the qualifying characteristics and/or the functions of the resource. Based on the scope of the undertaking; the fact that the undertaking does not adversely affect the function/qualities of the Section 4(f)/Section 2002 property on a permanent or temporary basis; and with agreement from the official with jurisdiction, the proposed action constitutes a *de minimis*/no adverse use; and therefore, no analysis of avoidance alternatives is required.

Name and Organization of Preparer: Kathleen Krommes, HDR

Date: 3/28/2022

Project Manager: *Brandy L Rotz*

Date: 03/28/2022

Environmental Manager: *Wentzel*

Date: 3/29/22

PennDOT, BOPD: Nina Ertel

Digitally signed by Nina Ertel  
Date: 2022.03.29 07:45:02 -04'00'

Date:

FHWA: CAMILLE A OTTO

Digitally signed by CAMILLE A OTTO  
Date: 2022.03.29 10:10:05 -04'00'

Date:

**LIST OF ATTACHMENTS**

Attachment A: Figures

Figure 1 – Project Location Map, Original Project Study Area

Figure 2 – Project Location Map, Expanded Project Study Area

Figure 3 – SR 940 Gabion Wall Location Map

Figure 4 – Section 4(f) / 6(f) Location Map

Figure 5 – Proposed Soldier Pile & Lagging Wall (SR 940 Gabion Wall Replacement Conceptual Plan)

Figure 6 – Temporary Closures to Lehigh Gorge State Park

Attachment B: Photographs

Attachment C: Public Outreach

Public Notice Flyer, Temporary Closure to Lehigh Gorge State Park

Newspaper Advertisement Proof of Publication

Comments Received

Attachment D: Concurrence Letters



On Behalf of the Federal Highway  
Administration—Pennsylvania Division Office

**Determination of Section 4(f) De Minimis Use**  
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Section 4(f) De Minimis Concurrence Letter, signed 3/25/2022  
DCNR Section 6(f) Concurrence Letter, dated 02/22/2022  
NPS Section 6(f) Concurrence Email, dated 11/22/2021

**List Section 4(f) mitigation measures associated with this use that are part of this project:**

The project will not adversely affect the Lehigh Gorge State Park. The majority of the state park will remain open during bridge construction, and access to the Lehigh River via the three new boat launches currently being constructed by DCNR south of the bridges will be maintained (the existing boat launch under the bridges will be temporarily closed). Access to the Lehigh Gorge Trail will be maintained; however, a portion of the trail may be closed under the bridge during low and no usage timeframes during construction. The contractor will restore disturbed areas, and full access to the park amenities will be provided upon completion. The area within Proposed Aerial Easements adjacent to the bridges will be available for use by LGSP after construction.

Mitigation measures will include:

- Access to and use of the Lehigh Gorge Trail will be maintained to the extent possible during construction:
  - April through October: Overnight closure of the trail (during hours that the state park is closed) will be allowed. Short-term closure of the trail (up to 15 minutes) will be allowed during daylight hours Monday through Friday using flaggers and advance signage.
  - November through March: Long-term closure of the trail will be allowed for certain construction activities including bridge superstructure demolition, pier demolition, pier construction, and setting of beams and deck forms, with consideration of working room and safety.
  - Advance notice of trail closures will be provided to DCNR, D&L National Heritage Corridor, the Borough of White Haven, and the public.
  - The contractor may temporarily realign the trail through the project site if approved by DCNR. A 10' wide trail will be maintained except as needed for closures noted above.
- If necessary due to pier locations, reconfiguration of the driveway, parking, or boat launches will be coordinated with DCNR.



On Behalf of the Federal Highway Administration—Pennsylvania Division Office

**Determination of Section 4(f) De Minimis Use  
Section 2002 No Adverse Use  
Public Parks, Recreation Areas,  
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<b>Project Name:</b>	I-80 Lehigh River Bridges	<b>FPN:</b>	Enter FPN.	<b>MPMS:</b>	99552

- An Aids to Navigation Plan will be implemented and will include advance signing. Signs will be placed at boat launches upstream of the project. Additional advanced notification to PFBC, DCNR and the public will be provided.
- The contractor will restore disturbed areas, and full access to the park amenities will be provided upon completion. As part of restoring the disturbed areas, a planting plan will be prepared in coordination with DCNR during Final Design.
- PennDOT will coordinate with DCNR and White Haven Borough to provide wayfinding signs.

PennDOT will provide replacement land for Section 6(f) conversion (Required Right-of-Way, Proposed Slope Easement, Proposed Aerial Easements, and TCE) within LGSP totaling approximately 6 acres. This replacement land will also serve as mitigation of the Section 4(f) use of LGSP. The replacement land being provided is forested land; therefore, the replacement land offsets the removal of trees in the construction staging area, and in the area where the existing gabion wall along SR 940 will be replaced. Concurrence letters from DCNR and NPS are in Attachment D.


**Typical attachments for this form include, but are not limited to:**

- Project location map
- Map of affected Section 4(f) property and other Section 4(f) property(ies) in the project vicinity
- Photographs of the Section 4(f) property
- Project plan sheet to show impacts
- Correspondence with the official with jurisdiction
- Public involvement information

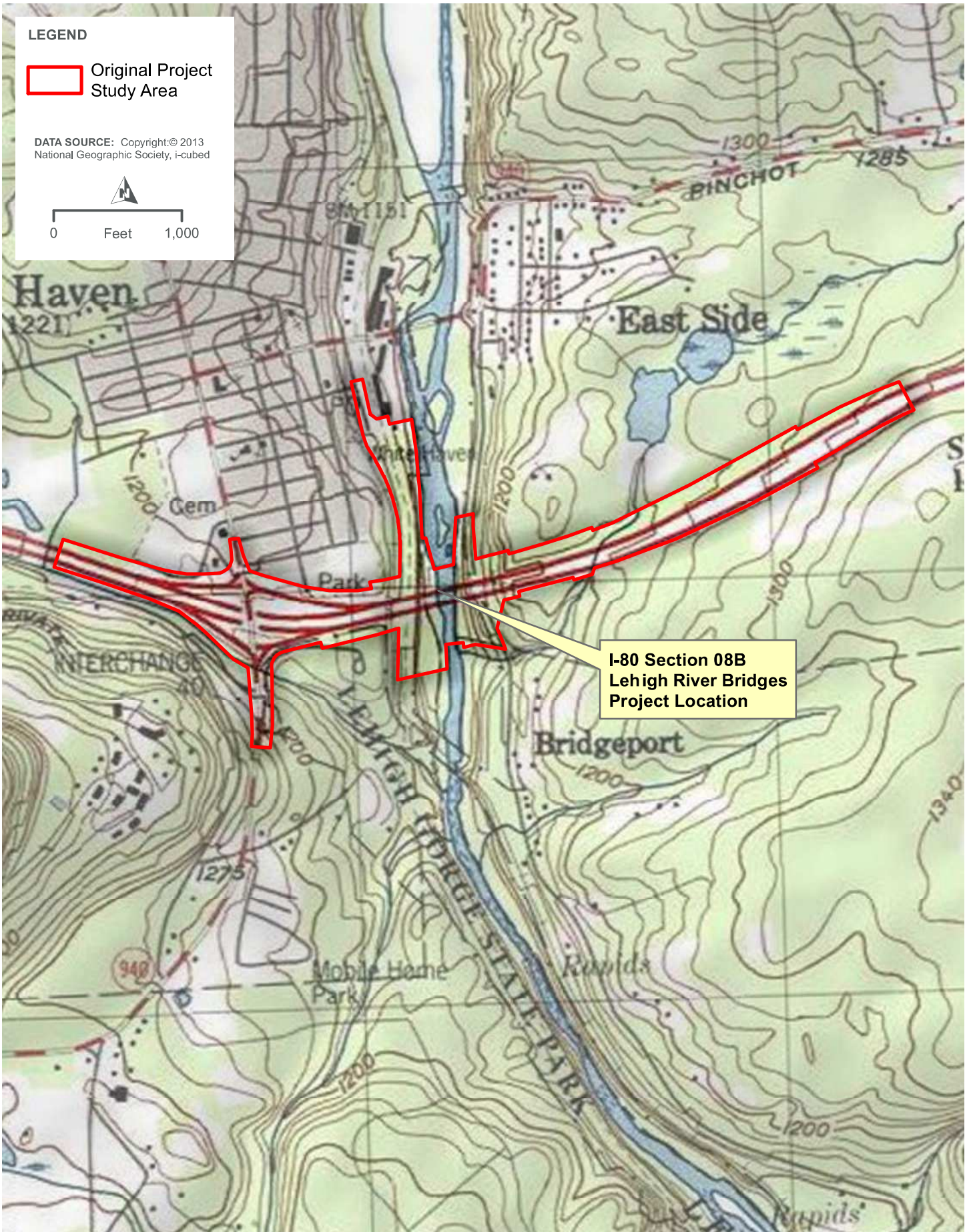
**Attachment A**  
**Figures**



LEGEND

 Original Project Study Area

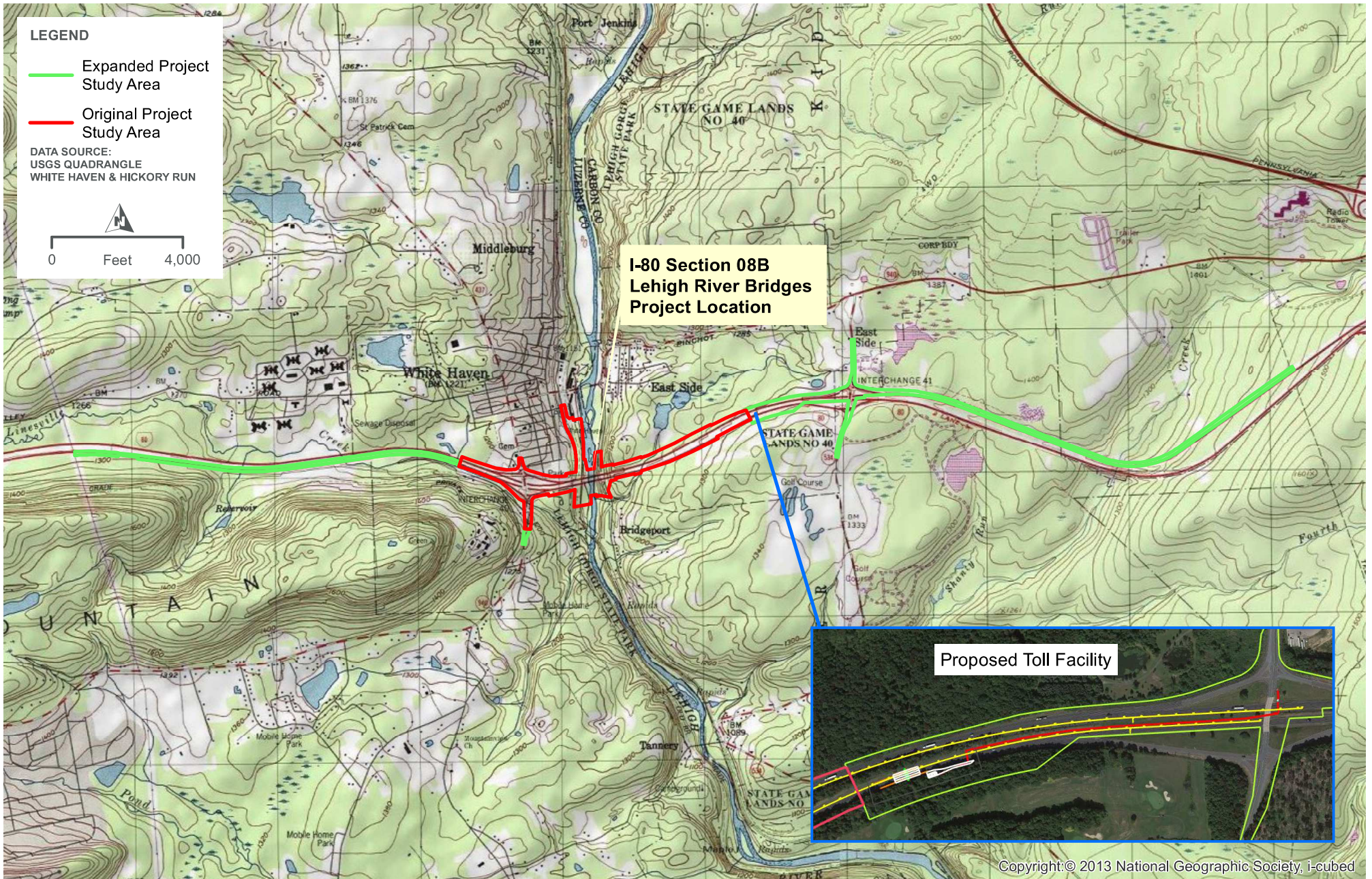
DATA SOURCE: Copyright:© 2013 National Geographic Society, i-cubed



I-80 SECTION 08B, LEHIGH RIVER BRIDGES  
PROJECT LOCATION MAP  
ORIGINAL PROJECT STUDY AREA



**FIGURE 1**



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**I-80 SECTION 08B, LEHIGH RIVER BRIDGES  
PROJECT LOCATION MAP  
EXPANDED PROJECT STUDY AREA**

**FIGURE 2**



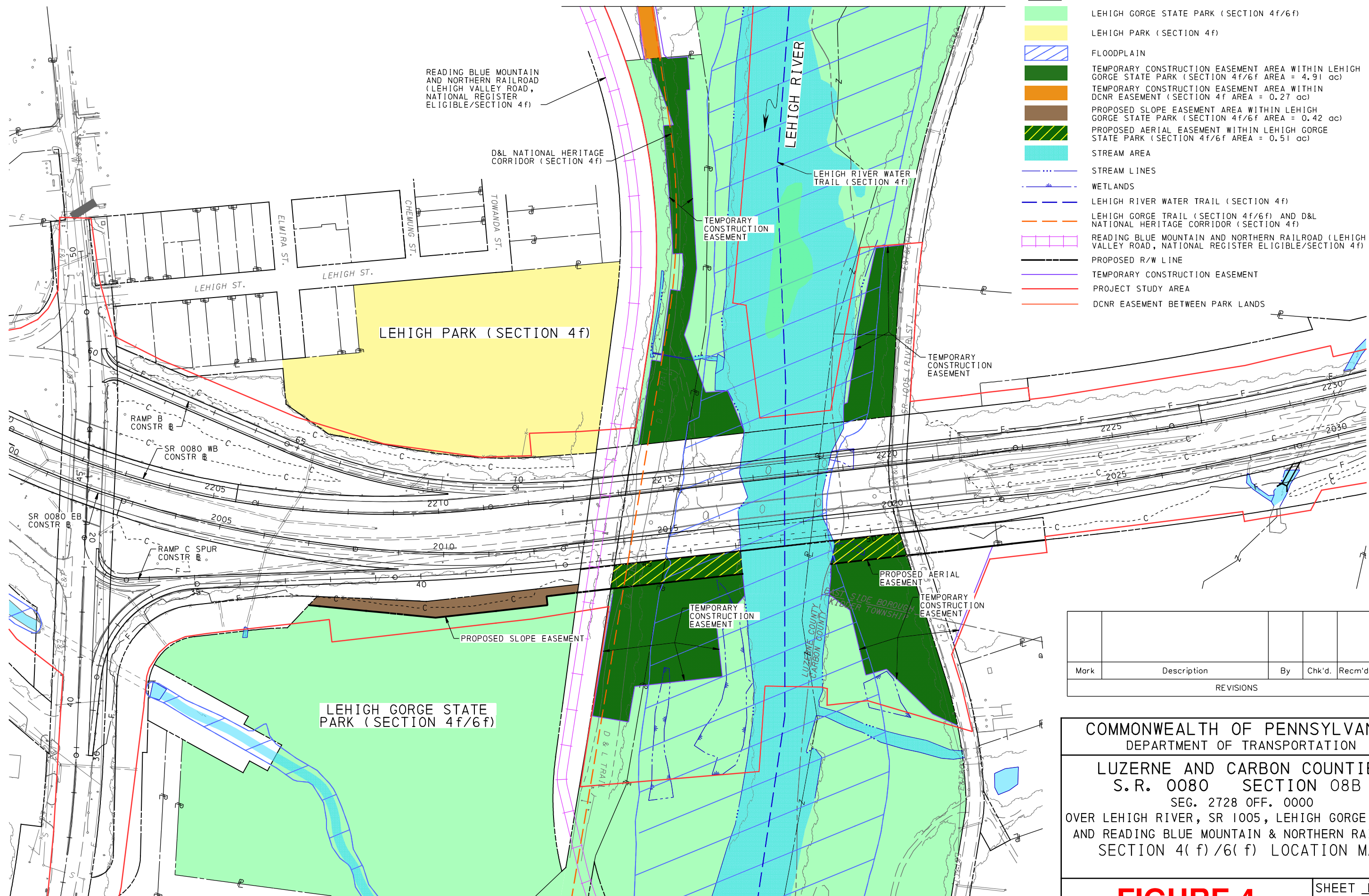
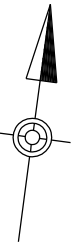


I-80 SECTION 08B, LEHIGH RIVER BRIDGES

SR 940 GABION WALL LOCATION MAP

**FIGURE 3**

- LEHIGH GORGE STATE PARK (SECTION 4f/6f)
- LEHIGH PARK (SECTION 4f)
- FLOODPLAIN
- TEMPORARY CONSTRUCTION EASEMENT AREA WITHIN LEHIGH GORGE STATE PARK (SECTION 4f/6f AREA = 4.91 ac)
- TEMPORARY CONSTRUCTION EASEMENT AREA WITHIN DCNR EASEMENT (SECTION 4f AREA = 0.27 ac)
- PROPOSED SLOPE EASEMENT AREA WITHIN LEHIGH GORGE STATE PARK (SECTION 4f/6f AREA = 0.42 ac)
- PROPOSED AERIAL EASEMENT WITHIN LEHIGH GORGE STATE PARK (SECTION 4f/6f AREA = 0.51 ac)
- STREAM AREA
- STREAM LINES
- WETLANDS
- LEHIGH RIVER WATER TRAIL (SECTION 4f)
- LEHIGH GORGE TRAIL (SECTION 4f/6f) AND D&L NATIONAL HERITAGE CORRIDOR (SECTION 4f)
- READING BLUE MOUNTAIN AND NORTHERN RAILROAD (LEHIGH VALLEY ROAD, NATIONAL REGISTER ELIGIBLE/SECTION 4f)
- PROPOSED R/W LINE
- TEMPORARY CONSTRUCTION EASEMENT
- PROJECT STUDY AREA
- DCNR EASEMENT BETWEEN PARK LANDS

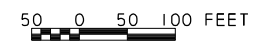


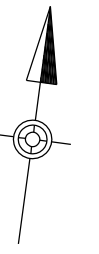
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 FILE: 180-Section-4f-Map

Mark	Description	By	Chk'd.	Recm'd.	Date
REVISIONS					

**COMMONWEALTH OF PENNSYLVANIA**  
 DEPARTMENT OF TRANSPORTATION  
**LUZERNE AND CARBON COUNTIES**  
**S.R. 0080 SECTION 08B**  
 SEG. 2728 OFF. 0000  
 OVER LEHIGH RIVER, SR 1005, LEHIGH GORGE TRAIL,  
 AND READING BLUE MOUNTAIN & NORTHERN RAILROAD  
 SECTION 4(f)/6(f) LOCATION MAP

**FIGURE 4**





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 PATH: c:\pennwork\img\leas\01\12084107\ FILE: 180-Section-4f-Map MODEL: Sheet 2

- LEGEND:**
- LEHIGH GORGE STATE PARK (SECTION 4f/6f)
  - LEHIGH PARK (SECTION 4f)
  - FLOODPLAIN
  - TEMPORARY CONSTRUCTION EASEMENT AREA WITHIN LEHIGH GORGE STATE PARK (SECTION 4f/6f AREA = 4.91 ac)
  - TEMPORARY CONSTRUCTION EASEMENT AREA WITHIN DCNR EASEMENT (SECTION 4f AREA = 0.27 ac)
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  - STREAM AREA
  - STREAM LINES
  - WETLANDS
  - LEHIGH RIVER WATER TRAIL (SECTION 4f)
  - LEHIGH GORGE TRAIL (SECTION 4f/6f) AND D&L NATIONAL HERITAGE CORRIDOR (SECTION 4f)
  - READING BLUE MOUNTAIN AND NORTHERN RAILROAD (LEHIGH VALLEY ROAD, NATIONAL REGISTER ELIGIBLE/SECTION 4f)
  - PROPOSED R/W LINE
  - TEMPORARY CONSTRUCTION EASEMENT
  - PROJECT STUDY AREA
  - DCNR EASEMENT BETWEEN PARK LANDS

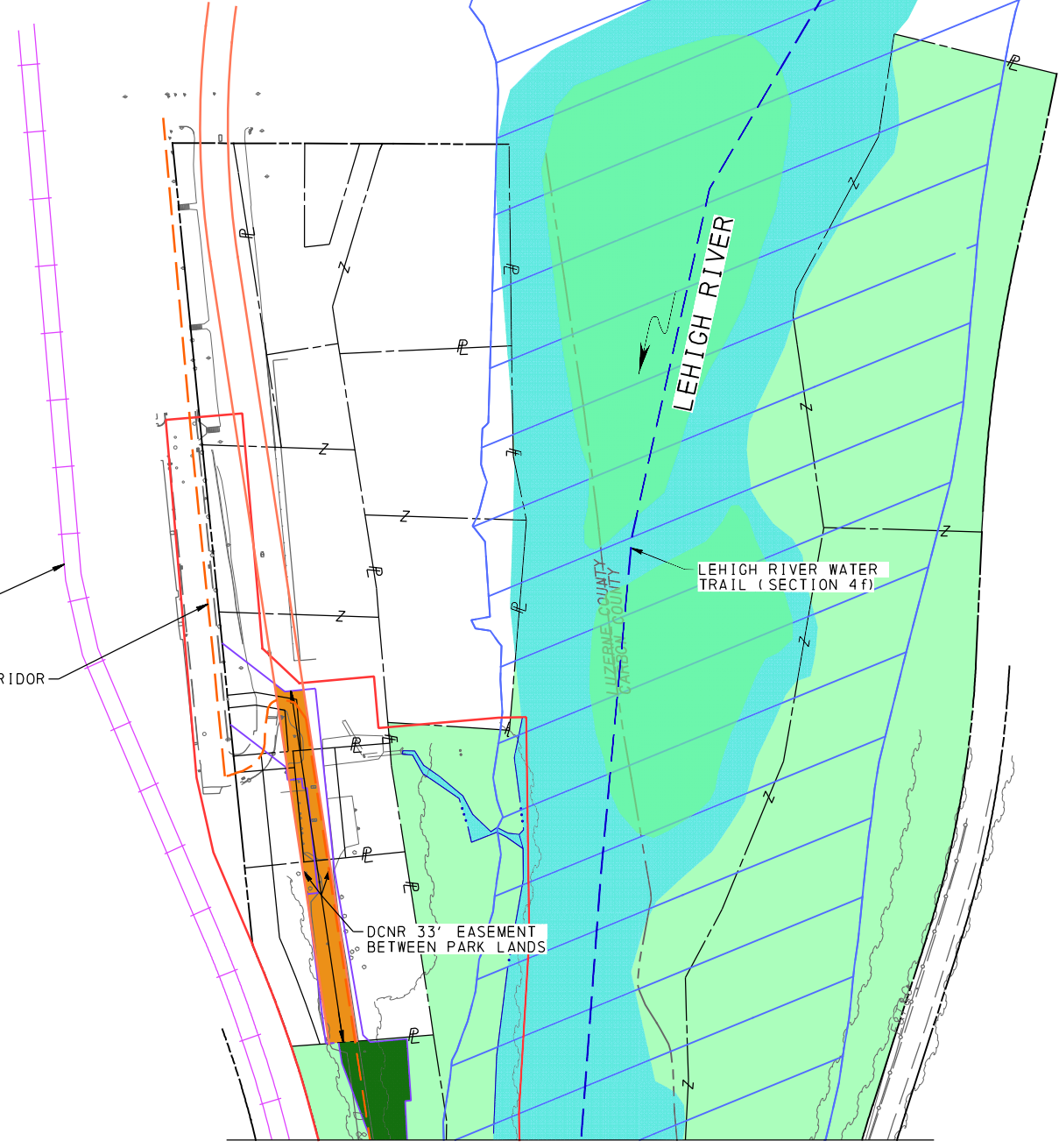
READING BLUE MOUNTAIN AND NORTHERN RAILROAD (LEHIGH VALLEY ROAD, NATIONAL REGISTER ELIGIBLE/SECTION 4f)

D&L NATIONAL HERITAGE CORRIDOR

LEHIGH RIVER

LEHIGH RIVER WATER TRAIL (SECTION 4f)

DCNR 33' EASEMENT BETWEEN PARK LANDS



SEE SHEET 1 OF 2

50 0 50 100 FEET

Mark	Description	By	Chk'd.	Recm'd.	Date
REVISIONS					

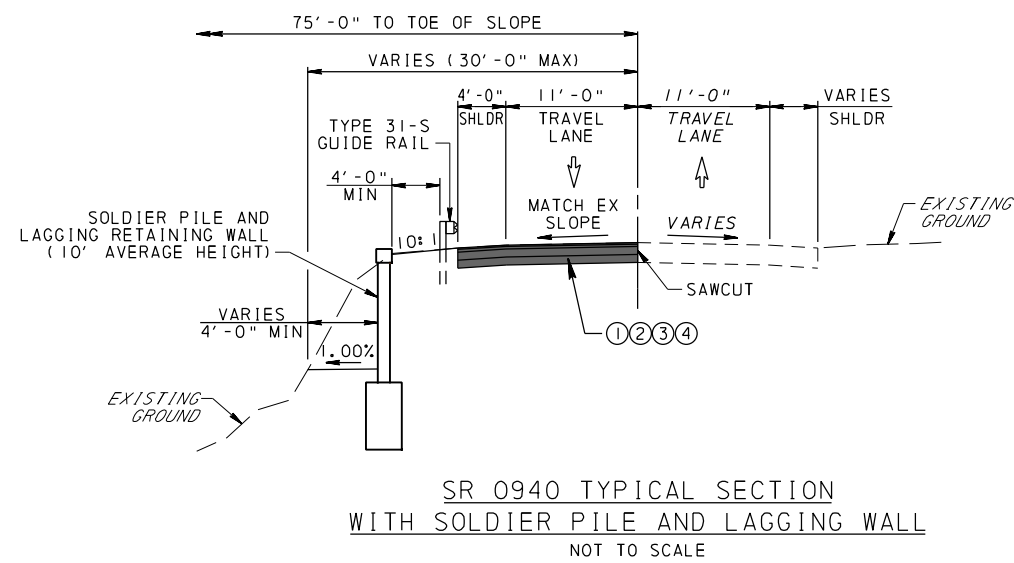
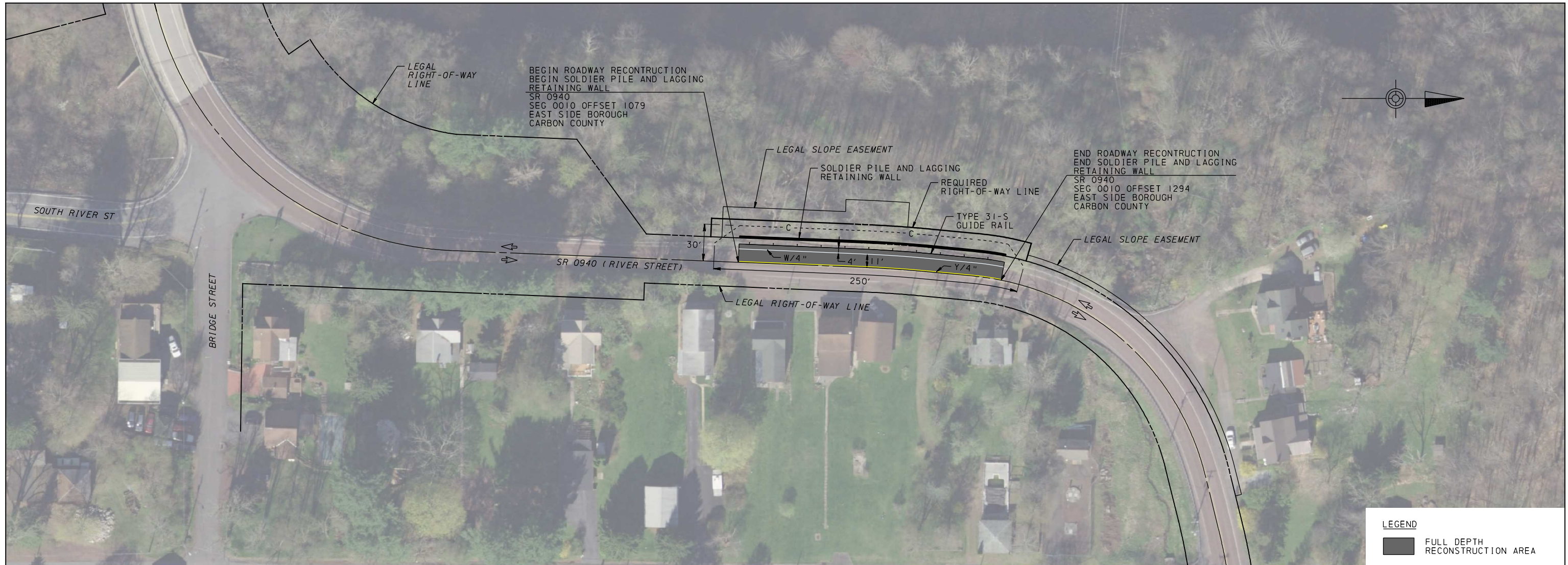
COMMONWEALTH OF PENNSYLVANIA  
DEPARTMENT OF TRANSPORTATION

LUZERNE AND CARBON COUNTIES  
S.R. 0080 SECTION 08B  
SEG. 2728 OFF. 0000  
OVER LEHIGH RIVER, SR 1005, LEHIGH GORGE TRAIL,  
AND READING BLUE MOUNTAIN & NORTHERN RAILROAD  
SECTION 4(f)/6(f) LOCATION MAP

**FIGURE 4**

SHEET 2 OF 2

S-XXXXX



- ① SUPERPAVE ASPHALT MIXTURE DESIGN, WEARING COURSE, PG 64S-22, 0.3 TO < 3 MILLION ESALS, 9.5 MM MIX, 1 1/2" DEPTH, SRL-H
- ② SUPERPAVE ASPHALT MIXTURE DESIGN, BINDER COURSE, PG 64S-22, 0.3 TO < 3 MILLION ESALS, 19.0 MM MIX, 2 1/2" DEPTH
- ③ SUPERPAVE ASPHALT MIXTURE DESIGN, BASE COURSE, PG 64S-22, 0.3 TO < 3 MILLION ESALS, 25.0 MM MIX, 8" DEPTH
- ④ SUBBASE 8" DEPTH (NO. 2A)

NOTE: APPLY ASPHALT TACK COAT BETWEEN ALL ASPHALT PAVEMENT LAYERS.

**GENERAL NOTES:**

CONSTRUCT PROJECT IN ACCORDANCE WITH PUBLICATION 408 DATED 2020 AND CONTRACT SPECIAL PROVISIONS.

ALL WORK IS TO BE CONFINED WITHIN THE EXISTING HIGHWAY RIGHT-OF-WAY AND/OR PROPERTY OF THE COMMONWEALTH OF PENNSYLVANIA

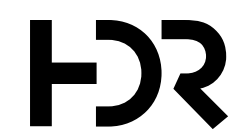
FURNISH, ERECT, PLACE AND MAINTAIN TRAFFIC CONTROL SIGNS AND DEVICES, AND MAINTAIN TRAFFIC DURING HOURS OF CONSTRUCTION AND AT ALL OTHER TIMES IN ACCORDANCE WITH THE METHODS INDICATED ON THE DRAWINGS AND THE FOLLOWING:

- SPECIAL PROVISIONS OF THE CONTRACT
- PUBLICATION 213, TEMPORARY TRAFFIC CONTROL GUIDELINES
- PA CODE, TITLE 67, CHAPTER 212, OFFICIAL TRAFFIC CONTROL DEVICES
- PENNDOT PUBLICATION 35, APPROVED CONSTRUCTION MATERIALS (BULLETIN 15)
- PENNDOT PUBLICATION 46, TRAFFIC ENGINEERING MANUAL
- PENNDOT PUBLICATION 408, LATEST EDITION
- PENNDOT PUBLICATION 111M, TRAFFIC CONTROL PAVEMENT MARKING AND SIGNING STANDARDS
- PENNDOT PUBLICATION 236, HANDBOOK OF APPROVED SIGNS
- MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD), 2009

MAINTAIN ACCESS TO ALL SIDE ROADS, PRIVATE DRIVEWAYS, AND BUSINESSES AT ALL TIMES.

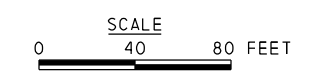
ACCURATELY MARK ALL EXISTING PAVEMENT MARKINGS FOR FUTURE REFERENCE AND REPLACEMENT BEFORE BEGINNING ANY ON SITE OPERATION.

INSTALL ALL MARKING AND DELINEATORS IN ACCORDANCE WITH THE CURRENT PUBLICATION STANDARDS (PUB. 111M, TC-8600 SERIES)

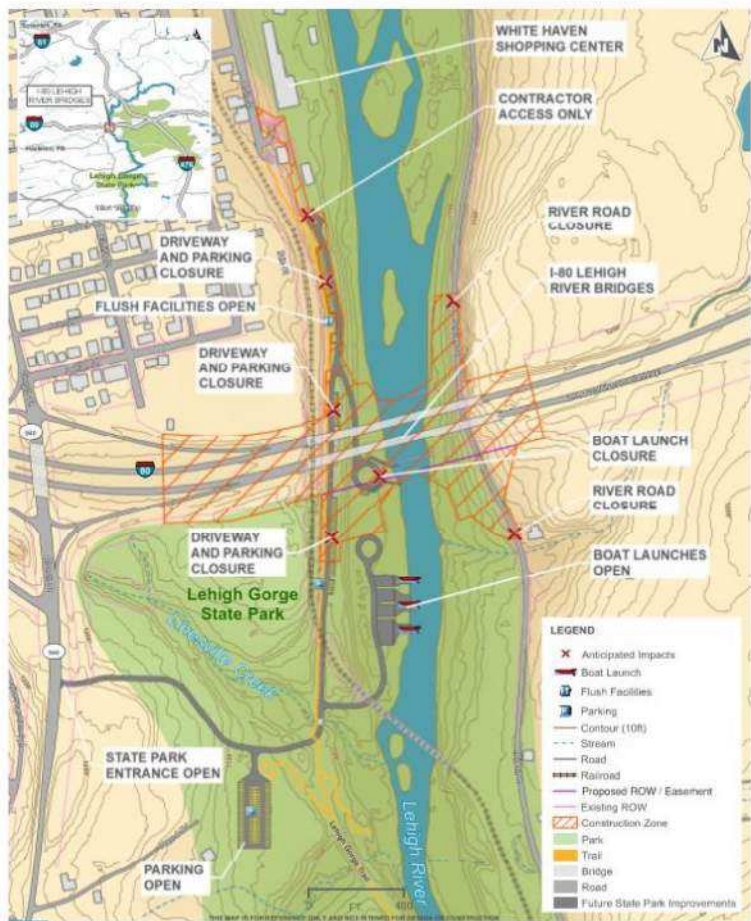


**PROPOSED SOLDIER PILE & LAGGING WALL  
SR 0940 (RIVER STREET)  
EAST SIDE BOROUGH, CARBON COUNTY**

CONCEPTUAL PLAN  
WHITE HAVEN BRIDGE PACKAGE



DATE  
FEBRUARY 2022  
**FIGURE 5**



**I-80 LEHIGH RIVER BRIDGE PROJECT**  
**TEMPORARY CLOSURES TO LEHIGH GORGE STATE PARK**  
 WHITE HAVEN & EAST SIDE BOROUGHS AND KIDDER TOWNSHIP  
 LUZERNE & CARBON COUNTIES, PA

**FIGURE 6**

**Attachment B**  
**Photographs**



Photographs



Lehigh Gorge State Park White Haven Access, facing south



Lehigh Gorge Trail and driveway, facing south



Lehigh Gorge Trail and driveway to existing boat launch, facing south



Boat launch area under I-80 Lehigh River Bridge on west shore of the Lehigh River, facing east



Underneath the I-80 Lehigh River Bridges on west shore of the Lehigh River, facing east



River gauge and raft requirements posted on the eastbound I-80 Lehigh River bridge pier



West shore of Lehigh River in Lehigh Gorge State Park, facing upstream (north)



West shore of Lehigh River in Lehigh Gorge State Park, facing downstream (south)



View of I-80 Lehigh River Bridges and Lehigh River, facing southeast



Lehigh Gorge Trail and driveway, facing north



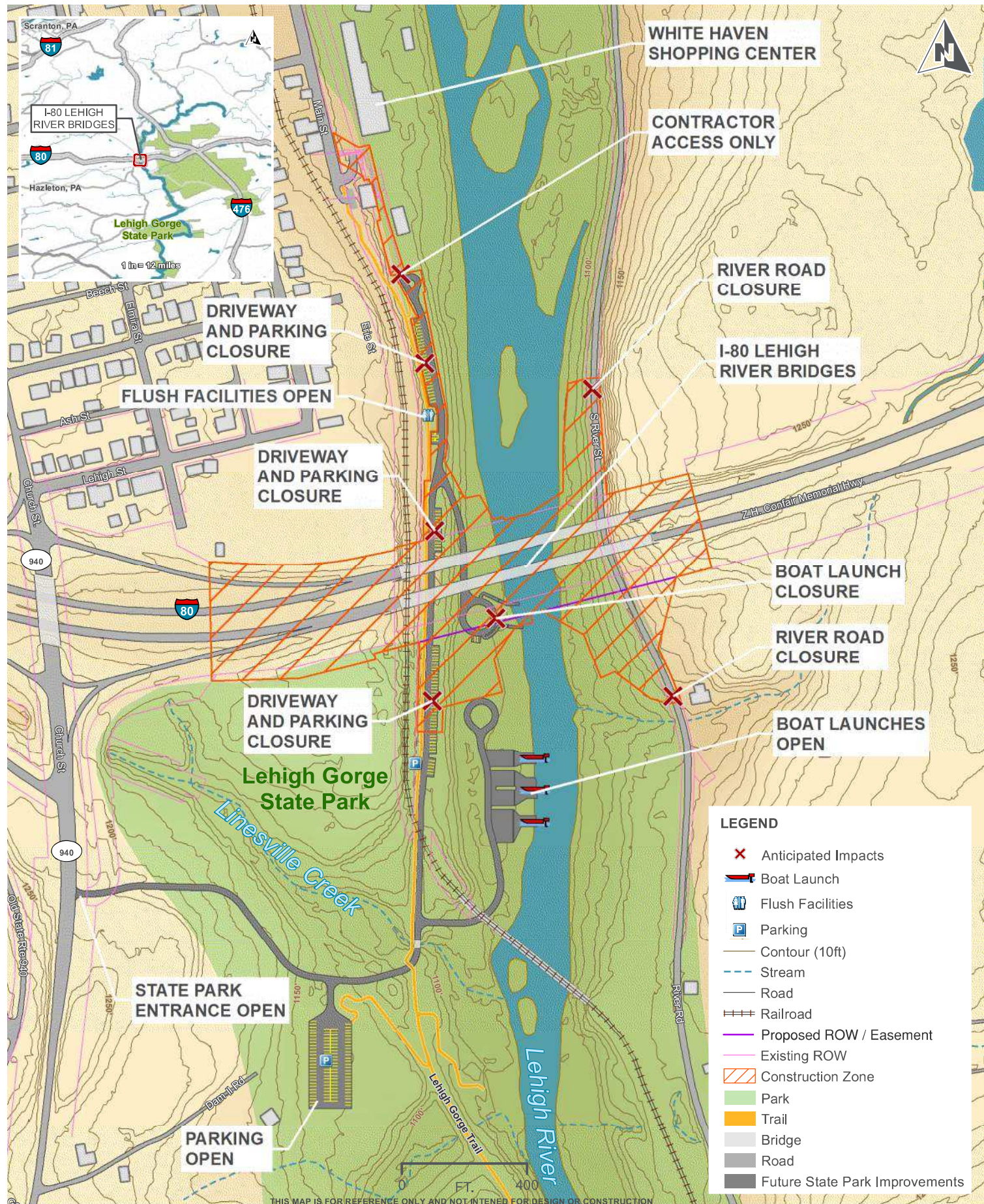
View of SR 940 Existing Gabion Wall, facing north



View of SR 940 Existing Gabion Wall, facing south

**Attachment C**  
**Public Outreach**





**NOTICE OF PUBLIC INVOLVEMENT  
I-80, Section 08B, Lehigh River Bridges Project**

The Pennsylvania Department of Transportation (PennDOT) is proposing to replace the I-80 Lehigh River Bridges. The dual bridges carry I-80 over the Lehigh River, Lehigh Gorge State Park and Trail, Reading Blue Mountain & Northern Railroad, and River Road in White Haven Borough, Luzerne County, and East Side Borough and Kidder Township, Carbon County. Required Right-of-Way and Easements are necessary for the project, including approximately 7 acres from Lehigh Gorge State Park.

Construction of the I-80 bridges is expected to take approximately four years beginning in 2023. DCNR has broken ground on a capital improvements project in Lehigh Gorge State Park that is planned to be completed prior to PennDOT's bridge replacement project. Anticipated temporary impacts to Lehigh Gorge State Park property and the Lehigh River Water Trail are summarized below.

Lehigh Gorge Trail:

- April through October: Overnight trail closures are anticipated. Short-term trail closures are anticipated during daylight hours Monday through Friday using flaggers and advance signage.
- November through March: Long-term trail closures in the vicinity of the bridges are anticipated.

Lehigh Gorge State Park driveway and parking:

- The driveway and parking will be closed from the White Haven Shopping Center to the future park facilities south of the bridges.
- The future park entrance south of the bridges will remain open during construction, including the overflow parking area and a portion of the parking along the driveway.

Boat launches:

- The boat launch directly under the bridges will be closed during construction.
- The three future boat launches downstream of the bridges will remain open during construction.

Lehigh River Water Trail:

- Temporary causeways will extend from both riverbanks but will not completely block the river.
- An Aids to Navigation Plan will be implemented to ensure safe passage for boaters through the construction area and will include advance signing.

PennDOT will also provide replacement land as part of mitigation for Required Right-of-Way and Temporary Construction Easements within Lehigh Gorge State Park.

The majority of Lehigh Gorge State Park will remain open during bridge construction, and access to the boat launches and Lehigh River will be maintained. The contractor will restore disturbed areas, and full access to park amenities will be provided upon completion. PennDOT's opinion is the project will not adversely impact the recreational functions of the resources.

PennDOT is seeking public input. Those wishing to comment on the impacts of the project on Lehigh Gorge State Park and Lehigh River Water Trail, can send written comments to the address or email below. All comments should be received by October 26, 2021. If you require additional information concerning this project, contact Brandy Rotz, AICP, Consultant Project Manager (717) 540-6040, c-brotz@pa.gov.

Mr. Michael W. Rebert, P.E., District Executive  
PennDOT Engineering District 5-0  
1002 Hamilton Street  
Allentown, PA 18101  
Attn: Brandy Rotz, AICP – Consultant Project Manager  
c-brotz@pa.gov



**I-80 LEHIGH RIVER BRIDGE PROJECT  
TEMPORARY CLOSURES TO LEHIGH GORGE STATE PARK  
WHITE HAVEN & EAST SIDE BOROUGHS AND KIDDER TOWNSHIP  
LUZERNE & CARBON COUNTIES, PA**

**LEGAL NOTICE**

IN RE: ESTATE OF LUISE H. MCFADDEN, a/k/a LUISE H. SCHILLER, Deceased, late of the Borough of Weatherly, County of Carbon and Commonwealth of Pennsylvania.

Notice is hereby given that Letters Testamentary have been granted in the Estate of Luise H. McFadden, a/k/a Luise H. Schiller, who died on the 16th day of January, 2021. All persons having claims against the estate are

requested to make known the same, and all persons indebted to the decedent to make payment without delay to the Executor, Anthony G. McFadden, 21 East Main Street, Weatherly, PA 18255, or his attorney:

**CYNTHIA S. YURCHAK,  
ESQUIRE  
121 Carbon Street  
Post Office Box 49  
Weatherly, PA 18255**

10/14

The Borough of White Haven will receive sealed bids for Lawn Mowing and Landscape Maintenance of right of ways and Borough owned properties. The bids will be received at the White Haven Municipal Building 312 Main Street, White Haven PA 18661 until 2:00 p.m. local time on Friday, October 22, 2021. The bids will be publicly opened and read aloud at the borough building. Bids will be considered by Borough Council at their meeting on October 25th, 7pm.

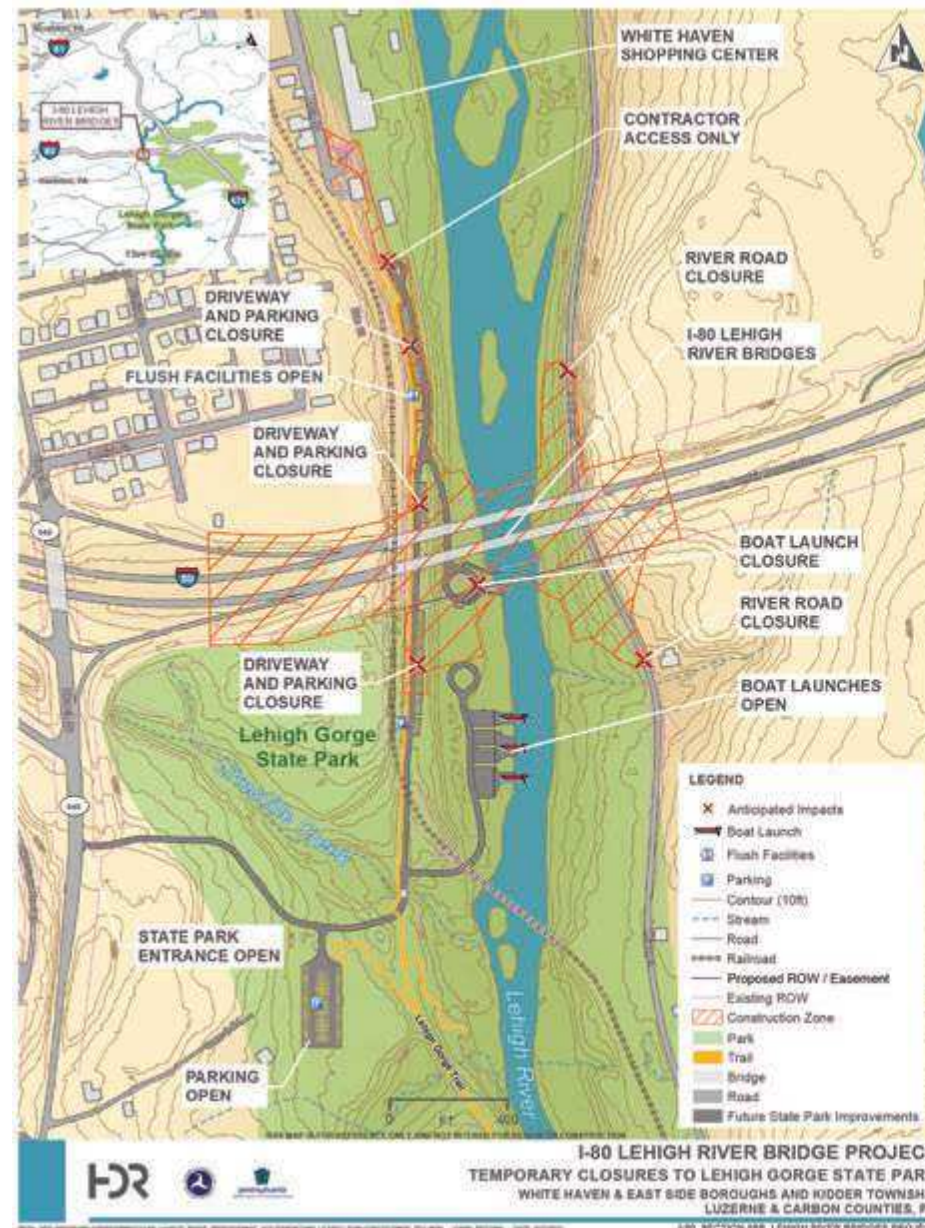
**ADVERTISEMENT FOR BIDS**

SEALED BID  
Contractor Name  
Contractor Address  
Re: Lawn Mowing and Landscape Maintenance Borough of White Haven  
Attn: Linda Szoke, Borough Manager  
312 Main Street  
White Haven PA 18661  
570-443-9129  
The bid packet can be emailed by emailing the Borough at [whborough@whboro.org](mailto:whborough@whboro.org) or can

be picked up at borough building beginning October 5, 2021 Bidders are welcome by appointment to meet with Borough Manager and look over locations listed for lawn maintenance

**Linda Szoke  
Borough Manager  
Zoning and Code Officer  
312 Main Street  
White Haven PA 18661  
570-443-9129**

10/21



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Construction of the I-80 bridges is expected to take approximately four years beginning in 2023. DCNR has broken ground on a capital improvements project in Lehigh Gorge State Park that is planned to be completed prior to PennDOT's bridge replacement project. Anticipated temporary impacts to Lehigh Gorge State Park property and the Lehigh River Water Trail are summarized below.

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# THE JOURNAL-HERALD

THURSDAY, OCTOBER 14, 2021 • Volume 41 – No. 7

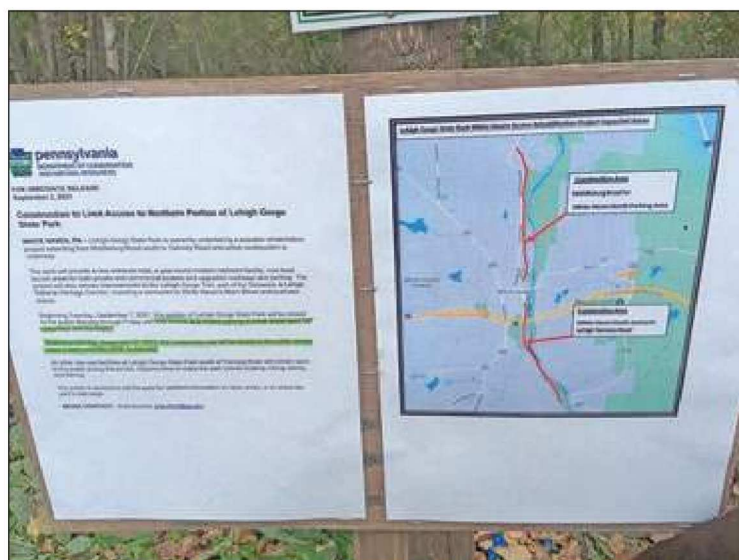
SINGLE COPY—75¢

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(USPS 277440)

CONTINUING: *THE WHITE HAVEN JOURNAL*  
ESTABLISHED 1879–142<sup>nd</sup> YEAR, NO. 46

CONTINUING: *THE WEATHERLY HERALD*  
ESTABLISHED 1880–142<sup>nd</sup> YEAR, NO. 19



**CLOSED:** Construction to the White Haven portion of Lehigh Gorge State Park will close the trail until spring. Above, a fence prevents access to the trail at Tannery; at left a posting gives details of the project. The fence was down over part of the weekend, but back up by Tuesday. Other signs around the parking area give details of the proposed Interstate 80 bridge project, also on page 3 of this issue. Comments on that plan are due by October 26. *JH: Ruth Isenberg*

## Both of Kidder's Fire Companies need new equipment

by Seth Isenberg

Some fire engines in the bays of the Albrightsville (AVFD) and Lake Harmony Volunteer Fire Companies (LHVFD) have gotten old. Through 2021, both fire companies have been working on plans to get new equipment.

Kidder Township supervisors called a public meeting on September 28 to talk to the companies about what they need, and how much the new equipment will cost.

Members of each fire company understood that in order to get something new, they had to retire two of their existing trucks. The LHVFD wants to replace its two oldest vehicles with a rescue pumper, with an estimated price tag of \$836,000 if ordered this year.

The AVFD is looking to replace both fire apparatus in the Albrightsville #2 Station with one new pumper tanker, AND buy a rescue pumper with an aerial flow tower for the Albrightsville main station, at a combined cost of just over \$1.5 million.

Placing the orders on these trucks before the end of the year will save about 3%

each — over \$70,000 total for the three.

Kidder Township has about \$1.2 million in its Emergency Services Capital Equipment Fund. It has been getting \$200,000 added from the township budget each year. There was discussion as to how much the township is allowed to spend from the fund. As the ordinance governing the fund is now written, the township can only cover about 60% of the value of the trucks. The ordinance also does not allow payments over time, which will have to be amended.

It was suggested that, given how fire company fundraisers are raising less and less, the ordinance should also be amended to allow 100% funding. While the trend was acknowledged, supervisors are more likely to boost the percentage over time, and as township finances allow.

For now, the 60% limit remains in place.

Supervisors were unanimous in approving a letter to

See KIDDER FIRE, page 7



# PUBLIC NOTICES

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ESQUIRE  
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10/14

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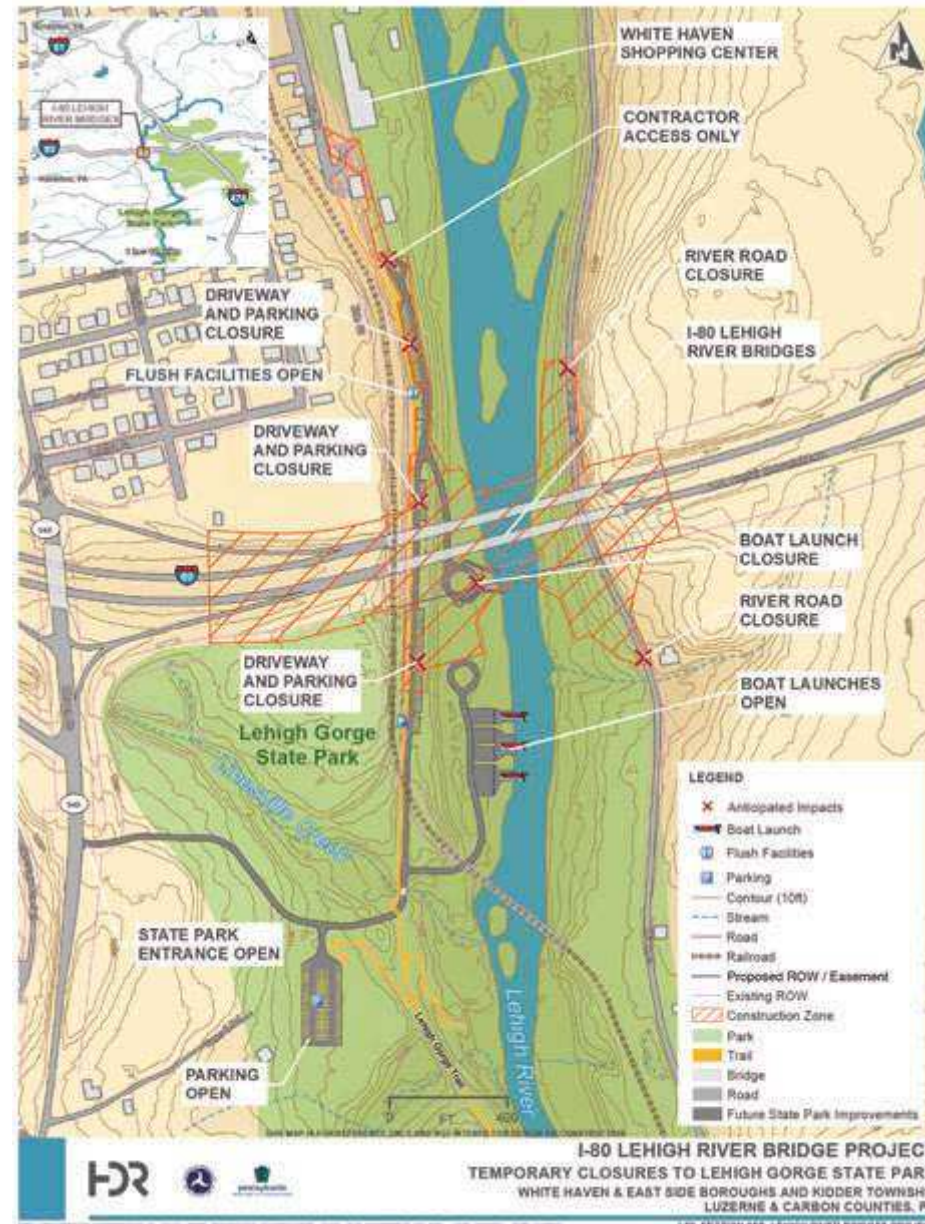
## ADVERTISEMENT FOR BIDS

**SEALED BID**  
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Contractor Address  
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Attn: Linda Szoke, Borough Manager  
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White Haven PA 18661  
570-443-9129  
The bid packet can be emailed by emailing the Borough at [whborough@whboro.org](mailto:whborough@whboro.org) or can

be picked up at borough building beginning October 5, 2021 Bidders are welcome by appointment to meet with Borough Manager and look over locations listed for lawn maintenance

**Linda Szoke  
Borough Manager  
Zoning and Code Officer  
312 Main Street  
White Haven PA 18661  
570-443-9129**

10/21



## NOTICE OF PUBLIC INVOLVEMENT I-80, Section 08B, Lehigh River Bridges Project

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PennDOT Engineering District 5-0  
1002 Hamilton Street  
Allentown, PA 18101  
Attn: Brandy Rotz, AICP – Consultant Project Manager  
[c-brotz@pa.gov](mailto:c-brotz@pa.gov)

**From:** Markowitz, Katherine  
**Sent:** Friday, January 7, 2022 2:08 PM  
**To:** Markowitz, Katherine  
**Subject:** RE: [External] Lehigh River Bridges I-80 08B Project Feedback

**From:** Cwalina Construction <cwalinaconstruction@gmail.com>  
**Sent:** Friday, October 15, 2021 2:43 PM  
**To:** Rotz, Brandy <c-brotz@pa.gov>  
**Subject:** [External] Lehigh River Bridges I-80 08B Project Feedback

**ATTENTION:** *This email message is from an external sender. Do not open links or attachments from unknown sources. To report suspicious email, forward the message as an attachment to CWOPA\_SPAM@pa.gov.*

Hi,

In response to your request for public feedback, I'd like you to consider noise reduction barriers on the bridge. We reside at 100 Towanda St in White Haven (directly up the path of the river to the other side of town) and the truck traffic traveling on the bridge causes significant noise to echo up the river which can be heard in the house even if the windows are closed. Additionally, a barrier might deter any people from attempting suicide off the bridge.

Is it possible to deaden sound, keep the public safe and keep the aesthetic view of the town as part of this project?

Thank you,

Valerie Love  
570-947-2690

--

Brian Cwalina  
Cwalina Construction  
570-606-6016  
[cwalinaconstruction@gmail.com](mailto:cwalinaconstruction@gmail.com)

**From:** Markowitz, Katherine  
**Sent:** Friday, January 7, 2022 2:15 PM  
**To:** Markowitz, Katherine  
**Subject:** RE: [External] White Haven bridge - Public Comment

---

**From:** Barbara Murphy <bmurphy@pa.metrocast.net>  
**Sent:** Tuesday, October 26, 2021 2:34 PM  
**To:** Rotz, Brandy <c-brotz@pa.gov>  
**Subject:** [External] White Haven bridge

ATTENTION: This email message is from an external sender. Do not open links or attachments from unknown sources. To report suspicious email, forward the message as an attachment to CWOPA\_SPAM@pa.gov.

Consultant Project Manager  
Brandy Rotz,

As a concerned citizen/neighbor of the Borough of White Haven we would like to know if the proposed new bridge (east and west bound route 80) will be built in the exact same area as the existing bridge? If so how will this be accomplished? For instance: lane closures , new on and off ramps which could possibly include taking land from our Lehigh Ballpark. How will the construction impact our local neighborhood? Please respond to my email [bmurphy@pa.metro cast.net](mailto:bmurphy@pa.metrocast.net) or [patmurf81@gmail.com](mailto:patmurf81@gmail.com).

Thank you ,

Patrick and Barbara Murphy  
909 Elmira St.  
White Haven, PA 18661

Sent from my iPhone

**From:** Markowitz, Katherine  
**Sent:** Friday, January 7, 2022 2:15 PM  
**To:** Markowitz, Katherine  
**Subject:** RE: [External] White Haven bridge project

---

**From:** Seth Isenberg <journalseth@pa.metrocast.net>  
**Sent:** Tuesday, October 26, 2021 11:21 PM  
**To:** Rotz, Brandy  
**Cc:** Seth Isenberg  
**Subject:** [External] White Haven bridge project

ATTENTION: This email message is from an external sender. Do not open links or attachments from unknown sources. To report suspicious email, forward the message as an attachment to CWOPA\_SPAM@pa.gov.

Brandy Rotz  
Consultant Project Manager

October 26, 2021

Four years of closure of the Lehigh Gorge Heritage Corridor trail could be very damaging to the economy of White Haven borough. Even the temporary closure of the trail this year has had a measurable impact on plaza retailers this month - as trail traffic is just not around even during the foliage season.

Looking at the plans, White Haven's downtown will be cut off from the south trail for an amazing four years. This year alone, local businesspeople were working with the Heritage Corridor to enhance the trail experience. We were looking to establish a "trail tenders" group of volunteers as ambassadors to visitors who start their trip from here, or would be passing through.

As a Main Street business ourselves, The Journal-Herald was hoping to see continued reviving of our downtown. Slowly, we have seen stores open, and recently, there is a prospect of a new building on Main Street. Losing the tourists is going to hurt. We are going to need some help from PennDOT, including highway signs indicting that we have a downtown business district, and signs on the highway for the remaining downtown trailhead.

Is it also possible for help to route bikers and hikers through town via the new driveway? Trail travelers may take the detour as an adventure.

I will add my concerns about any traffic that comes in heavy onto Route 940 that clogs up White Haven's roads. When there is an accident and the highway is closed, the simple task of pulling out from Main Street can become a ten to 15 minute wait. Through vehicles will not yield. Going to the grocery store or drug store becomes a fight with trucks and cars stuck on our little state road. We also have foot traffic, mainly young people, and I am very concerned about their safety when Route 940 becomes the only path for Interstate-using vehicles.

If an accident with a detour happens in bad weather, vehicles get stuck on the big hill on Route 940.

So, as a wrap up -

I am asking for PennDOT to provide signs to aid the downtown commercial area, and signs to promote the northern trailhead in the downtown to help draw visitors.

I am also asking for help to improve the local roads to handle the surges in traffic, and an answer to the safety problem within White Haven when bumper to bumper traffic does appear.

Seth Isenberg

General manager

The Journal-Herald of White Haven

LAKE NEWS of Lake Harmony

Journal of the Pocono Plateau of Blakeslee and Tobyhanna

And

Journal of Penn-Kidder of Albrightsville

211 Main Street, White Haven, PA 18661

570 215-0204



Comment Date	Comment Origin	Comment	Comment Tone	Concerns	Response	First Name (Person)	Last Name (Person)	Org-Company	Email (Person)	Phone (Person)	Project Association
12/16/2021	web comment	<p>The Delaware and Lehigh National Heritage Corridor (D&amp;L Trail) needs to be kept in the chain of communication for this project along with the State Park. The D&amp;L was noted in some locations of this public meeting post, but not consistently. The trail within this project scope is part of a currently 145-mile network, soon to be 165+ miles- any notice regarding the disruption of trail activities must be shared with the D&amp;L so the trail users can be accurately updated. Please add the D&amp;L to your list of organizations to keep informed (this webpage noted DCNR and White Haven).</p> <p>For the timelines regarding trail closures- Please clarify the YEAR of these anticipated impacts to the D&amp;L Trail- is the schedule outlined for the full 4 years of the project or just a portion? Are the April to October overnight closures M-F or 7 days a week? When is "night" considered to begin- will this line up with the State Park's hours of operation? Is the November to March closure M-F or 7 days a week?</p> <p>Can you expand on the "replacement" land PennDOT will provide? Does the bridge permanently impact the current alignment of the D&amp;L Trail in any way and is PennDOT considering "replacing" any portion of the D&amp;L Trail if the alignment is affected?</p> <p>When the capacity analysis was conducted did it account for pedestrian and bicycle traffic utilizing the D&amp;L Trail along Main Street? White Haven Borough is exploring intersection improvements for pedestrians (RRFB's and crosswalk repainting like you mentioned)- how might this new "stop point" affect traffic congestion and does this need to be considered when evaluating the traffic controls on adjacent intersections?</p> <p>Please note when you conduct your before/after intersection study that the D&amp;L Trail is currently closed during the State Park improvements project and any trail user counts will not be accurate until the project is complete.</p>	neutral	Information Request; Trails/Recreation/Parks		Elizabeth	Rosencrans	Delaware and Lehigh National Heritage Corridor (D&L Trail)	trail@delawareandlehigh.org	6109233548227	I-80 White Haven

**Attachment D**  
**Concurrence Letters**



February 17, 2022

Ryan Dysinger  
Assistant Director  
PA Department of Conservation and Natural Resources  
Bureau of State Parks  
PO Box 8551  
Harrisburg, PA 17105-8551

Re: I-80, Section 08B Bridges over Lehigh River Project  
White Haven Borough, Luzerne County and  
East Side Borough and Kidder Township, Carbon County, PA  
Requesting Concurrence with Section 4(f) Determination

Dear Mr. Dysinger:

The Pennsylvania Department of Transportation (PennDOT) proposes to replace I-80 Section 08B Lehigh River Bridges. The dual bridges carry I-80 over the Reading Blue Mountain Northern (RBMN) railroad, Lehigh Gorge State Park and Trail, Lehigh River, and SR 1005 (River Road), in White Haven Borough, Luzerne County, and East Side Borough and Kidder Township, Carbon County, Pennsylvania.

The bridges are proposed to be replaced using staged construction maintaining two lanes of traffic in both directions during construction. The eastbound bridge will be replaced on a new alignment immediately south of the existing eastbound bridge. The westbound bridge will be replaced on the same approximate alignment as the existing westbound bridge. Due to the shift south of the existing bridge, Slope and Aerial Easements are required from Lehigh Gorge State Park property for replacement of the bridges. Additional Temporary Construction Easements are necessary for contractor access and staging. Replacing the failing gabion wall along SR 940 in East Side Borough will require Right-of-Way. Required Right-of-Way, Proposed Slope Easement, Proposed Aerial Easements, and Temporary Construction Easements totaling approximately 6 acres are proposed to be acquired from Lehigh Gorge State Park.

Temporary impacts to State Park property and facilities are anticipated; however, the project will not adversely affect the Lehigh Gorge State Park. The majority of the state park will remain open during bridge construction, and access to the Lehigh River via the three new boat launches currently being constructed by DCNR south of the bridges will be maintained (the existing boat launch under the bridge will be temporarily closed). Access to the Lehigh Gorge Trail will be maintained; however, a portion of the trail may be closed under the bridge during low and no usage timeframes during construction. The contractor will restore disturbed areas, and full access to the park amenities will be provided upon completion. The area within the Proposed Aerial Easement adjacent to the bridges will be available for use by Lehigh Gorge State Park after construction.

As the work will take place within Lehigh Gorge State Park, a recreational facility under your jurisdiction, we are requesting your concurrence in writing with PennDOT's determination that the project will not adversely affect the recreational use of the property. We have attached for your review a draft copy of the Section 4(f) De Minimis form, which provides details of the anticipated impacts to Lehigh Gorge State Park and proposed mitigation measures.

Thank you for your assistance in this matter.


If you have any questions, please feel free to contact me at 610-871-4550 or via email, [svottero@pa.gov](mailto:svottero@pa.gov)

Sincerely,



Scott G. Vottero, PE  
Acting Assistant District Executive – Design  
PennDOT District 5-0

Enclosures: Lehigh Gorge State Park Draft Section 4(f) De Minimis Form

Concurrence:  \_\_\_\_\_ Date: 3/25/2022  
Ryan J. Dysinger  
Assistant Director, Bureau of State Parks  
Department of Conservation and Natural Resources

BUREAU OF RECREATION AND CONSERVATION

February 22, 2022

Nicole L. Auker  
Environmental Planning Supervisor  
PA Department of Transportation  
400 North Street, 7<sup>th</sup> Floor  
Harrisburg, PA 17120

RE: I-80 WHITE HAVEN BRIDGE REPLACEMENT & GABION WALL REPLACEMENT  
SR 0080 & SR 0940 River Street  
Luzerne County, Carbon County  
LWCF 42-00033, 42-00081, Lehigh Gorge State Park

Dear Ms. Auker:

This letter is being written based on PennDOT's need to proceed with the environmental process for the above referenced bridge replacement project. The Federal Highway Administration requires acknowledgement of coordination between agencies to allow the project to continue through the environmental process, to begin the Right-Of-Way process, engage with the landowner for potential acquisition, and complete the National Park Service (NPS) Compliance and Stewardship Form (C&S Form).

The replacement of the I-80 bridge and Gabion wall along SR 0940 in East Side Borough will require permanent/TCE of 6.2 acres of impact to the Land and Water Conservation Fund protected Lehigh Gorge State Park. The impact area breakdown is as follows; 0.1 acre of Required Right-of-way, 0.4 acres of proposed Slope Easement, 0.5 acres of proposed Aerial Easement, 4.9 acres of TCE within LGSP, and 0.3 acres of TCE within DCNR Easement.

The proposed replacement property acreage is approximately 6.45 acres, Parcel ID (65A-51-C402) in Penn Forest Township. DCNR received NPS concurrence on the proposed conversion and replacement property on 11/22/2021. DCNR Bureau of State Parks, DCNR Bureau of Recreation and Conservation, and PennDOT agree on the proposed replacement property to satisfy the value, location and recreational usefulness criteria under the Land and Water Conservation Fund Act as mitigation for both the I-80 bridge replacement and the SR 0940 gabion wall replacement.

DCNR recognizes PennDOT's need to proceed through the project planning and design phases and agrees with the proposed conversion footprint and replacement property while we await NPS concurrence. Please understand NPS has ultimate approval authority of the conversion package including replacement property.

If you have questions or concerns, please contact me at (717) 783-0318 or Ashley Rebert at (717) 772-3322.

Sincerely,

*Emma Harrison*

Emma Harrison  
Land Conversion Specialist  
Bureau of Recreation and Conservation

cc:, Ashley Rebert

## Krommes, Kathy

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**From:** Krommes, Kathy  
**Sent:** Monday, March 28, 2022 3:06 PM  
**To:** Krommes, Kathy  
**Subject:** FW: [External] I-80 Bridges conversion/Replacement properties - NPS concurrence

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**From:** Morrison, Mary (Missy) <[Mary\\_Morrison@nps.gov](mailto:Mary_Morrison@nps.gov)>  
**Sent:** Monday, November 22, 2021 3:26 PM  
**To:** Rebert, Ashley <[arebert@pa.gov](mailto:arebert@pa.gov)>  
**Cc:** Imgrund, Lauren <[limgrund@pa.gov](mailto:limgrund@pa.gov)>  
**Subject:** [External] I-80 Bridges conversion/Replacement properties - NPS concurrence

**ATTENTION:** This email message is from an external sender. Do not open links or attachments from unknown sources. To report suspicious email, forward the message as an attachment to [CWOPA\\_SPAM@pa.gov](mailto:CWOPA_SPAM@pa.gov).

Good Afternoon Ashley-

The NPS, LWCF program has reviewed the proposed conversion footprint and replacement property for the SR0080 Section 550 conversion at LWCF 42-00033 Walter Dick Memorial Park and the I-80 White Haven Bridge, conversion replacement property materials (Impacts Lehigh Gorge State Park, Luzerne County).

### Walter Dick Mem Park

The NPS, LWCF can concur that the proposed replacement property adjacent to Walter Dick Memorial Park satisfies the LWCF Act and Post Completion Compliance Regulations (36 CFR 59.3) for appropriate replacement property.

However, please confirm the final conversion footprint acreage: I had to go back and check through Jack's emails and a February email with attachment from you indicates the conversion footprint impact to this park is 7.6 acres; within the PennDot letter of August 31st 2021, PennDOT indicates a 5.5 acre footprint and approximately 6 acres replacement property.

An email from you with the updated/explanation of what the actual conversion footprint is meant to be will suffice. Also keep in mind that when the conversion package is submitted we will need maps indicating the correct acreage.

### Lehigh Gorge SP conversion - I-80 White Haven Bridge, Impacts Lehigh Gorge State Park, Luzerne County

The NPS LWCF also concurs with the proposed replacement property at Unionville Road for the replacement of 7 acres of land converted from Recreation use at Lehigh Gorge SP meets the LWCF Act and regulations.

I sincerely apologize for the delay in reviewing the documentation for the above projects; thank you for your patience!

If you have any additional questions, please let me know.

Missy

**Missy Morrison**

State and Local Assistance Programs / Compliance Team Lead

National Park Service

1849 C Street NW

Washington, DC 20240

Mobile: (202) 641-6557



**Appendix H**  
**Bicycle and Pedestrian Checklist**



## Planning and Programming Checklist

Project I-80 Section 08B, White Haven Bridges Project

SR 0080, Section 08B Segment start 2720 EB, 2721 WB Offset start 1542 EB, 2330 WB

Team Members PennDOT District 5-0, Brandy Rotz, PM; HDR, John Baumgardner, PM

Date 4/30/2021

Item	Considerations	Check	Comments
1. Consistency with Bicycle/Pedestrian Planning Documents	Is the transportation facility included in or related to bicycle and pedestrian facilities identified in a master plan? <ul style="list-style-type: none"> <li>• MPO/LDD bike/ped plan.</li> <li>• Local planning documents.</li> <li>• BicyclePA Routes.</li> <li>• Statewide Bicycle and Pedestrian Master Plan.</li> </ul>	Included in? No  Related to? Yes	The transportation facility, I-80 and the bridges over Lehigh River, has no existing bicycle and/or pedestrian facilities and none are proposed. Bicycles and pedestrians are generally not allowed on interstate highways. During construction, the project will temporarily close and detour PA Bike Route L which follows River Road (SR 1005) along the east side of the Lehigh River through the project area. The PA Bike Route L detour will be established and implemented through coordination with PennDOT's Statewide Bicycle and Pedestrian Coordinator.
	Will the transportation facility provide continuity and linkages with existing or proposed bicycle/pedestrian facilities?	Yes	As described in multiple sections within this Checklist, the project will incorporate measures to ensure continuity and linkages to the existing PA Bike Route L and Lehigh Gorge Trail/D&L Trail that the I-80 bridges cross over. No permanent disruption of continuity or linkages will result from the project.
	Is the transportation facility included in or related to a regional/local recreational plan? <ul style="list-style-type: none"> <li>• Rails-to-Trails.</li> <li>• Greenways.</li> <li>• Local, State, National Parks.</li> </ul>	Included in? No  Related to? Yes	The transportation facility, I-80 and the bridges over Lehigh River, has no existing bicycle and/or pedestrian facilities and none are proposed. The bridges cross over Lehigh Gorge State Park and Lehigh Gorge Trail/D&L Trail. Mitigation measures will be incorporated into the project to ensure safety of bicycle riders and pedestrians during construction. It is anticipated that the trail will remain open for the majority of construction duration but may require short-term closures during certain construction activities such as bridge demolition, pier construction and setting beams. Agency coordination is ongoing to determine details of mitigation measures.
2. Existing and Future Usage	Do bicycle/pedestrian groups regularly use the transportation facility? <ul style="list-style-type: none"> <li>• Bike clubs.</li> <li>• Bicycle commuters.</li> <li>• Hiking, walking, or running clubs.</li> <li>• Skateboarding or rollerblading groups.</li> <li>• Bicycle touring groups.</li> <li>• General tourism/sightseeing.</li> </ul>	No	The transportation facility, I-80 and the bridges over Lehigh River, has no existing bicycle and/or pedestrian facilities and none are proposed.
	Does the existing transportation facility provide the only convenient transportation connection/linkage between land uses in the local area or region?	No	While I-80 is a heavily traveled east/west interstate highway in the region, it is not the only connection/linkage in the vicinity of the project. SR 940 crosses the Lehigh

			River north of the project and connects the communities of White Haven and East Side in the project area. PA Bike Route L crosses the Lehigh River on SR 940.
	<p>Could the transportation facility have favorable or unfavorable impacts upon the bike tourism/economy of an area/region? Consider:</p> <ul style="list-style-type: none"> <li>• Local businesses</li> <li>• Chamber of Commerce</li> <li>• Tourism Promotion Agencies.</li> </ul>	Yes	<p>The transportation facility, I-80 and the bridges over Lehigh River, will be constructed in phases to maintain two lanes of traffic in both directions during construction to minimize disruption to local businesses and tourism. Mitigation measures will be incorporated into the project to ensure safety of bicycle riders and pedestrians in Lehigh Gorge State Park during construction. Agency coordination is ongoing to determine details of mitigation measures.</p> <p>As currently planned, the project bridges will be tolled facilities. Potential favorable and unfavorable impacts of tolling at this location are being studied and impacts will be described in the NEPA document, including mitigation measures as appropriate.</p>
	Are there physical or perceived impediments to bicycle or pedestrian use of the transportation facility?	Yes	The transportation facility, I-80 and the bridges over Lehigh River, has no existing bicycle and/or pedestrian facilities and none are proposed. Bicycles and pedestrians are generally not allowed on interstate highways.
	Is there a higher than normal incidence of bicycle/pedestrian crashes in the area?	No	
3. Safety	Is the transportation facility in a high-density land use area that has pedestrian/bike/motor vehicle traffic?	Yes	The transportation facility, I-80 and the bridges over Lehigh River, has no existing bicycle and/or pedestrian facilities and none are proposed. The bridges cross over Lehigh Gorge State Park and Lehigh Gorge Trail/D&L Trail, a popular local and tourist destination for pedestrians and bicycle users.

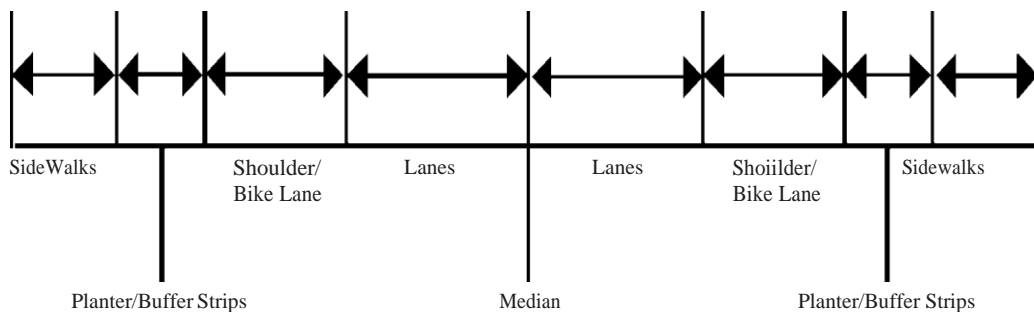
3. Safety (continued)	Is there a high amount of crossing activity at intersections?  • Midblock • Night crossing activity • Adequate lighting.	Not Applicable	
	Would the transportation facility (and all users) benefit from widened or improved shoulders or improved markings (shoulders, crosswalks)?	Yes	The transportation facility, I-80 and the bridges over Lehigh River, has no existing bicycle and/or pedestrian facilities and none are proposed. While not proposed due to pedestrian or bicycle use, wider shoulders will be provided on the bridges.
4. Community and Land Use	Is the transportation facility in a city, town, or village?	Yes	The boroughs of White Haven and East Side are located west and east of the project bridges, respectively. Portions of both boroughs are within the project area.
	Is the transportation facility within/near a community or neighborhood?	Yes	The boroughs of White Haven and East Side are located west and east of the project bridges, respectively. Portions of both boroughs are within the project area.
	Is the transportation facility the “main street” in a community or town?	No	
	Could bicycle or pedestrian usage impact economic development?	No	
	Are sidewalks needed in the area?  • Presence of worn paths along the facility. • Adjacent land uses generate pedestrian traffic. • Possible linkages/continuity with other pedestrian facilities.	No	
	Is the transportation facility a link between complimentary land uses?  • Residential and commercial. • Residential and business.	No	
	Is the transportation facility in close proximity to hospitals, elderly care facilities, or the residences or businesses of persons with disabilities?	No	
	Is the transportation facility within or near educational buildings?	No	
5. Transit	Is the transportation facility on a transit route?	No	The Lackawanna & Luzerne Counties Joint Comprehensive Plan & Long-Range Transportation Plan and Northeastern Pennsylvania (NEPA) MPO Long-Range Transportation Plan show no mass transit in the project area.
	Is the transportation facility near park and-ride lots?	Yes	A park-and-ride lot is located along SR 940 south of the I-80/SR 940 interchange.
	Are there existing or proposed bicycle racks, shelters or parking available? Are there bike racks on buses?	Yes	A bike rack is located at the park-and-ride lot along SR 940.

6. Traffic Calming	Is the community considering traffic calming as a possible solution to speeding and cut-through traffic?	Not Applicable	
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## Scoping Checklist

Project I-80 Section 08B, White Haven Bridges Project  
 SR 0080, Section 08B Segment start 2720 EB, 2721 WB Offset start 1542 EB, 2330 WB  
 Team Members PennDOT District 5-0, Brandy Rotz, PM; HDR, John Baumgardner, PM  
 Date 4/30/2021

### Right-of-Way Needs Diagrams



Element	Number Required	Width Required	Total Width
Sidewalks	0	0	0
Buffer Strips	0	0	0
Shoulders	2	8 ft (inside) 12 ft (outside)	40 ft
Lanes	4	12 ft	48 ft
Median	1	Varies, 60 - 110 ft	110 ft
<i>Total Right-of-Way Required</i>			198 ft

\* Table presents typical Roadway Section.

### Pedestrian Facilities

Item	Considerations	Check	Comments
1. Sidewalks	Appropriate width: <ul style="list-style-type: none"> <li>• 1.5 m —2.1 m (3'-7') for residential, commercial, and industrial.</li> <li>• 2.5 m (8') minimum for high use areas/CBD.</li> <li>• 2.1 m (7') width for bridges.</li> <li>• 0.6 m (2') shy distance for vertical barriers.</li> <li>• 1.2 m —2.1 m barrier separating traffic from pedestrians on bridges.</li> </ul>	Not Applicable	

1. Sidewalks (continued)	Applicability of planter or buffer strips.		
	Connectivity with other pedestrian facilities.		
	Proximity to transit bike/ped generators: <ul style="list-style-type: none"> <li>• Transit stops.</li> <li>• Schools.</li> <li>• Park &amp; rides.</li> <li>• Nursing homes.</li> <li>• Offices.</li> <li>• Business environments.</li> <li>• Athletic fields.</li> <li>• Recreation facilities.</li> </ul>		
	Observe pedestrian patterns for special needs such as: <ul style="list-style-type: none"> <li>• Midblock crossings.</li> <li>• Islands and refuges.</li> <li>• Night crossing activity.</li> </ul>		
	ADA needs and concerns.		
2. Signalized Intersections	Crosswalks provided and marked.	Not Applicable	
	Intersection bike/ped crash history reviewed.		
	Is there a dedicated pedestrian phase, if so how long?		
	Crossing distance is minimized.		
	Ped heads and ped pushbuttons provided.		
	ADA needs and concerns.	Retirement Homes	
	Schools		
	Medical Facilities		
3. Traffic Calming	Is the community considering traffic calming as a means to curb speeding and cut-through traffic?	Not Applicable	

## Bicycle Facilities

Item	Considerations	Check	Comments
1. Bikelanes/Paved Shoulders	Appropriate width of bike lane: <ul style="list-style-type: none"> <li>• 1.5m (3') adjacent to curb</li> <li>• 1.8m (6') standard.</li> </ul>	Not Applicable	
	Connectivity with other facilities. <ul style="list-style-type: none"> <li>• Bike lanes</li> <li>• Shared use trails</li> <li>• Trail heads/parking areas.</li> </ul>		
	Maximize width of shoulders and provide appropriate markings as per AASHTO Green Book.		
	3 m (10') vertical clearance from fixed obstructions (excluding road signs).		
	Angle and smoothness of railroad crossings. Avoid angles of incidence of <70 degrees or re-design.		
	Bridge accesses provided/pinch points avoided.		
2. Signalized Intersections	Parking parallel or angled.	Not Applicable	
	Inventory existing bicycle facilities.		
	Intersection bike/ped crash history reviewed.		
	Crossing distance is minimized.		
	Considerations for bikes making turns.		
	Bike detection. Elevated push buttons.		
3. Traffic Calming	Is the community considering traffic calming as a means to curb speeding and cut-through traffic?	Not Applicable	



**Appendix I**  
**Technical Support Data Index**

## **I-80 Lehigh River Bridges Technical Support Data Index**

Technical reports and memos have been prepared for the subject areas within the I-80 Lehigh River Bridges Environmental Assessment and are included in the project file. Specific documentation for each chapter of the Environmental Assessment is referenced within the chapters and listed (with links) below:

### **Chapter 1: Introduction**

- *Alternative Funding: Planning and Environmental Linkages Study* (September 2021)

### **Chapter 3: Alternatives**

- *I-80 Lehigh River Bridges Diversion Traffic Evaluation* (March 2022)

### **Chapter 4.1: Aquatic Resources**

- *I-80 Lehigh River Bridges Final Wetland ID and Delineation Report* (February 2021, Revised February 2022)
- *I-80 Lehigh River Bridges Hydrologic and Hydraulic Report* (November 2021)

### **Chapter 4.2: Land**

- *I-80 Lehigh River Bridges Phase I ESA report* (April 2021)
- *I-80 Lehigh River Bridges Phase I Expanded ESA report* (November 2021)

### **Chapter 4.4: Cultural Resources**

- Project Path (<https://path.penndot.gov/ProjectDetails.aspx?ProjectID=55546>)

### **Chapter 4.6: Air Quality and Noise**

- *I-80 Lehigh River Toll Diversion Noise Analysis Report* (January 2022)

### **Chapter 5: Public Involvement**

- *I-80 Over Lehigh River Bridges Project Virtual Public Meeting* (November 17 to December 17, 2021) – available on website
- *I-80 Over Lehigh River Bridges Public Meeting Summary* (January 2022)

### **Chapter 6: Environmental Justice**

- *I-80 Lehigh River Environmental Justice (EJ) Analysis* (March 2022)

**Appendix J**  
**Distribution List**

## DISTRIBUTION LIST

### Federal Agencies

**Advisory Council on Historic Preservation**

*Eastern Office of Review*

Attn: Preservation Specialist

**Federal Emergency Management Agency**

Attn: Mitigation Division

**U.S. Army Corps of Engineers (USACE)**

*Philadelphia District*

Attn: Chief, Environmental Resources  
Branch

**U.S. Fish and Wildlife Service (USFWS)**

*Pennsylvania Field Office*

**U.S. Department of Health & Human Services**

*Centers for Disease Control & Prevention*

Attn: Chief, Special Programs Group

**U.S. Department of Housing & Urban  
Development**

*Pennsylvania State Office*

Attn: Environmental Officer

**U.S. Department of Interior**

*Office of Environmental Policy and Compliance*

Attn: Director

**U.S. Department of Transportation**

*Federal Transit Administration*

*Office of Planning and Program Development*

Attn: Transportation Program Specialist

**U.S. Environmental Protection Agency**

*Region III (3ES43)*

Attn: Chief, Environmental Assessment and Protection  
Division

**U.S. Department of Agriculture National**

*Resources Conservation Service* Attn:

Water Resources Department

**U.S. Environmental Protection Agency**

*Office of Federal Activities*

**National Park Service**

*Office of Policy*

**D&L National Heritage Corridor**

## State Agencies

### **PA Department of Agriculture**

*Bureau of Farmland Preservation* Attn:  
Director

### **PA Department of Community and Economic Development**

*Policy Office*  
Attn: Director

### **PA Department of Conservation and Natural Resources**

*Office of Policy*  
Attn: Director

### **PA Department of Environmental Protection**

*Office of Policy*  
Attn: Director

### **PA Department of Environmental Protection**

*Northeast Regional Office*

### **PA Department of Health**

*Office of Policy*  
Attn: Executive Policy Assistant

### **PA Fish and Boat Commission**

*Environmental Services Division*  
Attn: Chief, Environmental Services Division

### **PA Game Commission**

*Environmental Planning and Habitat Protection*  
Attn: Chief, Environmental Planning and  
Habitat Protection Division

### **PA Game Commission**

*Northeast Region*

**PA Hist PA Historical and Museum Commission** *Bureau  
for Historic Preservation Commonwealth* Attn: Chief,  
Division of Archaeology and Protection

### **Pennsylvania Governor's Office**

*Policy Development*

### **Public Utility Commission (PUC)**

*Utility Office*  
Attn: Administrator

## RPO

### **Northeastern Pennsylvania Alliance**

Attn: Community and Government Services Manager

## OWJ (Section 4(f))

### **PA Department of Conservation and Natural Resources**

*Bureau of State Parks*  
Attn: Administrative Manager

### **PA Department of Conservation and Natural Resources**

*Bureau of Scenic Rivers*  
Attn: Administrative Manager

### **PA Fish and Boat Commission**

*Bureau of Boating*  
Attn: Chief, Waterways and Marina Management Branch

## **Native American Tribes**

**Absentee-Shawnee Tribe of Indians of  
Oklahoma**

**Delaware Nation, Oklahoma**

**Delaware Tribe of Indians**

**Eastern Shawnee Tribe of Oklahoma**

**Oneida Indian Nation**

**Onondaga Nation**

**Seneca-Cayuga Nation**

**Shawnee Tribe**

**Stockbridge-Munsee Community, Wisconsin**

**Tuscarora Nation**

**Appendix K**  
**List of Preparers**

<b>Name</b>	<b>Organization</b>	<b>EA Role</b>	<b>Education</b>	<b>Years</b>
Camille Otto Director of Planning, Environment, and Finance	FHWA PA Division	FHWA Approver	B.S. Biology	25
Jon Crum Senior Environmental Specialist	FHWA PA Division	FHWA Reviewer	B.S. Biology M.S. Environmental Science and Management	17
Scott Vottero, PE Acting Assistant District Executive for Design	PennDOT District 5-0	Engineering Reviewer	B.S. Civil Engineering	26
Jerry Neal District Environmental Supervisor	PennDOT District 5-0	Environmental Reviewer	B.A. Biology	32
Brandy Rotz Consultant Project Manager	PennDOT District 5-0	Consultant Project Manager	M.S. Geo-Environmental Studies	19
Drew Ames Environmental Planning Manager	PennDOT Central Office	Environmental Reviewer	B.H Communications M.S. Community and Regional Planning	26
Kenda Gardner Deputy Chief Counsel	PennDOT Office of Chief Counsel	Legal Review	B.S. Chemistry J.D.	28
Neal Brofee Environmental Counsel	PennDOT Office of Chief Counsel	Legal Review	B.A. Mathematics J.D.	24
Kristina Thompson Architectural Historian	PennDOT Central Office	Above-Ground Cultural Resources	B.S. Historic Preservation; M.A. Anthropology	28
Steven McDougal Archeologist	PennDOT Central Office	Archaeology	M.A. Anthropology	29
Nina Ertel, PE Project Development Engineer	PennDOT Central Office	Engineering Reviewer	B.S. Civil Engineering M.S. Civil Engineering	11
Diane Nulton Environmental Project Manager	HDR	EA Project Manager	B.S. Biology/Ecology	35
John Baumgardner, PE Structures Section Manager	HDR	Bridge Replacement Project Manger	B.S. Civil Engineering M.S. Civil Engineering	20
Kathleen Krommes, ENV SP Environmental Project Manager	HDR	Environmental Lead, Technical Writer/Editor	B.S. Chemical Engineering	35
Katherine Markowitz Environmental Scientist	HDR	EA Technical Writer/Editor	B.S. Marine and Environmental Biology and Policy	8
Jennifer Nolan-Kremm Transportation Hydraulics Lead	HDR	H&H and Permitting Lead	B.S. Physics & M.S. Civil Engineering	16
James Koval Project Manager	HDR	Hazardous Materials	BS Geography – Environmental Science	20
John McPherson, AICP Env. Services Director	HDR	EA, Cumulative Impacts	B.A. Math/Economics; M.U.P.	30
Linda Smith Senior Environmental Planner	HDR	Author, Cumulative Impacts	M.S.E. Civil Engineering	17



<b>Name</b>	<b>Organization</b>	<b>EA Role</b>	<b>Education</b>	<b>Years</b>
Jenn Walsh, PE Traffic & Planning Section Manager	HDR	Traffic Diversion Analysis	B.S. Civil Engineering; M.S. Civil Engineering	28
Ken O'Brien, PE Senior Project Manager	HDR	Traffic Diversion Analysis	B.S Civil Engineering;	27
Audrey Heffernan Senior Environmental Planner	HDR	Environmental Justice	B.A. Math; M.A. Math; M.S. City & Regional Planning	28
Connie Eskin Administrative Coordinator	HDR	Technical Editor	Pennsylvania State University	25
Tina Adair Technical Editor	HDR	Technical Editor	B.S. Communications	35
Frank Brillhante GIS Manager	HDR	GIS Analysis	B.S. Engineering; M.S Environmental Engineering	28
Ben Reiman Sr. Project Manager	Lotus Environmental Consulting /Division of NTM Engineering	Environmental Analysis – Natural Resources	B.S. Environmental Science	24
Margaret Parker Sr. Historian/ Project Manager	Lotus Environmental Consulting /Division of NTM Engineering	Environmental Analysis – Cultural Resources	B.A. History	30
Gabrielle Vicari Historian	Lotus Environmental Consulting /Division of NTM Engineering	Environmental Analysis – Cultural Resources	M.A. Historic Preservation	8
Connor Sullivan Environmental Scientist	Lotus Environmental Consulting /Division of NTM Engineering	Environmental Analysis – Natural Resources	M.S. Environmental Science	9
Alyssa Hyziak Environmental Scientist	Lotus Environmental Consulting /Division of NTM Engineering	Environmental Analysis – Natural Resources	B.A. Anthropology	6
Terri Slack National Discipline Lead, Trans. Revenue Systems & Operations	CDM Smith	Traffic Forecasting	BA Economics; BA Political Science M.B.A Management	33
Tarannum Rima Travel Demand Modeler	CDM Smith	Traffic Forecasting	B.S. Civil Engineering M.S Transportation Engineering M.S. Computer Systems Engineering	16
Nathaniel Weinstock Air Quality and Acoustical Group Leader, Sr. Air Quality and Acoustical Scientist	Navarro & Wright	Diversion Route Noise Analysis	B.S. Public Service	22
Robert C. Kolmansberger Director of Environmental Services, Sr. Air Quality & Acoustical Scientist	Navarro & Wright	Diversion Route Noise Analysis, QA/QC	B.A. Geography & Environmental Planning	30

## **Appendix L**

### **References**

## REFERENCES

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